

DEPARTMENT OF PUBLIC WORKS AND ENVIRONMENTAL SERVICES

STAFF REPORT

- PROPOSED COUNTY CODE AMENDMENT
- PROPOSED PFM AMENDMENT
- APPEAL OF DECISION
- WAIVER REQUEST

Proposed Amendments to the PFM Re: Design of Public Streets and Sidewalks

Authorization to Advertise	<u>March 29, 2011</u>
Planning Commission Hearing	<u>May 5, 2011 at 8:15 p.m.</u>
Board of Supervisors Hearing	<u>June 7, 2011, at 4:00 p.m.</u>
Prepared by:	Code Analysis Division <u>JAC (703) 324-1720</u> March 29, 2011

STAFF REPORT

A. Issues:

The proposed amendments to the Public Facilities Manual of Fairfax County, Virginia (PFM) address the new State Secondary Street Acceptance Requirements and Subdivision Street Design Guide and emergency access and operations requirements.

B. Recommended Action:

Staff recommends that the Board of Supervisors adopt the proposed amendments.

C. Timing:

Board of Supervisors Authorization to Advertise – March 29, 2011.

Planning Commission Public Hearing – May 5, 2011, at 8:15 p.m.

Board of Supervisors Public Hearing – June 7, 2011, at 4:00 p.m.

Effective Date – June 8, 2011, at 12:01 a.m.

D. Source:

Department of Public Works and Environmental Services

E. Coordination:

The proposed amendments have been prepared by the Fairfax County Department of Public Works and Environmental Services and coordinated with the Fairfax County Department of Transportation (FCDOT), Fairfax County Fire and Rescue Department, and the Office of the County Attorney. In addition, the proposed PFM amendments have been recommended for approval by the Engineering Standards Review Committee (ESRC) except for the minimum street widths for streets with parking on both sides. A letter to the Board from the ESRC is included as Attachment B. The proposed minimum street widths for streets with parking on both sides have been recommended by the Fire Marshal to comply with the Virginia Fire Prevention Code.

F. Background:

SECONDARY STREET ACCEPTANCE REQUIREMENTS

The 2007 Virginia General Assembly added § 33.1-70.3 to the Code of Virginia, which requires that the Commonwealth Transportation Board (CTB) develop secondary street acceptance requirements. These new requirements, called the Secondary Street Acceptance Requirements (SSAR), must be met before secondary streets constructed by developers, localities and entities other than VDOT will be accepted into the secondary system of state highways for maintenance. The SSAR (24 VAC 30-92) replaces and supersedes the old Subdivision Street Requirements (24 VAC 30-91). The Fairfax County Department of Transportation provided comments, on the Board's behalf, to VDOT on the SSAR prior to their adoption in 2008. The SSAR became effective on March 9, 2009.

Section 33.1-70.3 of the Code of Virginia provides that the new regulations shall include provisions that the CTB deems necessary and appropriate to achieve the safe and efficient operation of the state's transportation network and include the following:

- Requirements to ensure the connectivity of road and pedestrian networks with the existing and future transportation network;
- Provisions to minimize stormwater runoff and impervious surface area; and
- Provisions for performance bonding of new secondary streets and associated cost recovery fees.

The major elements of the SSAR that are different from the old Subdivision Street Requirements are described below:

Connectivity Requirements

The SSAR increases the number of access points to adjacent properties or developments. It is expected that increased connectivity will result in more effective use of the transportation infrastructure. In general, increasing the connectivity of a street network will provide for more alternative routes, instead of forcing all traffic in a subdivision to one entrance or exit onto an arterial street. The SSAR establishes three area types in the Commonwealth: compact, suburban, and rural, and the connectivity requirements in the SSAR are based on area type. Currently, Fairfax County is located entirely within a compact area type according to U.S decennial census data.

The connectivity requirements include the following:

- The public streets in all area types must be designed and constructed in accordance with the SSAR and the VDOT Road Design Manual.
- The street layout in all area types must provide sufficient connections in multiple directions and to multiple properties.
- A measurement of connectivity, called the connectivity index, must be

equal to or greater than 1.6 in compact area types, and equal to or greater than 1.4 in suburban area types. The connectivity index is calculated by dividing the number of street segments by the number of intersections and cul-de-sacs in the development's street layout. A more grid-like street layout would have a higher connectivity index, while a street layout comprising only cul-de-sac streets and one way in or out would have a lower connectivity index.

- The block layout and other features of the development in compact and suburban areas types must be designed to provide reasonably direct pedestrian movement throughout the development and to adjoining properties.

The connectivity requirements are addressed in the proposed PFM amendments by referencing the requirements in the SSAR.

Pedestrian Accommodation Requirements

The SSAR provides pedestrian accommodation (e.g. sidewalks) requirements based on the median lot size or the floor to area ratio (FAR) of the development, proximity to public schools, the adjoining developments pedestrian network, and the functional classification of the street.

Pedestrian accommodations must be provided under the following conditions:

- In developments where the median lot size is one-half acre or less, or the FAR is 0.4 or greater, pedestrian accommodations must be provided on both sides of the street. The proposed PFM amendments include these requirements.
- In developments where the median lot size is between one-half acre to two acres, pedestrian accommodations must be provided on at least one side of the street. The proposed PFM amendments include this requirement.
- On new streets that are within one-half mile of a public school, pedestrian accommodations must be provided on at least one side of the street. The current PFM requirement is stricter than this requirement and no change to the PFM requirement is proposed.
- A new street shall include pedestrian accommodations if a connection is proposed to a stub street that has pedestrian accommodations. The proposed PFM amendments refer to the SSAR connectivity requirements that address pedestrian movement to adjoining properties and consequently, address this particular requirement regarding stub street connections.
- On all new streets that are functionally classified as collectors and arterials with two travel lanes, pedestrian accommodations shall be provided on at least one side of the street. The proposed PFM amendments address this requirement.

- On all new streets that are functionally classified as collectors and arterials with three or more travel lanes, pedestrian accommodations shall be provided on both sides of the street. The proposed PFM amendments address this requirement.

The SSAR indicates that sidewalks and trails that are located entirely within the VDOT right-of-way and constructed to VDOT standards will be maintained by VDOT. Currently, VDOT will accept a 10-foot wide asphalt trail for maintenance. However, VDOT may consider accepting an 8-foot wide trail under rare instances where bicycle traffic is expected to be low, pedestrian use is expected to be occasional, there will be safe and frequent passing opportunities, and the path will not be subject to maintenance loading conditions that would cause pavement damage. The County continues to discuss this requirement with VDOT in connection with the County's pedestrian and bicycle initiatives. If narrower or alternative surface trails are constructed to conform to the Comprehensive Plan, they must be maintained by an entity other than VDOT.

Stormwater Management

The SSAR allows stormwater facilities to be placed in the right-of-way. However, VDOT will not maintain the facility and an agreement must be executed between the locality and VDOT regarding maintenance responsibility. This may provide some leeway in the future regarding the design of low impact development facilities such as bioretention, vegetated swales and tree box filters provided an agreement can be developed that does not place an undue burden on the County. Acceptable facilities must be included in VDOT's Drainage Manual, the Department of Conservation and Recreation's Stormwater Handbook, or supplemental directives. Currently, a limited number of stormwater management facility designs are included in these documents. However, the state is in the process of revising the Stormwater Handbook and the revisions may include additional low impact development facilities.

Performance Bonding and Cost Recovery Fees

The revisions to the performance bonding process do not affect the County's current land development process. However, the proposed regulations have established a new cost recovery fee structure and increased costs will be incurred on County projects and private developments that include new public roads or public road improvements.

APPENDIX B(1) - SUBDIVISION STREET DESIGN GUIDE OF THE VDOT ROAD DESIGN MANUAL

VDOT revised the Road Design Manual to address the new requirements in

the SSAR. The new design standards are provided in Appendix B(1) of the VDOT Road Design Manual. The proposed amendments to the PFM reference the VDOT manual for design standards rather than restating the standards. This is important because, unlike the SSAR, changes to the Road Design Manual are not required to go through a public hearing process and may be revised more frequently than regulations. In fact, the VDOT Road Design Manual has been revised at least every 6 months. These amendments would allow the PFM to remain current with the VDOT manual in order to facilitate VDOT acceptance of streets that are constructed through the land development process. Situations where the County design standards are purposely different from the state standards will remain in the PFM.

To date, the majority of VDOT revisions has been related to transportation engineering standards that are usually based on recommendations from nationally recognized organizations such as the American Association of State Highway and Transportation Officials (AASHTO) and the Federal Highway Administration (FHA). The majority of the revisions have been acceptable to the County. In the unusual instance where the County would desire a stricter standard than VDOT, an amendment to the PFM would have to be adopted in order to deviate from the state standards.

For example, it was determined that some of the widths established in the state design standards are too narrow to comply with the statewide fire prevention code that calls for an unobstructed width of no less than 20 feet on fire apparatus access roads, which include public streets. In addition, the VDOT Road Design Manual provides for different minimum street widths based on no parking allowed, parking allowed on one side of the street and parking on both sides of the street. In general, for streets where no parking is allowed on one or both sides of the street, it would be difficult to continually enforce those parking restrictions. In addition, restricted parking would be inconvenient for residents when accommodating guests or extra cars in the household. Consequently, the amendment requires a minimum street width of no less than 36 feet wide on secondary streets to accommodate parking on both sides of the street and the unobstructed width of 20 feet. In order to accommodate context-sensitive solutions, the amendment allows for narrower streets or restricted parking conditions that are approved by the Board through a zoning action, and urban road design standards that have been established through memoranda of understanding between VDOT and the County for specific areas such as Tysons Corner.

ESRC RECOMMENDATIONS

The ESRC does not recommend approval of the proposed amendments related to street width. A letter to the Board from the ESRC is included as Attachment B. The ESRC believes that there should be uniformity between

the County's public street standards and VDOT's street standards, and that adopting wider street widths runs counter to the County's environmental initiatives.

Staff agrees that County standards should align with VDOT standards wherever possible. However, some of VDOT's minimum street widths are too narrow to comply with the Virginia Fire Prevention Code and public safety is of paramount importance. Although staff recognizes that narrower streets encourage slower speeds, which is safer for pedestrians, and minimize impervious area, which is better from an environmental perspective, emergency access and operations should not be compromised.

The letter from the ESRC mentions that in many single-family detached developments there may not be a lot of on-street parking; therefore, narrower streets would be acceptable. Unless the development is signed as having restricted parking and that parking restriction is enforced, there can be no assurance that parked vehicles will not obstruct emergency access and operations. Should parking restrictions on public streets become widespread, it would put a strain on police resources. Therefore, this approach is not recommended by staff.

Staff presented the issues regarding minimum street widths to the Board at the Development Process Committee meeting on November 23, 2010.

G. Summary of Proposed Amendments:

Where possible, the proposed amendments refer to the SSAR, VDOT Road Design Manual or other relevant state manuals rather than restating requirements or design standards for sight distance, grade, right-of-way width, and geometric design. The proposed amendments include revisions to the text of Chapter 7 of the PFM as well as Plates 1-7, 2-7 and 3-7, and their metric counterparts.

The amendment deviates from the VDOT Road Design Manual by requiring a minimum street width of 36 feet (curb face to curb face) on curb and gutter streets, and a minimum total street width (pavement width and gravel shoulders) of 36 feet for ditch-section streets (see Attachment C for an illustration). These minimum widths accommodate parking on both sides of the street and an unobstructed width of 20 feet for emergency access and operations. In order to accommodate context-sensitive solutions, the amendment allows for narrower streets or restricted parking conditions that are approved by the Board through a zoning action, and urban road design standards that have been established through memoranda of understanding between VDOT and the County for specific areas such as Tysons Corner.

Staff Report
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Revisions to Chapter 8 of the PFM reflect the pedestrian accommodation requirements established in the SSAR, except that the County pedestrian accommodations requirements regarding proximity to schools are more extensive (1.0 mile County versus 0.5 mile VDOT) remain unchanged.

H. Attached Documents:

Attachment A- Proposed amendments to Chapters 7 and 8 of the PFM
Attachment B- ESRC letter, dated November 22, 2010
Attachment C- Street cross-section with fire truck

AMENDMENTS TO CHAPTER 7 OF THE PUBLIC FACILITIES MANUAL

Amend Table of Contents Section 7-0200, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

- 7-0200 PRELIMINARY STREET PLANNING**
- 7-0201 Public Benefit Requirements~~General Requirements~~
- 7-0202 Design Criteria
- 7-0203 Information to be Provided on Plans
- ~~7-0204~~ Trip Generation
- 7-020~~5~~3 Traffic Flow Characteristics
- 7-020~~6~~4 Traffic Counts
- 7-020~~7~~5 Street Cross-Section Determination

Amend Section 7-0101, where insertions are shown as underlines and deletions are shown as strike-outs, to read as follows:

7-0101 Street Design

7-0101.1 Streets shall be provided to give access to adjoining property to the satisfaction of the Director. Also, streets shall be provided to connect with appropriate highways and with appropriate streets to adjoining developments.

7-0101.2 Public streets shall meet the requirements set forth in the current VDOT Secondary Street Acceptance Requirements regarding public benefit (24 VAC 30-92-60). These requirements are based on area type and include public service, pedestrian accommodation, and connectivity requirements.

7-0101.~~3~~2 (99-07-PFM) All rights-of-way shall conform to the standards set forth in the current VDOT ~~Subdivision~~ Secondary Street Acceptance Requirements and Plates 1-7 (1M-7) through 11-7 (11M-7), and § 7-0406.14E. Context-sensitive urban road design standards, including but not limited to minimum street width and parking, that are established by a Memorandum of Understanding (MOU) between Fairfax County and VDOT may deviate from the requirements set forth in the VDOT Road Design Manual and the PFM, and shall be used only in areas specified in the MOU. All street construction shall be within the dedicated street right-of-way.

7-0101.3A Street widths. Streets widths shall accommodate parking on both sides of the street unless a restricted parking condition and/or narrower street width is specifically approved by the Board of Supervisors (Board) in conjunction with the approval of a rezoning, proffered condition amendment, special exception, or special exception amendment, and signed as approved by VDOT and the Director. Minimum street widths shall be in accordance with Appendix B(1) of the VDOT Road Design Manual except that the following minimums also must be met for streets with parking on both sides in order to provide space for emergency access and operations:

7-0101.3A(1) For ditch section streets with an ADT (average daily traffic count) of 5499 or less, the minimum total roadway width, including pavement and gravel shoulders, for both one-way and two-way streets shall be no less than 36 feet (11.0 meters).

7-0101.3A(2) For curb and gutter streets with an ADT of 5499 or less, the minimum street width (face of curb to face of curb) shall be no less than 36 feet (11.0 meters) for both one-way and two-way streets.

7-0101.4~~3~~ In subdivisions developed as R-C Clusters, 50' (15m) wide access easements only will be required to adjoining properties which are otherwise land locked by having access only by means of a right-of-way less than 50' (15m) wide. No street construction will be required within the required 50' (15m) wide access easement.

7-0101.5~~4~~ Subdivision blocks shall be spaced so as to provide reasonable traffic circulation within and between existing or anticipated subdivisions, except as limited above for R-C Cluster developments.

Amend Section 7-0102.1, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

7-0102.1 (99-07-PFM) The width of the right-of-way shall be established in accordance with the current VDOT ~~Subdivision~~ Secondary Street Acceptance Requirements and the current VDOT Road Design Manual. The width shall account for all features, which are to be maintained by VDOT. All street construction shall be within the dedicated right-of-way. Easements shall not be accepted to make up the minimum required right-of-way if any construction is proposed thereon. Slope construction easements shall be provided where required.

Amend Section 7-0103, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

7-0103 Curb & Gutter (See Plates 2-7 (2M-7), 3-7 (3M-7), 4-7 (4M-7), 7-7 (7M-7), 9-7 (9M-7), 11-7 (11M-7), 12-7 (12M-7))

7-0103.1 Curb and gutter shall be installed ~~on~~ along the side of arterial, collector and local streets which provide frontage to lots within new subdivisions in which the average lot size is less than 18,000 ft² (1672 m²).

7-0103.2 Header curbs shall be installed for raised medians and service drives for proper channelization of traffic as deemed necessary by the Director

7-0103.3 (35-91-PFM) Curb cuts for commercial and industrial use entrances shall be in accordance with ~~the latest edition of the current VDOT~~ Access Management Regulations. Publication "Minimum Standards of Entrances to State Highways".

7-0103.4 (35-91-PFM) Curb cuts for residential use entrances shall be located so as to provide a safe and convenient means of ingress and egress for motor vehicles to and from paved or otherwise improved parts of highways and streets, except that no curb cut on public streets shall be less than 30' (9m) from the point of curvature of the curb line/edge of pavement return of the intersecting streets, unless otherwise approved by the Director based upon the volume of traffic, angle of intersection, potential of expanded use in the future, conflicting turning movements, stacking and sight distance.

Amend Section 7-0200, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

7-0200 PRELIMINARY STREET PLANNING

7-0201 **Public Benefit Requirements** All public street networks and individual public streets must be designed to meet the public benefit requirements established in the current VDOT Secondary Street Acceptance Requirements. These requirements are based on area type and include public service, pedestrian accommodation and connectivity requirements. The terms, area type, connectivity index and network addition, are defined in the current VDOT Secondary Street Acceptance Requirements.

~~7-0202~~ **Design Criteria General Requirements.** (99-07-PFM, 89-05-PFM, 62-98-PFM) All streets, which are to be dedicated for public use, shall be designed to comply with the applicable design criteria ~~in geometric standard in accordance with~~ the current VDOT Road Design Manual and Plates 1-7(1M-7) through 3-7(3M-7).

7-0203 Information to be Provided on Plans

7-0203.1 The following information shall be provided on all plans, including preliminary plats, that propose public streets to show compliance with the public benefit requirements of the current VDOT Secondary Street Acceptance Requirements.

7-0203.1A The location and configuration, including terminus, of each street and the traffic volume anticipated when the land served is fully developed in accordance with the land use proposed within the development. The anticipated traffic volume shall include traffic flow that may be generated from adjoining properties with connecting streets or stub outs. For the purposes of the traffic volume calculations, the land use on the adjoining property shall be as designated on the Comprehensive Plan, existing zoning, or existing conditions, whichever generates the largest traffic volume.

7-0203.1B The location, area and density or floor area ratio (FAR) of each type of proposed land use within the development.

7-0203.1C The location of any proposed transportation facility including any public transportation facilities as well as bicycle and pedestrian accommodations within the development's boundary, and the median lot size used to determine the required pedestrian accommodations, if applicable.

7-0203.1D The proposed functional classification for each street in the development.

7-0203.1E The area type and connectivity index of the proposed network addition.

7-0203.1F The location of stub outs on adjoining property and the existing land use, existing zoning as well as the land use designated on the Comprehensive Plan for the adjoining property.

7-0203.1F(1) The area and type of land use from the adjoining property that will generate traffic on the connecting street; and

7-0203.F(2) The total number of vehicles per day expected to both enter and exit the development over the connecting street.

7-0203.1G The location of stub outs within the development.

7-0203.1G(1) The size of the adjoining property and the type of land use shown on the Comprehensive Plan for the adjoining property.

7-0203.1G(2) The total number of vehicles per day expected to both enter and exit the development on the connecting street once the adjoining property is developed.

7-0203.1H Any reductions in the connectivity requirements pursuant to 24 VAC 30-92-60-5a and the associated calculations. These reductions do not require a formal request for approval by VDOT.

7-0203.1I Any reductions, modifications or exceptions to the public service or connectivity requirements that require a formal request for approval by VDOT, and the associated VDOT approval letter.

7-0203.1J The location and type of stormwater facilities that are proposed to be located in the right-of-way.

7-0203.1K Any information on the proposed phasing of street construction, if applicable.

7-0203.21.1 In order to determine the proper street cross-section to use to facilitate review and approval of preliminary plats, the following information shall be provided for each street intersection:

7-0203.21.1A The number of vehicles per day entering and leaving the intersection shall be noted on each leg of each street in each direction.

7-0203.21.1B The proposed street right-of-way, together with the proposed width of street (face of curb to face of curb, or edge of pavement to edge of pavement and shoulder width) for each block on every street in the subdivision, is to be shown.

~~7-0201.1C All street construction shall be within the dedicated street right of way. Easements shall not be accepted to make up the minimum required right of way if any construction is proposed thereon. Slope construction easements shall be provided where required.~~

~~7-0201.2 The following information shall be shown for all streets which intersect the exterior boundary of the subdivision and which will provide access to adjoining undeveloped property:~~

~~7-0201.2A Number of acres (hectares) expected to contribute vehicles to this street;~~

~~7-0201.2B An indication of how the adjoining property is shown on the adopted comprehensive plan together with the number of units per acre residential density proposed;~~

~~7-0201.2C The total number of units expected to be contributing to the subject street; and~~

~~7-0201.2D The total vehicles per day expected to be using the street.~~

~~7-0201.3 For streets which intersect the exterior boundary of the subdivision and connect with existing, dedicated or proposed streets in adjoining subdivisions, the following shall be required:~~

~~7-0201.3A The number of lots from the adjoining subdivision from which vehicles will be expected to use the subject street; and~~

~~7-0201.3B~~ The number of vehicles expected to enter the subdivision over the subject street from said lots.

7-02042 Trip Generation

7-02042.1 (89-05-PFM, 62-98-PFM) Trip Generation shall be calculated in accordance with the current VDOT Road Design Manual ~~Appendix B~~.

7-02053 Traffic Flow Characteristics

7-02053.1 In the determination of traffic flow calculations, all preliminary plats shall show traffic divides, in a manner similar to the way in which drainage divides are currently shown, for the traffic which will be generated by the subject development and from the adjoining development passing through it or from adjoining undeveloped property which will ultimately be subdivided with traffic passing through the subject subdivision.

7-02053.2 The following items shall be considered in the calculations of the traffic volumes where appropriate:

7-02053.2A Will any of the proposed streets serve as principal or secondary access to any school, either existing or proposed?

7-02053.2B Will any of the streets provide access to existing or proposed parks?

7-02053.2C Will any of the streets provide access to a neighborhood or other shopping area?

7-02053.2D Will any of the streets provide access to a public or semi-public facility or institutional use, such as church, nursing home, lodge hall, community swimming pool, private school, fire station, library, etc?

7-02053.2E Could any street in the subdivision serve as a bypass or shortcut for traffic with both origin and destination outside of the subject subdivision or the adjacent subdivision or undeveloped property?

7-02053.3 An allowance for additional traffic volume shall be included when any of the above traffic generators could affect the proposed subdivision streets or which reasonably may be expected to at some future date.

7-02064 Traffic Counts

7-02064.1 (47-95-PFM) Peak hour traffic estimates shall be used for intersection analysis. Twenty four hour ADT counts shall be used to determine typical sections.

7-02064.2 (99-07-PFM) Since this is a relatively new area of study and rapidly changing as new research data are compiled, the figures contained herein for traffic estimates may change as new data are made available.

7-02064.3 More precise information will be sought for the traffic generation of other impact uses.

7-02075 Street Cross-Section Determination

7-02075.1 When traffic volume falls off to indicate a reduced cross-section in the middle of the block, no reduction shall be permitted in midblock.

7-02075.2 Cross-sections may be reduced at intersections and at entrances contributing high volumes of traffic, such as shopping centers and apartments.

7-02075.3 If a through street has a wide cross-section on both ends and traffic volumes would indicate a reduction for only several hundred feet in the middle of the development, the full width street section shall be continued for its entire length.

7-02075.4 Averaging of traffic volumes (e.g. averaging 6,000 VPD on one end of the street with 2,000 VPD on the other end to produce a 4,000 VPD volume and thus a lower cross-section) shall not be permitted for the purpose of reducing cross-sections.

7-02075.5 All calculations which indicate the number of vehicles per day for each portion of each street in the subdivision shall be shown to expedite the review for conformance of proposed typical sections with VDOT standards. Lacking this information, it shall be assumed that no estimates have been made and the plan shall be returned for inclusion of traffic data.

Amend Section 7-0401.1A , where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

7-0401.1A (89-05-PFM) All single-family residential and all multi-family housing developments shall provide standard curb-cut ramps located to conform to VDOT standards ~~or County standards~~. (IIM-LD-55.7 or subsequent revisions), if in the right-of-way, or Americans with Disabilities Act Accessibility Guidelines (ADAAG) and VUSBC. Consideration shall be given to curb cut ramps when establishing right-of-way widths.

Amend Section 7-0402.2F, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

7-0402.2F (99-07-PFM, 89-05-PFM) Sidewalk location in relation to guardrail shall be in accordance with the current Appendix B(1) of the VDOT Road Design Manual.

Amend Section 7-0405.1B, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

7-0405.1B (89-05-PFM) Roads within subdivisions, including commercial entrances and pipestem driveways or common driveways that serve more than two properties, must meet the intersection sight distance requirements and stopping sight distance requirements.

Amend Section 7-0405.5, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

7-0405.5 (89-05-PFM) ~~If the design speed is unknown, it shall be assumed to be 5 MPH (10KPH) above the posted speed limit.~~ The design speed shall be in accordance with VDOT Secondary Street Acceptance Requirements, VDOT Access Management Regulations, and the VDOT Road Design Manual.

Amend Section 7-0406.14D, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

7-0406.14D (72-01-PFM, 62-98-PFM) A sidewalk must be provided as required by § 101-2-2 of the Subdivision Ordinance and Zoning Ordinance, § 17-201. Further, sidewalks shall be constructed in accordance with the PFM. VDOT will accept maintenance in accordance with their current Secondary Subdivision Street Acceptance Requirements. All ~~P~~ proposed sidewalks must be constructed in accordance with UD-3 standards.

Amend Section 7-0406.14E, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

7-0406.14E (99-07-PFM, 89-05-PFM, 72-01-PFM) When a trail is shown on the adopted comprehensive plan in the right-of-way, the right-of-way width shall be wide enough to accommodate the trail as shown on the adopted comprehensive plan or a ~~trail-shared use path~~ path which is constructed to VDOT standards, whichever requires more width. Right-of-way width shall be determined in accordance with the current VDOT Road Design Manual. If the trail is adjacent to slopes greater than 3:1, additional right-of-way ~~may be needed~~ shall be provided to accommodate wider trail shoulders and safety features such as hand-rail.

Amend Section 7-0502.4A, where insertions are shown as underlines, to read as follows:

7-0502.4A (99-07-PFM, 89-05-PFM) The geometric design shall be in accordance with the current VDOT Road Design Manual for curb and gutter section streets and Plates 2-7 (2M-7) and 3-7(3M-7).

Amend Section 7-0701, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

7-0701 General Acceptance Policies

7-701.1 (62-98-PFM) VDOT policy on the acceptance of subdivision streets shall be in accordance with the current VDOT Secondary Subdivision Street Acceptance Requirements.

Amend Section 7-0702, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

7-0702 Stub Streets

~~7-0702.1 Stub streets of a lot depth in length will be accepted into the State Secondary Highway System under the following conditions:~~

~~7-0702.1A There is no entrance access from the contiguous lots to the stub streets.~~

~~7-0702.1B The approved preliminary plat of the subdivision delineates the extension of the stub street on future sections of the subdivision.~~

~~7-0702.1B(1) In this instance the construction plan shall note that the street will be extended as a part of subsequent construction plans yet to be submitted, or~~

~~7-0702.1B(2) A preliminary plat of an adjacent parcel has been submitted which shows a connection to the stub street, or~~

~~7-0702.1B(3) The stub street is the sole access to an adjacent parcel, or~~

~~7-0702.1B(4) The stub street is a portion of a future street as delineated on an adopted Comprehensive Plan, or~~

~~7-0702.1B(5) Stub streets, requested by the Director to facilitate future traffic circulation between a subdivision being developed and an adjoining undeveloped property when it eventually becomes subdivided, and which have the concurrence of VDOT, are acceptable.~~

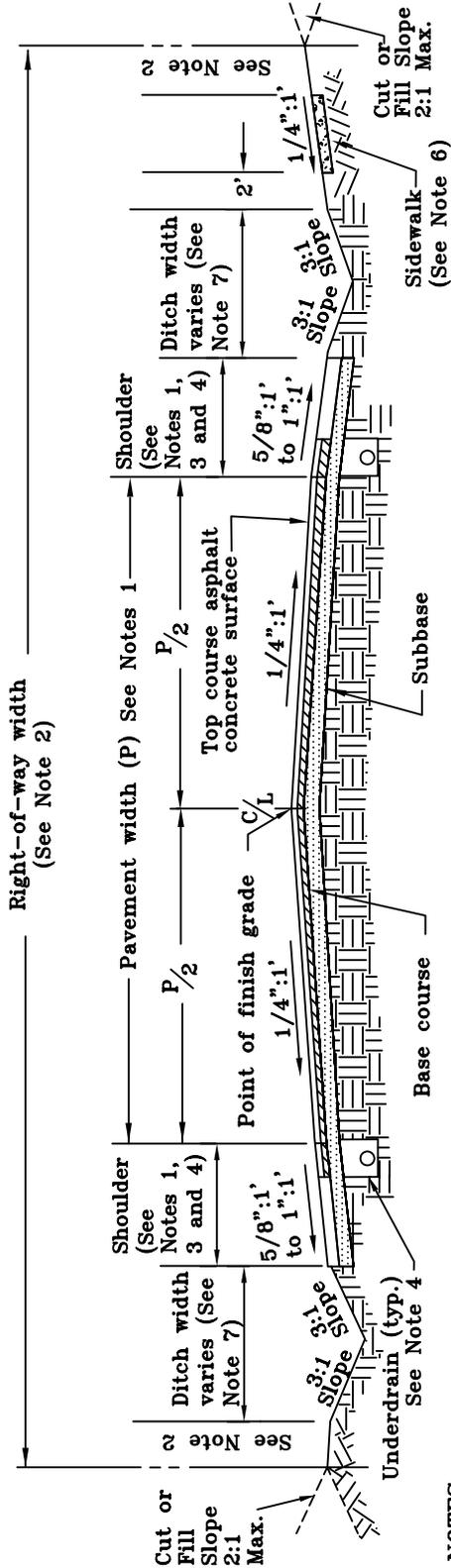
~~7-0702.1 Stub streets shall be located, accounting for on-site and off-site topography, 7-0702.1C~~
~~———— Adequate on site and off site topography is provided~~ to ensure that the stub streets can be extended to meet the current standards for street construction. Temporary construction and grading easements shall be provided to facilitate the future connection of the street.

7-0702.2 ~~Short dead end~~ Stub streets that terminate at the subdivision boundary which are proposed for future extension beyond the subdivision boundary, and serve 1 or more lots, will be provided with a temporary cul-de-sac, constructed in a temporary easement, located either on-site or off-site. Funds shall be deposited with the Director for removal of the temporary an on site cul-de-sac when the street is extended.

Amend Plates 1-7, 1M-7, 2-7, 2M-7, 3-7 and 3M-7 as follows:

FAIRFAX COUNTY PUBLIC FACILITIES MANUAL

FOR USE IN SUBDIVISIONS IN WHICH THE AVERAGE LOT SIZE IS 18,000 SF OR GREATER ON STREETS THAT HAVE AN ADT UP TO 5499



NOTES

- 1) Streets shall be designed in accordance with the VDOT Road Design Manual, except that the total roadway width, including the pavement and shoulders, for both one-way and two-way streets with parking on both sides shall be no less than 36 feet. The street widths shall accommodate parking on both sides of the street unless a no parking condition and/or a narrower roadway width is approved by the Board of Supervisors (Board) in conjunction with the approval of a rezoning, proffered condition amendment, special exception, or special exception amendment, and signed as approved by VDOT and the Director.
- 2) The required right-of-way width shall be based on containing the features to be maintained by VDOT within the right-of-way. Distances between features, and distances between features and the right-of-way line shall be in accordance with the current Appendix B(1) of the VDOT Road Design Manual. VDOT will accept sidewalk, which is a minimum of 1 foot from the right-of-way line.
- 3) Surface, base and subbase designs shall be predicated upon a subgrade CBR of 10 or greater and shall be designed in accordance with the current VDOT Pavement Design Guide for Subdivision and Secondary Roads in Virginia. Shoulders shall have a thickness equivalent to the adjoining pavement and subbase to provide proper drainage.
- 4) Underdrains and combination underdrains shall be provided in accordance with the current VDOT Road and Bridge standards. Underdrains are required on streets with an ADT of 1000 or greater. The section of the shoulder over the underdrain shall be paved.
- 5) Grades of relatively short length (up to 295'), which are on a street with a projected traffic volume up to 400 ADT, may be increased by 50% of the value upon approval by the Director and VDOT.
- 6) Sidewalk shall be provided in accordance with Section 8-0100 et seq. of the Public Facilities Manual and shall meet the standards set forth in the current Appendix B(1) of the VDOT Road Design Manual. Sidewalk cross slope, including passing areas (full 5-foot width) and driveway crossings (3-foot width), should not exceed 2%. The sidewalk shall be a minimum of 7' from the edge of the shoulder, if no physical barrier is provided. Sidewalks along ditch section streets shall be located on compacted subgrade and include underdrains in accordance with VDOT UD-3. Careful consideration shall be given to drainage and sidewalk/street intersections early in the design.
- 7) The ditch details (e.g. width, depth, and slope) shall be determined in accordance with Section 6 of the Public Facilities Manual and Appendix B(1) of the VDOT Road Design Manual. Ditches may not be required on fill sections with no sidewalk, if adequate drainage facilities are located out of the right-of-way. The width of the ditch must be considered when determining the appropriate right-of-way width.

REVISED 1-25-11

Ref. Plate 22-7
 Sec. 7-0101,
 7-0406.7B, 7-0502.3A

Rev. XX-XX

STANDARD TYPICAL SECTION FOR TWO-LANE STREETS WITHOUT CURB & GUTTER (DITCH SECTION STREET)

PLATE NO.

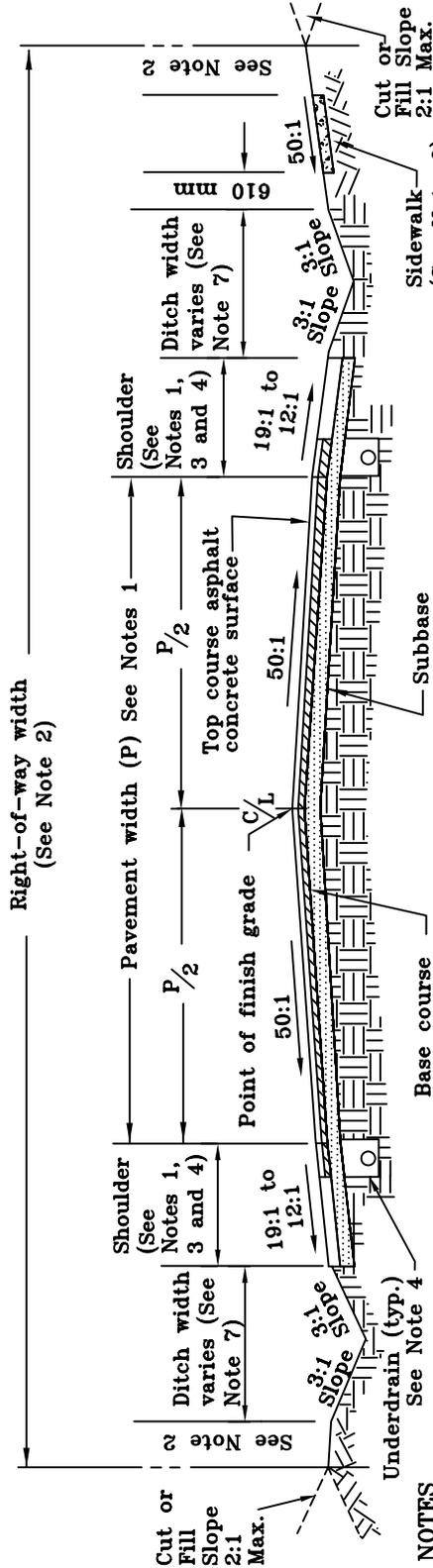
1-7

STD. NO.

TS-1

FAIRFAX COUNTY PUBLIC FACILITIES MANUAL

**FOR USE IN SUBDIVISIONS IN WHICH THE AVERAGE LOT SIZE IS 1,672 SQUARE METERS
OR GREATER ON STREETS THAT HAVE AN ADT UP TO 5499**



NOTES

- 1) Streets shall be designed in accordance with the VDOT Road Design Manual, except that the total roadway width, including the pavement and shoulders, for both one-way and two-way streets with parking on both sides shall be no less than 11 meters. The street widths shall accommodate parking on both sides of the street unless a no parking condition and/or a narrower roadway width is approved by the Board of Supervisors (Board) in conjunction with the approval of a rezoning, proffered condition amendment, special exception, or special exception amendment, and signed as approved by VDOT and the Director.
- 2) The required right-of-way width shall be based on containing the features to be maintained by VDOT within the right-of-way. Distances between features, and distances between features and the right-of-way line shall be in accordance with the current Appendix B(1) of the VDOT Road Design Manual. VDOT will accept sidewalk, which is a minimum of 305 mm from the right-of-way line.
- 3) Surface, base and subbase designs shall be predicated upon a subgrade CBR of 10 or greater and shall be designed in accordance with the current VDOT Pavement Design Guide for Subdivision and Secondary Roads in Virginia. Shoulders shall have a thickness equivalent to the adjoining pavement and subbase to provide proper drainage.
- 4) Underdrains and combination underdrains shall be provided in accordance with the current VDOT Road and Bridge standards. Underdrains are required on streets with an ADT of 1000 or greater. The section of the shoulder over the underdrain shall be paved.
- 5) Grades of relatively short length (up to 89.9 meters), which are on a street with a projected traffic volume up to 400 ADT, may be increased by 50% of the value upon approval by the Director and VDOT.
- 6) Sidewalk shall be provided in accordance with Section 8-0100 et seq. of the Public Facilities Manual and shall meet the standards set forth in the current Appendix B(1) of the VDOT Road Design Manual. Sidewalk cross slope, including passing areas (full 1.6-meter width) and driveway crossings (1-meter width), should not exceed 2%. The sidewalk shall be a minimum of 2.2 meters from the edge of the shoulder, if no physical barrier is provided. Sidewalks along ditch section streets shall be located on compacted subgrade and include underdrains in accordance with VDOT UD-3. Careful consideration shall be given to drainage and sidewalk/street intersections early in the design.
- 7) The ditch details (e.g. width, depth, and slope) shall be determined in accordance with Section 6 of the Public Facilities Manual and Appendix B(1) of the VDOT Road Design Manual. Ditches may not be required on fill sections with no sidewalk, if adequate drainage facilities are located out of the right-of-way. The width of the ditch must be considered when determining the appropriate right-of-way width.

Note: All units are in millimeters unless otherwise noted.

REVISED 1-25-11

Ref. Plate 22-7
Sec. 7-0101,
7-0406.7B, 7-0502.3A

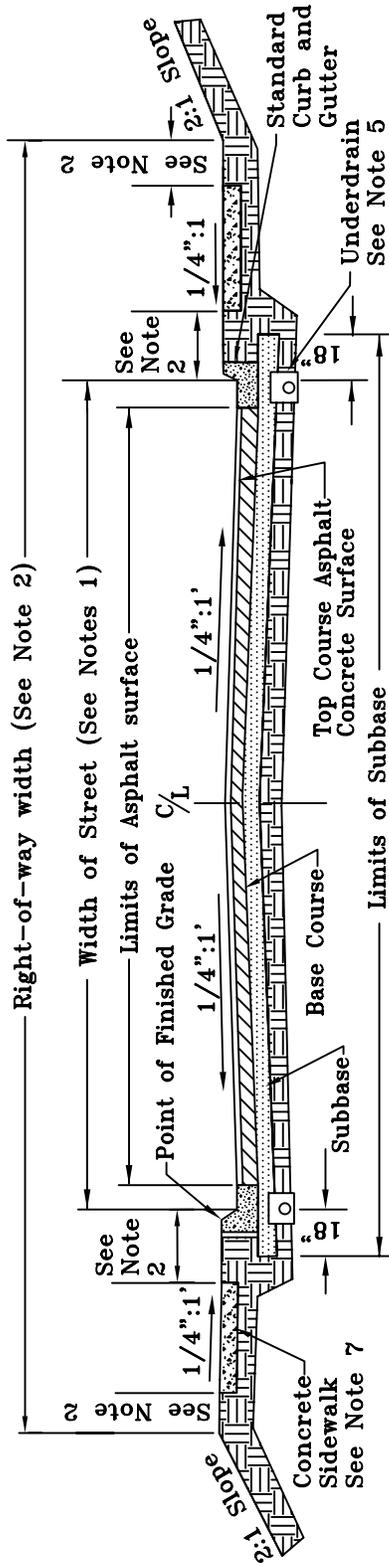
Rev. XX-XX

STANDARD TYPICAL SECTION FOR TWO-LANE STREETS WITHOUT CURB & GUTTER (DITCH SECTION STREET)

PLATE NO.	STD. NO.
1M-7	TS-1

FAIRFAX COUNTY PUBLIC FACILITIES MANUAL

FOR USE IN SUBDIVISIONS IN WHICH THE AVERAGE LOT SIZE IS
LESS THAN 18,000 SF ON STREETS THAT HAVE AN ADT UP TO 5499



NOTES

- 1) Streets shall be designed in accordance with VDOT Road Design Manual, except that the minimum street width shall be no less than 36 feet for both one-way and two-way streets with parking on both sides. Street widths shall accommodate parking on both sides of the street unless a no parking condition and/or a narrower street is approved by the Board of Supervisors (Board) in conjunction with the approval of a rezoning, proffered condition amendment, special exception, or special exception amendment, and signed as approved by VDOT and the Director.
- 2) The required right-of-way width shall be based on containing the features to be maintained by VDOT within the right-of-way. Distances between features, and distances between features and the right-of-way line shall be in accordance with the current Appendix B(1) of the VDOT Road Design Manual. VDOT will accept sidewalk, which is a minimum of 1 foot from the right-of-way line.
- 3) Surface, base and subbase designs shall be predicated upon a subgrade CBR of 10 or greater and shall be designed in accordance with the current VDOT Pavement Design Guide for Subdivision and Secondary Roads in Virginia.
- 4) Stone base or subbase material shall extend under the curb and gutter a minimum distance of 18" from the face of curb. The stone thickness under the curb and gutter shall be the pavement depth in excess of the 7" depth of the gutter face but no less than 4".
- 5) Underdrains and combination underdrains shall be provided in accordance with the current VDOT Road and Bridge Standards. Underdrains are required on streets with an ADT of 1000 or greater.
- 6) Grades of relatively short length (up to 295 feet), which are on a street with a projected traffic volume up to 400 ADT, may be increased by 50% of the value upon approval by the Director and VDOT.
- 7) Sidewalk shall be provided in accordance with Section 8-0100 et seq. of the Public Facilities Manual and shall meet the standards set forth in the current Appendix B(1) of the VDOT Road Design Manual. Sidewalk cross slope, including passing areas (full 5-foot width) and driveway crossings (3-foot width), should not exceed 2%.

REVISED 1-25-11

Ref. Sec. 7-0101,
7-0405.1D, 7-0406.7B
7-0502.4A, 7-0502.4B

Rev. XX-XX

STANDARD TYPICAL SECTION FOR UNDIVIDED STREETS WITH CURB & GUTTER

PLATE NO.

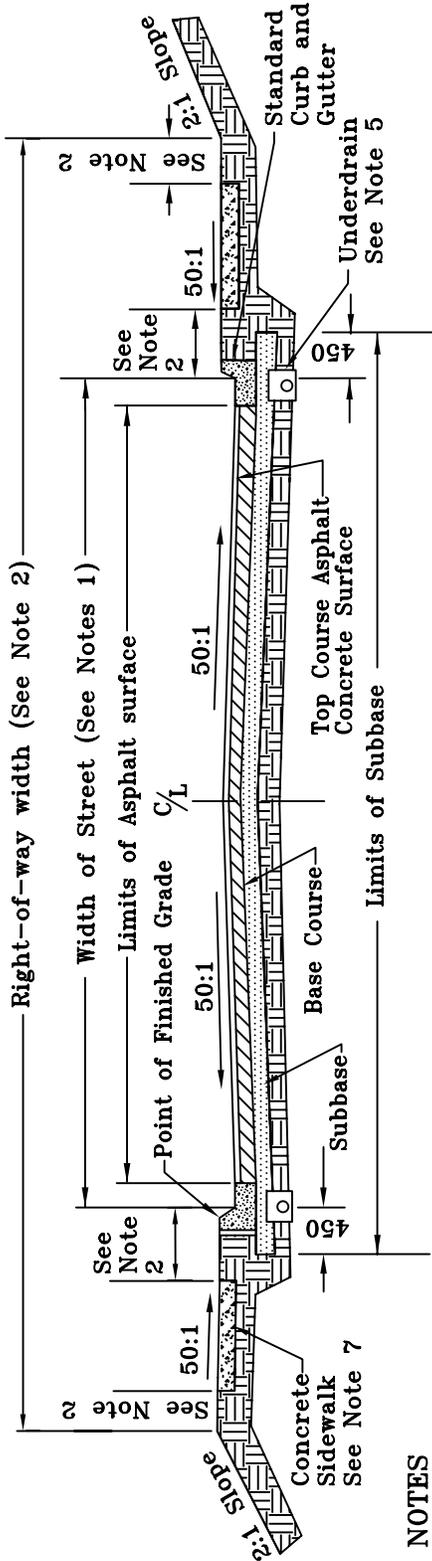
STD. NO.

2-7

TS-2

FAIRFAX COUNTY PUBLIC FACILITIES MANUAL

FOR USE IN SUBDIVISIONS IN WHICH THE AVERAGE LOT SIZE IS LESS THAN 1.672 SQUARE METERS ON STREETS THAT HAVE AN ADT UP TO 5499



NOTES

- 1) Streets shall be designed in accordance with VDOT Road Design Manual, except that the minimum street width shall be no less than 11 meters for both one-way and two-way streets with parking on both sides. Street widths shall accommodate parking on both sides of the street unless a no parking condition and/or a narrower street is approved by the Board of Supervisors (Board) in conjunction with the approval of a rezoning, proffered condition amendment, special exception, or special exception amendment, and signed as approved by VDOT and the Director.
- 2) The required right-of-way width shall be based on containing the features to be maintained by VDOT within the right-of-way. Distances between features, and distances between features and the right-of-way line shall be in accordance with the current Appendix B(1) of the VDOT Road Design Manual. VDOT will accept sidewalk, which is a minimum of 305 mm from the right-of-way line.
- 3) Surface, base and subbase designs shall be predicated upon a subgrade CBR of 10 or greater and shall be designed in accordance with the current VDOT Pavement Design Guide for Subdivision and Secondary Roads in Virginia.
- 4) Stone base or subbase material shall extend under the curb and gutter a minimum distance of 450 mm from the face of curb. The stone thickness under the curb and gutter shall be the pavement depth in excess of the 175 mm depth of the gutter face but no less than 100 mm.
- 5) Underdrains and combination underdrains shall be provided in accordance with the current VDOT Road and Bridge Standards. Underdrains are required on streets with an ADT of 1000 or greater.
- 6) Grades of relatively short length (up to 89.9 meters), which are on a street with a projected traffic volume up to 400 ADT, may be increased by 50% of the value upon approval by the Director and VDOT.
- 7) Sidewalk shall be provided in accordance with Section 8-0100 et seq. of the Public Facilities Manual and shall meet the standards set forth in the current Appendix B(1) of the VDOT Road Design Manual. Sidewalk cross slope, including passing areas (full 1.6-meter width) and driveway crossings (1-meter width), should not exceed 2%.

Note: All units are in millimeters unless otherwise noted.

REVISED 1-25-11

Ref. Sec. 7-0101,
7-0405.1D, 7-0406.7B
7-0502.4A, 7-0502.4B

Rev. XX-XX

STANDARD TYPICAL SECTION FOR UNDIVIDED STREETS WITH CURB & GUTTER

PLATE NO.

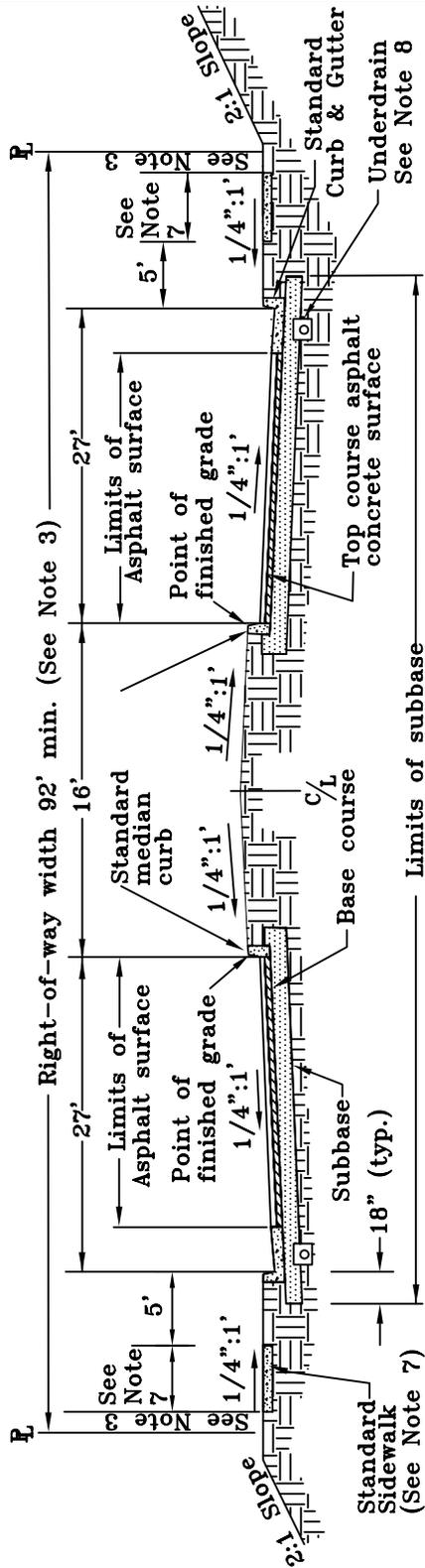
STD. NO.

2M-7

TS-2

FAIRFAX COUNTY PUBLIC FACILITIES MANUAL

FOR USE ON STREETS THAT HAVE AN ADT OF 5500 TO 8000



NOTES

- 1) Street shall be superelevated in accordance with Standard TC-5 of VDOT Road and Bridge Standards.
- 2) Sight distance, centerline radius, clear zone, grade, K values for sag and crest, distance between features, and distance between features and right-of-way line shall be in accordance with the current VDOT Road Design Manual based on urban road criteria and a 45 mph design speed.
- 3) The right-of-way width shown is based on the features shown, which are to be maintained by VDOT. If additional features such as guard rail are required, the right-of-way width may change and shall accommodate all features, which are to be maintained by VDOT, in accordance with the current VDOT Road Design Manual. VDOT will accept sidewalk, which is a minimum of 1 foot from the right-of-way line.
- 4) Surface, base and subbase designs shall be predicated upon a subgrade CBR of 10 or greater and shall be designed in accordance with current VDOT Pavement Design Guide for Subdivision and Secondary Roads in Virginia.
- 5) Stone subbase material shall extend under the curb and gutter a minimum distance of 18" from the face of curb. The stone thickness under the curb and gutter shall be the pavement depth in excess of the 7" depth of the gutter face but no less than 4".
- 6) 12'-wide left turn lanes shall be provided at all median crossovers. Any portion of the raised median that becomes less than 6' wide shall be constructed in concrete.
- 7) Sidewalk shall be provided in accordance with Section 8-0100 et seq. Width of sidewalk and distance between sidewalk and the back of curb shall be in accordance with the current VDOT Road Design Manual.
- 8) Underdrains and combination underdrains shall be provided in accordance with the current VDOT Road and Bridge Standards.
- 9) Streets with an ADT in excess of 8000 ADT shall be designed in accordance with VDOT Road Design Manual.

REVISED 1-25-11

Ref. Sec. 7-0101,
7-0405.1C, 7-0405.2,
7-0405.3A(2), 7-0406.7B

Rev. XX-XX

STANDARD TYPICAL SECTION FOR MULTI-LANE STREETS WITH CURB & GUTTER

PLATE NO.

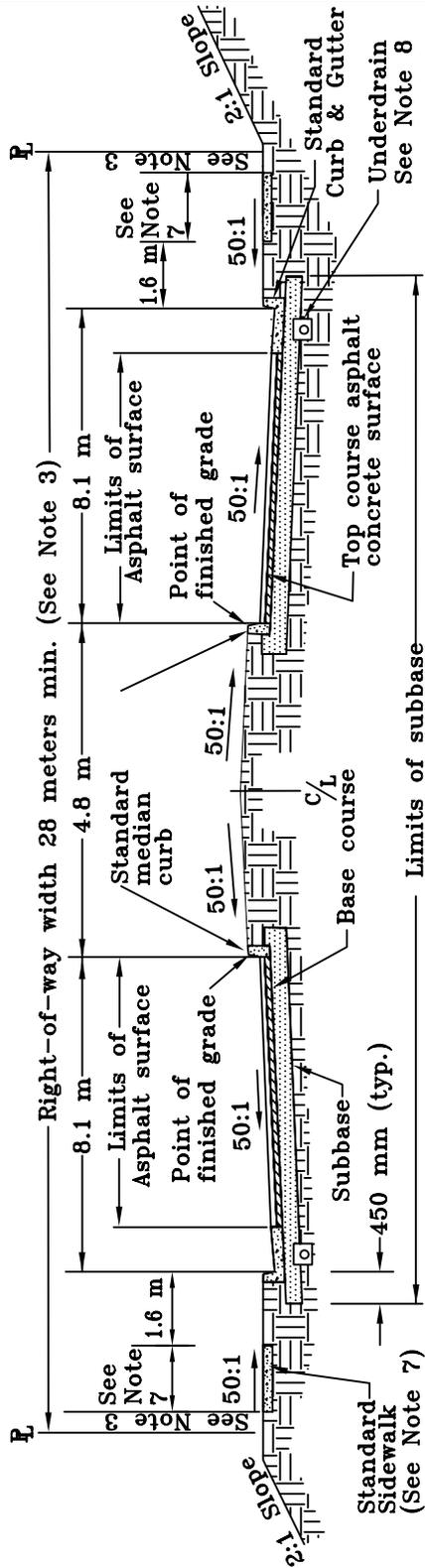
3-7

STD. NO.

TS-2

FAIRFAX COUNTY PUBLIC FACILITIES MANUAL

FOR USE ON STREETS THAT HAVE AN ADT OF 5500 TO 8000



NOTES

- 1) Street shall be superelevated in accordance with Standard TC-5 of VDOT Road and Bridge Standards.
- 2) Sight distance, centerline radius, clear zone, grade, K values for sag and crest, distance between features, and distance between features and right-of-way line shall be in accordance with the current VDOT Road Design Manual based on urban road criteria and a 70 km/h design speed.
- 3) The right-of-way width shown is based on the features shown, which are to be maintained by VDOT. If additional features such as guard rail are required, the right-of-way width may change and shall accommodate all features, which are to be maintained by VDOT, in accordance with the current VDOT Road Design Manual. VDOT will accept sidewalk, which is a minimum of 305 mm from the right-of-way line.
- 4) Surface, base and subbase designs shall be predicated upon a subgrade CBR of 10 or greater and shall be designed in accordance with current VDOT Pavement Design Guide for Subdivision and Secondary Roads in Virginia.
- 5) Stone subbase material shall extend under the curb and gutter a minimum distance of 450 mm from the face of curb. The stone thickness under the curb and gutter shall be the pavement depth in excess of the 175 mm depth of the gutter face but no less than 100 mm.
- 6) 3.6-meter wide left turn lanes shall be provided at all median crossovers. Any portion of the raised median that becomes less than 1.8-meter wide shall be constructed in concrete.
- 7) Sidewalk shall be provided in accordance with Section 8-0100 et seq. Width of sidewalk and distance between sidewalk and the back of curb shall be in accordance with the current VDOT Road Design Manual.
- 8) Underdrains and combination underdrains shall be provided in accordance with the current VDOT Road and Bridge Standards.
- 9) Streets with an ADT in excess of 8000 ADT shall be designed in accordance with VDOT Road Design Manual.

Note: All units are in millimeters unless otherwise noted.

REVISED 1-25-11

Ref. Sec. 7-0101,
7-0405.1C, 7-0405.2,
7-0405.3A(2), 7-0406.7B

Rev. XX-XX

STANDARD TYPICAL SECTION FOR MULTI-LANE STREETS WITH CURB & GUTTER

PLATE NO.

3M-7

STD. NO.

TS-2

AMENDMENTS TO CHAPTER 8 OF THE PUBLIC FACILITIES MANUAL

Amend Table of Contents Section 8-0100, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

8-0100 SIDEWALKS

8-0101 General Information

8-0102 Subdivisions ~~Containing Lots Averaging Less than 25,001 ft² (2323 m²)~~ with a Median Lot Size of One-Half Acre (2023 m²) or Less

8-0103 Subdivisions ~~Containing Lots Averaging 25,001 ft² (2323 m²) up to 52,000 ft² (4830 m²)~~ with a Median Lot Size Between One-Half Acre (2023 m²) and Two Acres (8094 m²)

8-0104 Subdivisions ~~Containing Lots Averaging 52,000 ft² (4831 m²)~~ with a Median Lot Size of Two Acres or Greater

8-0105 Subdivisions in Proximity of Elementary, Intermediate and High Schools

8-0106 Sidewalks along Arterial or Collector Streets

8-0107 Sidewalks in Developments Where the Floor Area Ratio (FAR) is 4.0 or greater

Amend Section 8-0102, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

8-0102 Subdivisions ~~Containing Lots Averaging Less than 25,001 ft² (2323 m²)~~ with a Median Lot Size of One-Half Acre (2023 m²) or Less. (100-07-PFM) (90-05-PFM) A sidewalk shall be constructed on both sides of all streets in these subdivisions including all reverse or side frontage lots and open space. When the peripheral boundary of the subdivision is contiguous to an existing or planned street, a sidewalk shall be constructed on the side of the street abutting the subdivision boundary (see § 8-0105).

Amend Section 8-0103, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

8-0103 Subdivisions ~~Containing Lots Averaging 25,001 ft² (2323 m²) up to 52,000 ft² (4830 m²)~~ with a Median Lot Size Between One-Half Acre (2023 m²) and Two Acres (8094 m²). (100-07-PFM) (90-05-PFM) Sidewalks shall be constructed on at least one side of all streets in these subdivisions including all reverse or side frontage lots and open space. When the peripheral boundary of the subdivision is contiguous to an existing or planned street, a sidewalk shall be constructed on the side of the street abutting the subdivision boundary (see § 8-0105).

Amend Section 8-0104, where insertions are shown as underlines and deletions are shown as strikeouts, to read as follows:

8-0104 Subdivisions ~~Containing Lots Averaging 52,000 ft² (4831 m²)~~ with a Median Lot Size of Two Acres or Greater. (100-07-PFM) (90-05-PFM) Sidewalks shall be constructed on at least one side of all streets in these subdivisions including all reverse or side frontage lots and open space and on all streets of the subdivision boundary when the subdivision is located in proximity to schools in accordance with § 8-0105.

Add Section 8-106 to read as follows:

8-0106 Sidewalks along Arterial or Collector Streets Sidewalks shall be constructed along streets that are functionally classified as arterials or collectors in accordance with the current VDOT Secondary Street Acceptance Requirements.

Add Section 8-107 to read as follows:

8-0107 Sidewalks in Developments Where the Floor Area Ratio (FAR) is 4.0 or greater If a development proposes a FAR of 4.0 or greater and proposes the construction of a public street, sidewalk shall be constructed on both sides of the street. If a development proposes a FAR of 4.0 or greater and abuts an existing public street, sidewalk shall be constructed on the side abutting the public street.

November 22, 2010

Fairfax County Board of Supervisors
12000 Government Center Parkway
Suite 533
Fairfax, VA 22035-0072

Re: ***Amendments to Public Facilities Manual Chapter 7***

Dear Board Member:

At its regularly scheduled April 8, 2010 meeting, the Fairfax County Engineering Standards Review Committee voted unanimously to recommend to the Board of Supervisors adoption of amendments to Chapter 7 of the PFM. The Committee was presented with two (2) options that were developed by staff over the course of nearly a year: "Option A", amends the PFM to use VDOT's street standards that were implemented in March 2009 as part of their new Subdivision Street Acceptance Requirements (SSAR) for all new public streets; and, "Option B", amends the PFM to use VDOT streets but with increased width of the proposed public streets where designated parking is provided on one or both sides. The Committee recommended that "Option A" be adopted; and, the County staff supported "Option B". It is fair to say that, since the introduction of this proposed amendment, the Committee has had many long discussions on their scope and content.

As background, VDOT's new requirements are comprehensive and include, but are not limited to, requirements for public street connectivity, minimizing impervious area by reducing roadway widths, reducing impervious area and reclassifying some streets. These changes were in response to legislation passed by the Virginia General Assembly and signed into Law by the Governor in 2007. Like most jurisdictions, after VDOT implemented these new standards, Fairfax County was then tasked with modifying their standards in response to VDOT's new policy so that new public streets in Fairfax County would meet VDOT's standards and be eligible for acceptance into the VDOT public street network.

When the Committee was initially presented with proposed amendments to Chapter 7 to coincide with VDOT's new SSAR requirements in the summer of 2009, the initial staff proposal provided for public street widths that, in many cases, were wider than the street widths adopted by VDOT. As part of the process of reviewing the changes to Chapter 7 proposed by staff, representatives from the Fire Marshal's made a presentation to the Committee wherein they outlined concerns as to their ability to adequately respond to fires and other related emergencies. Their primary complaint, as presented to the Committee, was that their access is impeded by streets where parking is permitted, thereby limiting the emergency access aisle. They stated that it took them longer to respond; and, once at the

scene of the emergency, their access to the fire or other incident was blocked, sometimes by illegally parked vehicles. They cited examples, accompanied by photos, that showed how their access was hampered by narrow streets and parking. The staff proposal reflected the concerns expressed by the Fire Marshal and allowed for wider public streets, especially those on which parking was permitted.

With respect to these changes, it was the consensus of the Committee that the overall goal was to have the new public street requirements in the PFM mirror the public street requirements in Appendix B(1) of VDOT's Road Design Manual, which achieve uniformity of public standards. And, that the street cross sections remain at a width that was reasonable for the corresponding trip generation and did not counter the numerous amendments that have been adopted over the years which sought to balance the impact of site developments. One of VDOT's specific goals of the SSAR was to reduce impervious area and stormwater runoff by reducing the street pavement sections; and, in the past, the Committee has been supportive of other changes to the PFM that would help to reduce the overall amount of impervious area resulting from development, as increased impervious area leads to more runoff, which in turn creates downstream drainage issues, among other items. One of the biggest differences in the Options is for ditch section streets. Under staff's option, the section increases to one standard with a pavement width of 29 ft. This represents a significant change especially in those parts of the County where a low volume ditch section street would be provided serving large lots with little potential for on-street parking. The Committee believes it counterproductive to adopt new public street requirements that are contrary to VDOT's standards and the goals of the enabling legislation. In response to questions from the Committee, staff presented information that both Loudoun and Prince William Counties revised their standards to adopt VDOT's standards for all new public streets and; therefore, the Committee's recommendation is consistent with other local jurisdictions. It is also noted that Arlington County and the City of Alexandria have similar, but different standards; however, except for certain streets, this is within their prerogative, as these two jurisdictions maintain their own secondary streets.

No one on the Committee questions the dedication and hard work by all representatives of the County's public safety and fire and rescue department or takes exceptions to their concerns. However nearly all of the examples presented by the Fire Marshal pertained to private streets associated with townhouses, multi-family and commercial development, not public streets; and, there were few examples presented where there was an access issue with a public street of similar impact. In general, Public streets are provided primarily in subdivisions and other types of development where sufficient off street parking is provided. For single family detached development, off street parking is required to be provided on the lot on which the primary structure is located; and, in those various non-residential developments where new public streets are provided, adequate off street parking and loading is required to be provided on the same lot as the principle use. Further, the County has the ability to modify their standards for private streets to reflect the concerns expressed by the Fire Marshal by amending the PFM standards for private streets, whether or not parking is permitted, so that adequate access by fire and rescue equipment and personnel is not impeded.

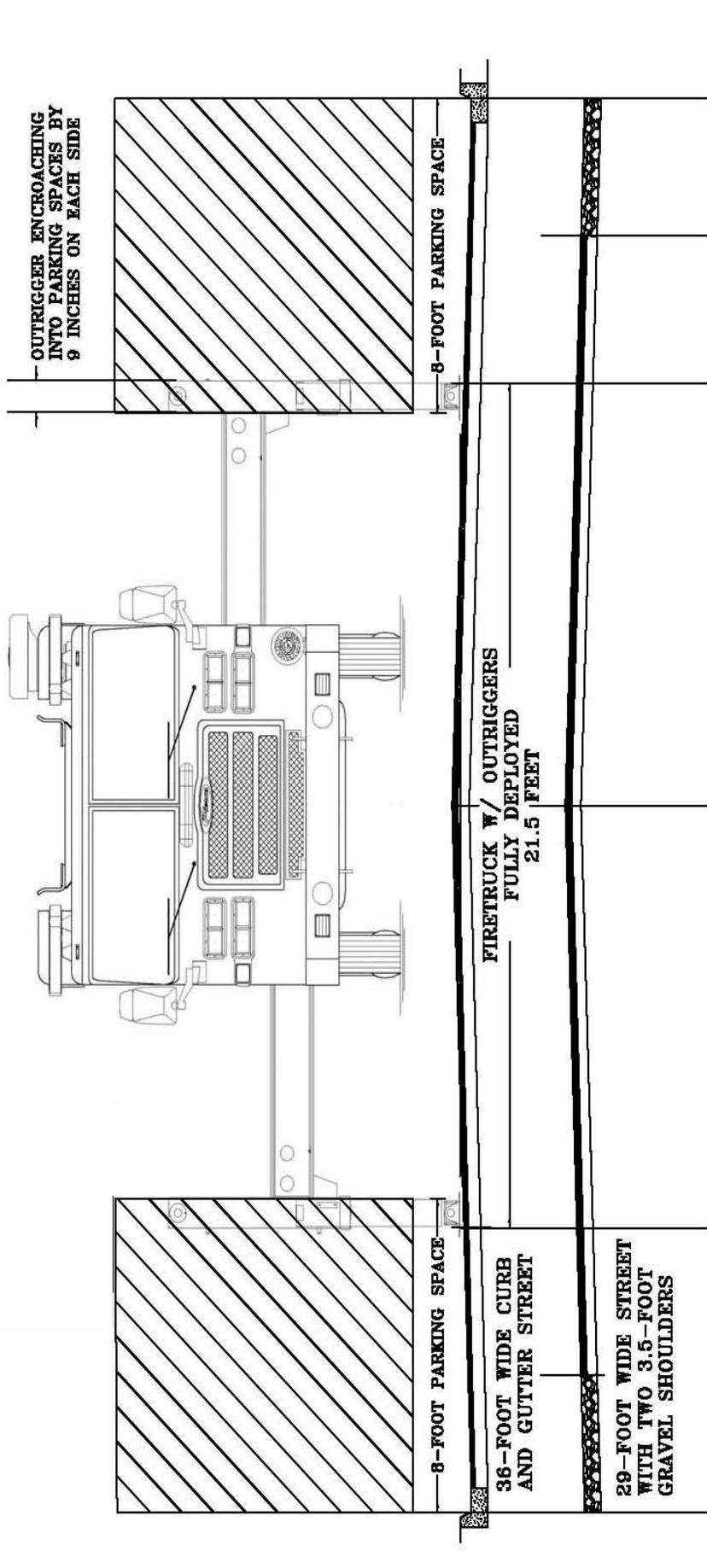
Summarizing, the principle difference is that VDOT's changes dealt solely with public streets; and, the Committee believes that uniformity between the County's and VDOT's standards for public streets is needed. The Committee does not believe that wider public street standards than those adopted by VDOT are warranted and believes that adopting wider standards would counter the many initiatives that have been adopted by the County over the years to balance development with the environment. Lastly, the Committee also believes that the County is well within their rights to promulgate standards for private streets that address the concerns expressed by fire and rescue. We would be happy to respond to any questions regarding this matter.

Respectfully submitted,

A handwritten signature in blue ink, appearing to be 'A. R.', enclosed within a large, loopy blue oval.

Chairman
Engineering Standards Review Committee

Cc: Judy Cronauer
Bijan Sistani
Engineering Standards Review Committee



Street cross-section with fire truck