



FAIRFAX COUNTY PLANNING COMMISSION

March 28, 2011

**PUBLIC HEARING
FOR
PLANNING DETERMINATION**

Pursuant to
Va. Code Sec. 15.2 - 2232

Public Hearing Date: April 28, 2011 at 8:15 p.m.

Application Number: **2232A-MD06-10-1**

Applicant: Metropolitan Washington Airports Authority in coordination with the Department of Rail and Public Transportation on behalf of Washington Metropolitan Area Transit Authority

Proposed Use: Sunset Hills Road & Dulles Airport Access Road Traction Power Substation (TPSS)

Supervisor District: Hunter Mill District

Subject Property: Tax Map 18-3 (VDOT Right-of-Way)

Size of Proposed Facility: Approximately 500 square feet

Application Accepted: March 4, 2011

Application Amended: March 21, 2011

Recommendation: In accordance with Va. Code Sec. 15.2-2232, as amended, staff recommends that the Planning Commission find the proposal by the Metropolitan Washington Airports Authority in coordination with the Department of Rail and Public Transportation, on behalf of Washington Metropolitan Area Transit Authority, as amended, to construct a Traction Power Substation (TPSS) south of Sunset Hills Road and west of Hunter Mill Road in the VDOT Right-of-Way, Reston, substantially in accord with provisions of the adopted Comprehensive Plan.

PLANNING DETERMINATION

Section 15.2 -2232 of the Code of Virginia



Number: 2232A-MD06-10-1

District: Hunter Mill

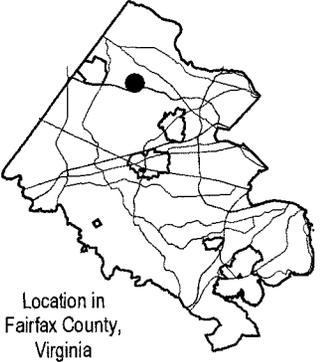
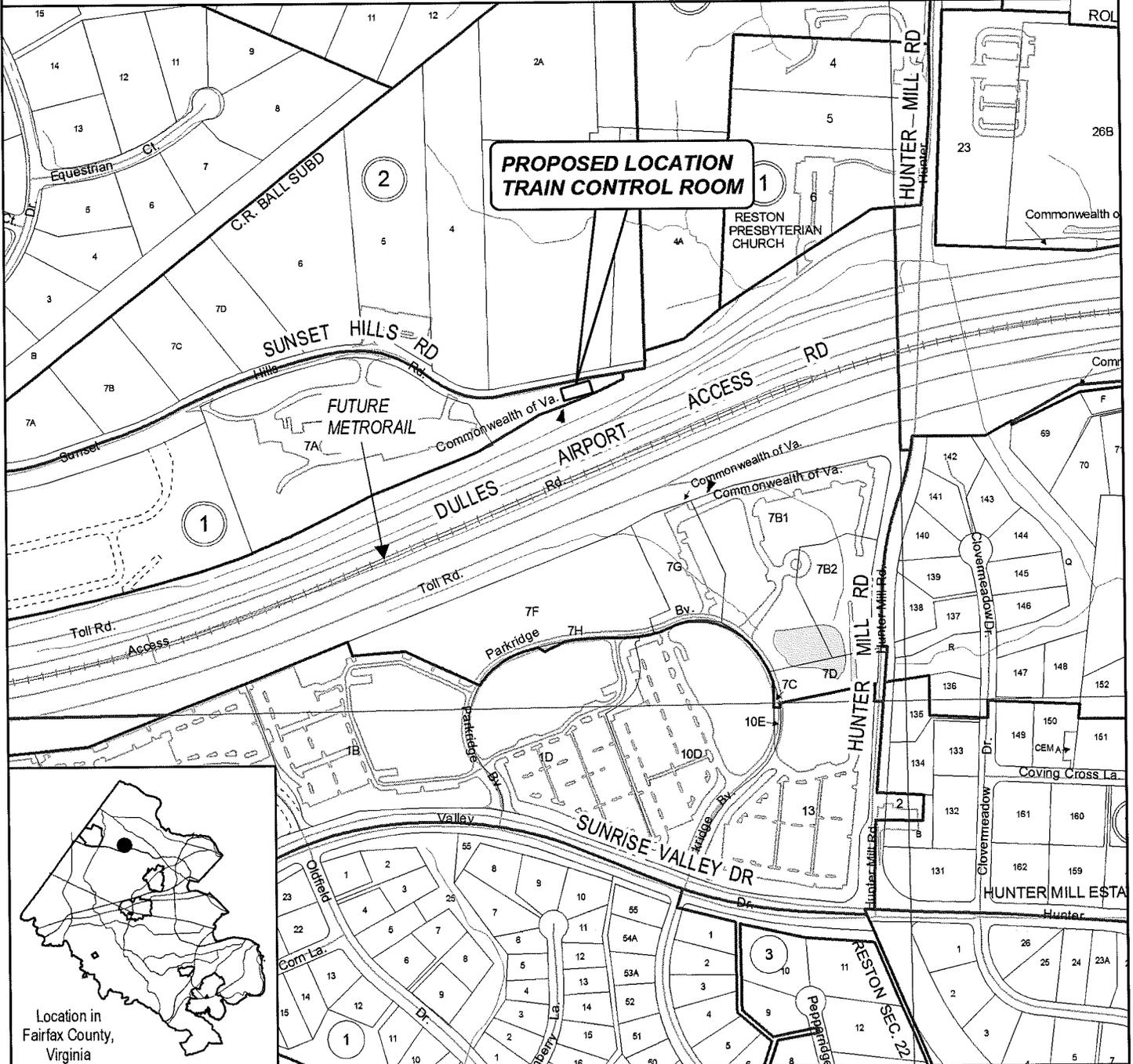
Acreage: Approx. 500 SF

Planned Use: VDOT (Right-of-Way)

Subject Property: 18-3

Applicant: Metropolitan Washington Airports Authority (MWAA)

Proposed Use: Train Control Room Associated with Extension of Metrorail



Location in Fairfax County, Virginia

500 FEET

PREPARED BY THE DEPARTMENT OF PLANNING AND ZONING
USING FAIRFAX COUNTY GIS



APPLICATION*Attachment A*

PROPOSAL: Construct a Traction Power Substation (TPSS) located south of Sunset Hills Road in the VDOT Right-of Way (ROW), associated with the extension of Metrorail service through Fairfax County

APPLICANT: Metropolitan Washington Airports Authority (MWAA) in coordination with the Department of Rail and Public Transportation (DRPT) on behalf of Washington Metropolitan Area Transit Authority (WMATA)

SUBJECT PROPERTY: Tax map 18-3 (VDOT Right-of Way)

Zoning District: R-E

Existing conditions: VDOT Right-of-Way. The site is developed with an existing Traction Power Substation (TPSS).

PROJECT JUSTIFICATION: The applicant states that TPSS houses the equipment necessary for safe operation of Metrorail trains, specifically equipment for the automatic control of the trains, communications to collect and transmit data to the Metrorail Operations Center and backup electrical power for the control and communications systems. The TPSS is also used to house equipment needed to operate Metrorail when there is a failure or blockage of a track. The Sunset Hills Road TPSS will house equipment needed to control operation of Metrorail trains.

The applicant states that the proposed TPSS at Sunset Hills Road is strategically located within the chain of TPSS facilities along the Metrorail extension to maintain a minimum voltage level available to train cars under certain emergency situations and allows maintenance workers to observe the performance of the switching system as periodic maintenance is performed at the TPSS. The applicant states that the proposed location will afford better access to the facility by eliminating the need for crossing through the active VDOT maintenance yard. The proposed location also allows for landscaping and screening along Sunset Hills Road in front of the facility.

Previous 2232 Approval and Need for Relocation

A TPSS co-located with a communication room was previously approved for this site with 2232-MD06-10 (January 18, 2007). The TPSS was proposed to be located immediately to the west of the existing TPSS. The applicant states that relocation of TPSS 10 to a location further east is necessary to resolve a conflict with a salt dome installed by VDOT in the area of the approved location. The proposed location will afford better access to the facility by eliminating the need for crossing through the active VDOT maintenance yard. The applicant states that the proposed location has an existing access point from Sunset Hills Road to a cell tower which can be used, thereby eliminating the need to construct an additional access point from Sunset Hills Road.

PROPOSED USE: TPSS associated with extension of Metrorail

Facility: See application (*Attachment A*) for full description; all dimensions are approximate:

- Structure – Preamsembled modular building with a footprint of 500 square feet (40 feet long x 72 feet wide), consisting of a train control room and a communications room. The facility is proposed to be 12 feet in height and surrounded by a 15-foot tall screen wall. The screen wall (brick façade) will be constructed around the entire modular building with a set-out distance of 5 feet from the building face to the inside of the screen wall. The screen wall is approximately one foot thick.
- Location – Approximately 50 feet south of Sunset Hills Road within the VDOT Right-of-Way.

- **Screening** – 15-foot tall screen wall (brick façade) around the entire modular building. Applicant proposes landscaping to the north of the facility in the 25' setback area to screen the facility from the adjacent residential uses, as well as to the east and west of the facility, as shown on the landscape plan. The landscaping for the proposed location has been designed so as to allow for the future widening of Sunset Hills Road to a 4-lane transportation corridor.
- **Access and Parking** – Existing access point to existing cell tower from Sunset Hills Road located immediately to the west of the facility. Adequate asphalt exists to the west of the facility for maintenance vehicle parking.
- **Operations** – unmanned; operates 24 hours/day, 7 days per week; routine maintenance visit once per week.

Off-Site Impacts: Applicant states that proposed facility will have no impact on air or water quality. Stormwater runoff will be handled within a basin designed to accommodate the runoff similar to the other TPSS facilities in the system. The applicant states that the facility will not have an adverse impact on traffic, and that there is no noise associated with the facility. The building will have a light above the secured doorways. The applicant states that there will not be any adverse impact to environmental features of the site and that the proposed location will allow for greater re-vegetation than the previously approved location.

Alternate locations: Applicant considered the following alternate locations (see *Attachment A* for full discussion and associated graphics); note this list includes the previously approved location:

- **Alternative 1** – the only other area offered by VDOT for relocation of the TPSS was a wooded area in the northwest corner of the property, a distance too great between site and tracks in the median of the Dulles Access Road.

DESCRIPTION OF SURROUNDING AREA

LOCATION: VDOT Right-of-Way, south of Sunset Hills Road, Tax Map 18-3

CHARACTER: In proximity of the Edlin School (470'), Reston Presbyterian Church (1,160') and single family detached residential (150' to closest) on large lots; transportation right-of-way

Adjacent and Nearby Land Uses:

- **Subject property** – VDOT Right-of-Way (ROW); existing TPSS
- **North** – single-family detached residential, school and church
- **East** – ROW/Dulles Airport Access Road
- **South** – Dulles Airport Access Road
- **West** – VDOT Facility/ROW

COMPREHENSIVE PLAN PROVISIONS

COMPREHENSIVE PLAN MAP:

Planning Area, District, and Sector: Area III, Reston-Herndon Suburban Center and Transit Station Areas, Upper Potomac Planning District

Land Use Recommendations:

- **Subject property** – road right-of-way (ROW)
- **North** – single family residential, .2-.5 du/ac

- East – ROW
- South – ROW and Dulles Airport Access Road-enhanced public transportation corridor
- West – ROW and planned development community (PDC)

The Transportation Plan Map identifies Dulles Airport Access Road as an Enhanced Public Transportation Corridor.

COMPREHENSIVE PLAN CITATIONS: An assessment of this proposal for substantial conformance with land use and design recommendations of the Comprehensive Plan (“the Plan”) is guided by the following citations from the Plan:

Area Plan: There are no site specific recommendations for the subject property.

Fairfax County Comprehensive Plan, Area III, 2007 Edition; UP5-Reston Community Planning Sector, as amended through September 10, 2007, Upper Potomac Planning District, Character, page 117:

...CHARACTER

Reston has its own Master Plan because the community was planned and developed as one of the nation's landmark new towns, beginning in the 1960s. It is located between Tysons Corner and the Washington Dulles International Airport along the Dulles Airport Access Road and extends as far north as Route 7 and as far south as Stuart Mill Road. With its planned development almost complete, Reston is comprised of 7,100 acres and may ultimately be the home of more than 60,000 people. This new town is designed around the concept of clustering the community into five "villages," each with its own village center. These centers provide for neighborhood-serving retail, office, and social needs.

The community is focused around the Town Center, an urban concentration of high-density housing, offices and cultural facilities. Substantial office development has occurred in recent years along the Dulles Airport Access Road, increasing development pressure both within and adjacent to the community. (This area is further addressed in the Reston-Herndon Suburban Center.) An integral part of the Reston Plan is the lower density residential development located on Reston's periphery, buffering adjacent areas from the higher density development in Reston.

Reston offers a wide range of housing, including high-rise apartments, garden apartments, townhouses, and single-family detached and semi-detached homes. The majority of dwellings in this sector were built after 1975. There are approximately 1,300 low- and moderate-income units in Reston. This housing includes units for the elderly which are found mostly in the village centers. There is scattered new and older residential development outside Reston. Generally these areas are planned to maintain a low density residential character, including areas along Route 7.

Lake Anne Village and Bowman Distillery are significant heritage resources listed in the Fairfax County Inventory of Historic Sites. A list and map of heritage resources are included in the Upper Potomac Planning District Overview section, Figures 4, 5 and 6. Additional historic sites in this sector are also included in the inventory. Bowman Distillery is also listed in the Virginia Landmarks Register and the National Register of Historic Places. Lake Anne Village is protected by a County Historic Overlay District.

“The entire sector is characterized by stable residential development...The predominant housing type in this well-established, stable area is single-family detached. The sector contains a mixture of old and new residential development. Most houses are generally well-maintained and most lots contain mature trees...”

Policy Plan:

Fairfax County Comprehensive Plan, Policy Plan, 2007 Edition; Public Facilities, as amended through January 10, 2005; COUNTYWIDE OBJECTIVES AND POLICIES, pages 2-4:

“Objective 1: Locate new facilities to provide convenient service to the greatest number of people or service consumers and users.

Policy a. Site facilities appropriately to the area they are intended to serve...

Objective 4: Mitigate the impact of public facilities on adjacent planned and existing land uses...

Policy b. Co-locate public facilities whenever appropriate to achieve convenience and economies of scale, as long as the integrity of the Comprehensive Plan is not impinged.

Policy c. Design facilities to promote and enhance the community identity of existing character.

Policy d. Ensure that public facilities are properly screened and buffered in order to mitigate visual impact on adjacent planned development of a different use or nature.

Policy e. Ensure that site size and development conforms to all requirements of the Fairfax County Zoning Ordinance and exceeds site acreage requirements, as possible, to achieve maximum compatibility with surrounding land uses.

Objective 5: Acquire sites which are appropriate for the facility's specific purpose. Apply acceptable criteria when evaluating public facility sites...

Policy c. Avoid areas of environmental sensitivity except where site acquisition is in support of open space.

Policy d. Evaluate engineering considerations, such as slopes and soils and other factors pertinent to knowing the extent of the site's development cost...”

Policy Plan:

Fairfax County Comprehensive Plan, Policy Plan, 2007 Edition; Transportation, as amended through March 9, 2009; COUNTYWIDE OBJECTIVES AND POLICIES, pages 2-8:

“Objective 2: Increase use of public transportation and non-motorized transportation.

Policies on Facilities

Policy a. Support the extension of the Metrorail system in the Dulles Corridor to the Dulles Airport and Loudoun County.

Policy b. Provide public transportation facilities (such as rail transit, commuter rail, bus rapid transit and/or HOV lanes) in major radial and intra-county commuter corridors designated as Enhanced Public Transportation Corridors in the Transportation Plan Map. Preserve land and rights-of-way where appropriate...

Policy f. Provide supporting facilities for the transit system, and provide resources to maintain County-owned equipments and facilities effectively...

Objective 7: Provide transportation facilities and services that minimize community disruption and adverse environmental impacts...

- Policy b. Plan and design transportation facilities and services to minimize and mitigate adverse impacts to residents and neighborhoods.”

STAFF ANALYSIS: See *Attachments B - K* for detailed discussion.

Fairfax County Department of Planning and Zoning

- Zoning Administration Division – Ordinance Administration Branch *Attachment B*
 - Zoned R-E. The facility is an accessory electrically-powered regional rail transit facility that is permitted by right in any zoning district subject to Sect. 2-516 of the Zoning Ordinance. The proposed facility meets Sect. 2-516.
- Planning Division – Historic Preservation/Heritage Resources *Attachment C*
 - No action is required.
- Environmental & Development Review *Attachment D*
 - No action required
- Zoning Evaluation Division *Attachment E*
 - No action required

Fairfax County Department of Public Works and Environmental Services

- Environmental and Site Review Division *Attachment F*
 - Proposed construction is within VDOT ROW and is not reviewed by ESRD.
- Urban Forest Management Division *Attachment G*
 - Alternative landscape screening should be provided along Sunset Hill Road and located further from the road in order to be retained with the potential widening of Sunset Hills Road. Shrubs could also be planted on both sides of the screening yard.

Fairfax County Fire & Rescue Department

No action required

Attachment H

Fairfax County Department of Transportation

- Transportation Planning Section *Attachment I*
 - Proposed location of TPSS may require a modification to the proposed design for widening Sunset Hills Road.

Fairfax County Water Authority

- Planning and Engineering Division
 - No action is required.

Attachment J

CONFORMANCE WITH THE COMPREHENSIVE PLAN

Attachment K

Va. Code Sec. 15.2-2232, as amended, requires the Planning Commission to determine whether the general location or approximate location, character, and extent of the proposed facility, as amended, are substantially in accord with the adopted Comprehensive Plan.

Location

According to the applicant, traction power substations (TPSS) are associated with the extension of Metrorail service throughout Fairfax County. This facility will also have an associated train control room and a communications room. TPSS facilities are typically spaced approximately one mile apart to maintain a minimum voltage level available to train cars under emergency conditions.

The proposed location is on land owned by the Commonwealth of Virginia between Sunset Hills Road and the Dulles Airport Access Road. Land on the north side of Sunset Hills Road is zoned R-E. The Edlin School, Reston Presbyterian Church, and the single family detached residences on large lots are located on the north side of Sunsets Hills Road. Property to the west is developed as a VDOT maintenance yard. The Dulles Airport Access Road is identified as an Enhanced Public Transportation Corridor in the Comprehensive Plan. The location of the TPSS conforms to Plan guidelines to provide public transportation facilities in major radial and intra-county commuter corridors designed as Enhanced Public Transportation Corridors and to preserve land and rights-of-way where appropriate. The proposed location is adjacent to an existing VDOT maintenance yard rail facility, and does not conflict with existing utilities, which conforms to Plan guidelines to co-locate public facilities whenever appropriate to achieve conveniences and economies of scale.

The applicant has investigated alternative sites, including the previously approved location, which conforms with Plan guidelines to evaluate engineering considerations pertinent to knowing the extent of the site's development cost. The proposed location also conforms to Plan guidelines to avoid areas of environmental sensitivity.

Character

The TPSS will be a pre-constructed modular building 40 feet by 72 feet in size and 12 feet in height. The TPSS will have an enclosed courtyard, 40 feet by 40 feet, on the north side, containing three transformers. The train control room and communications room will be located north of the courtyard in a pre-constructed modular building 40 feet by 12.5 feet and 11 feet in height. A 15-foot brick façade will be constructed as a screen wall 5 feet from the edge of the building around the entire perimeter of the building and courtyard.

Staff recognizes that the proposed facility will have a visual impact on adjacent residential properties. Clearing of existing trees will be required for construction of the facility. Staff believes the proposed plantings and the screen wall will mitigate the facility's visual impact on adjacent properties, minimizing the impact to the greatest extent practical

Given the proposed use as an accessory electrically-powered regional rail transit facility, the facility is not required to comply with the specific setback requirements for an R-E Zoning District, nor with the specific transitional screening requirements for the use, although according to Section 2-516 of the Zoning Ordinance, the facility must be designed in a manner that minimizes adverse impacts to the adjacent properties to the greatest extent practical through the use of landscaping, screening, design and architectural techniques. A 25-foot landscaped setback area is proposed between Sunset Hills Road and the screen wall for the facility. Staff believes that this proposed landscaped area and the screen wall helps to minimize the visual impact to the adjacent properties while keeping generally in character and meeting the intent of the requirements of this Zoning District. Finally, staff believes that given the proposed screening, the facility conforms to Comprehensive Plan guidelines to ensure that public facilities are property screened and buffered in order to mitigate visual impact on adjacent development of a different use or nature.

Extent

The proposed facility is approximately 500 square feet and will be surrounded by a screen wall (one foot thick) set out approximately five feet from the building itself. The proposed building size is less than the maximum permitted size for freestanding train control rooms per the Zoning Ordinance of 700 square feet. The building height is proposed to be 12 feet, surrounded by a 15-foot high screen wall, which is less than the maximum allowed building height of 20 feet for these facilities. This height is also less than would be allowed in the R-E Zoning District for a non-residential structure at 60 feet. Although the specific location on site is approximately 700 feet to the east from the originally approved location, staff believes that the visual impact of the facility is mitigated by the proposed wall and landscaping, as discussed above.

The proposed use will not have a traffic impact on the adjacent road network. The proposed building location will accommodate the planned future widening of Sunset Hills Road to a 4-lane section. Vegetative buffer on the north side of the building, facing Sunset Hills Road, will not be impacted by the road widening. Fairfax DOT notes that the Fairfax County Transportation Plan shows a future full interchange improvement for the Hunter Mill Road / Dulles Toll Road (DTR) interchange. Three conceptual options for this interchange were developed in 2000. The preferred option, A, would slightly modify the current Sunset Hills Road intersection. The proposed TPSS would require a modification to this design. DOT notes that, under Option A of possible redesigns of the DTR/Hunter Mill Road interchange, the proposed location of the TPSS is at the same location of the DTR westbound on-ramp. However, Option A, as well as the other options, were conceptual designs and not engineered plans. Should Option A proceed at some time in the future, DOT states that the DTR on-ramp/Sunset Hills intersection could be located farther to the east so that it would not conflict with the TPSS.

The proposed use would have minimal environmental impacts and no impacts on water or air quality.

CONCLUSIONS AND RECOMMENDATIONS

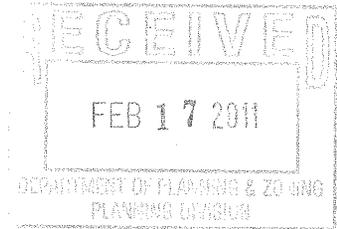
Staff concludes that the subject proposal by the Metropolitan Washington Airports Authority in coordination with the Department of Rail and Public Transportation, on behalf of Washington Metropolitan Area Transit Authority, as amended, to construct a train control room between Sunset Hills Road and the Dulles Airport Access Road in the VDOT Right-of Way, Reston, satisfies the criteria of location, character, and extent as specified in Va. Code Sec. 15.2-2232, as amended.

Staff therefore recommends that the Planning Commission find the subject Application **2232A-MD06-10-1**, as amended, substantially in accord with provisions of the adopted Comprehensive Plan.

METROPOLITAN WASHINGTON AIRPORTS AUTHORITY



February 16, 2011



Mr. Chris Caperton
Department of Planning and Zoning
Fairfax County
7th Floor, Suite 730
Herrity Building
12055 Government Center Parkway
Fairfax, Virginia 22035

Subject: Dulles Corridor Metrorail Project
Application for Determination Pursuant to Section 15.2-2232 of
the Code of Virginia
Traction Power Substation 10

Letter No: MWAA-04477

Dear Mr. Caperton:

The Metropolitan Washington Airports Authority acting in coordination with the Virginia Department of Rail and Public Transportation is pleased to submit the enclosed Section 15.2-2232 (Section 2232) application on behalf of the Washington Metropolitan Area Transit Authority. This application addresses a revised location for Traction Power Substation 10 as was previously approved by the Planning Commission on January 18, 2007.

The submission package includes the following application documents:

1. Completed and signed Section 2232 Application Form.
2. Table setting out information on the ownership of the property parcel required.
3. The Statement of Justification, which includes a description of the proposal and a discussion of needs, an alternative site and anticipated impacts.
4. Property identification map identifying the proposed location.
5. Aerial photograph depicting the approved location, the proposed location and an alternate site evaluated.
6. Preliminary engineering drawing included in the original 2232 application showing the approved location.
7. Description of the use, included in the original 2232 application.

Mr. Chris Caperton
Letter No.: MWAA-04477
February 16, 2011
Page 2

8. Layout and elevation plans.
9. Landscaping plans.

This application has been prepared following a meeting on February 9, 2011 with the Hunter Mill Planning Commissioner who requested that revision in location be scheduled for a public hearing rather than being administratively approved. We are not including in this submission, information listed in the 2232 Review Application document that is not considered relevant.

The information provided in this application is based on current plans for the facility. There may be refinements in design of the facility, but we do not anticipate further changes in the general character, location and extent.

Thank you for your attention to this matter. Please contact Lori Greenlief of McGuireWoods LLP at 703-712-5433 if you have questions or require additional information.

Sincerely,



Charles S. Carnaggio, P.E.
Project Director
Dulles Corridor Metrorail Project

CSC/ft/me

cc: Supervisor Cathy Hudgins, Hunter Mill District
Commissioner Frank de la Fe, Hunter Mill District
Rick Stevens, Fairfax County
John Thomas, WMATA
Mike Harris, VDRPT
Jim Van Zee, MWAA
Lori Greenlief, McGuireWoods LLP

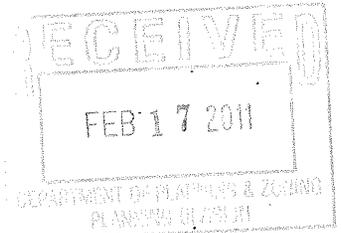
COUNTY OF FAIRFAX, VIRGINIA
APPLICATION FOR DETERMINATION
PURSUANT TO
SECTION 15.2-2232 OF THE CODE OF VIRGINIA

Application Number: 2232 A-FM-000-10-1
(Assigned by staff)

The application contains three parts: I. Application Summary; II. Statement of Justification; and III. Telecommunication Proposal Details. Please do not staple, bind or hole-punch this application. Please provide at least one copy of all pages, including maps and drawings, on 8.5 x 11 inch paper.

(Please Type or Print All Requested Information)

PART I: APPLICATION SUMMARY



ADDRESS OF PROPOSED USE

Street Address See attached property table
City/Town _____ Zip Code _____

APPLICANT(S)

Metropolitan Washington Airports Authority in coordination with the Department of Rail and Public Transportation on behalf of Washington Metropolitan Area Transit Authority

Name of Applicant _____
Street Address 1593 Springhill Road, Ste 300
City/Town Vienna State VA Zip Code 22182
Telephone Number: Work (703) 572-0500 Fax (____) _____
E-mail Address _____
Name of Applicant's Agent/Contact (if applicable) Jonathan P. Rak
Agent's Street Address 1750 Tysons Blvd., Ste 1800
City/Town McLean State Va Zip Code 22102
Telephone: Work (703) 712-5000 Fax (____) _____

PROPOSED USE

Street Address See attached property table

Fairfax Co. Tax Map and Parcel Number(s) 18-3

Brief Description of Proposed Use _____

Change in location of previously approved Traction Power Substation and Train Control Room located on property between Sunset Hills Road and the Dulles Airport Access Road associated with the extension of Metrorail service through the Dulles Corridor

Total Area of Subject Parcel(s) N/A (acres or square feet)

Portion of Site Occupied by Proposed Use _____ (acres or square feet)

Fairfax County Supervisor District Hunter Mill

Planned Use of Subject Property (according to Fairfax County Comprehensive Plan)
N/A

Zoning of Subject Property N/A

List all applicable Proffer Conditions, Development Plans, Special Exceptions, Special Permits or Variances previously approved and related to this site

None

PROPERTY OWNER(S) OF RECORD

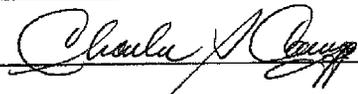
Owner See attached property table

Street Address _____

City/Town _____ State _____ Zip Code _____

PART II, entitled "Statement of Justification," pages 4 through 6, shall be completed by all applicants and included as part of the application. **PART III**, entitled "Telecommunication Proposal Details," pages 7 through 9, also shall be completed and included for all proposed telecommunication uses.

Name of Applicant or Agent Charles S. Carnaggio

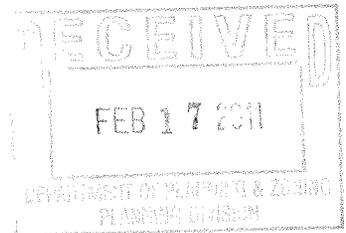
Signature of Applicant or Agent 

Date 2/16/11

Please do not staple, bind or hole-punch this application. Please provide at least one copy of all pages, including maps and drawings, on 8.5 x 11 inch paper.

Submit completed application to:

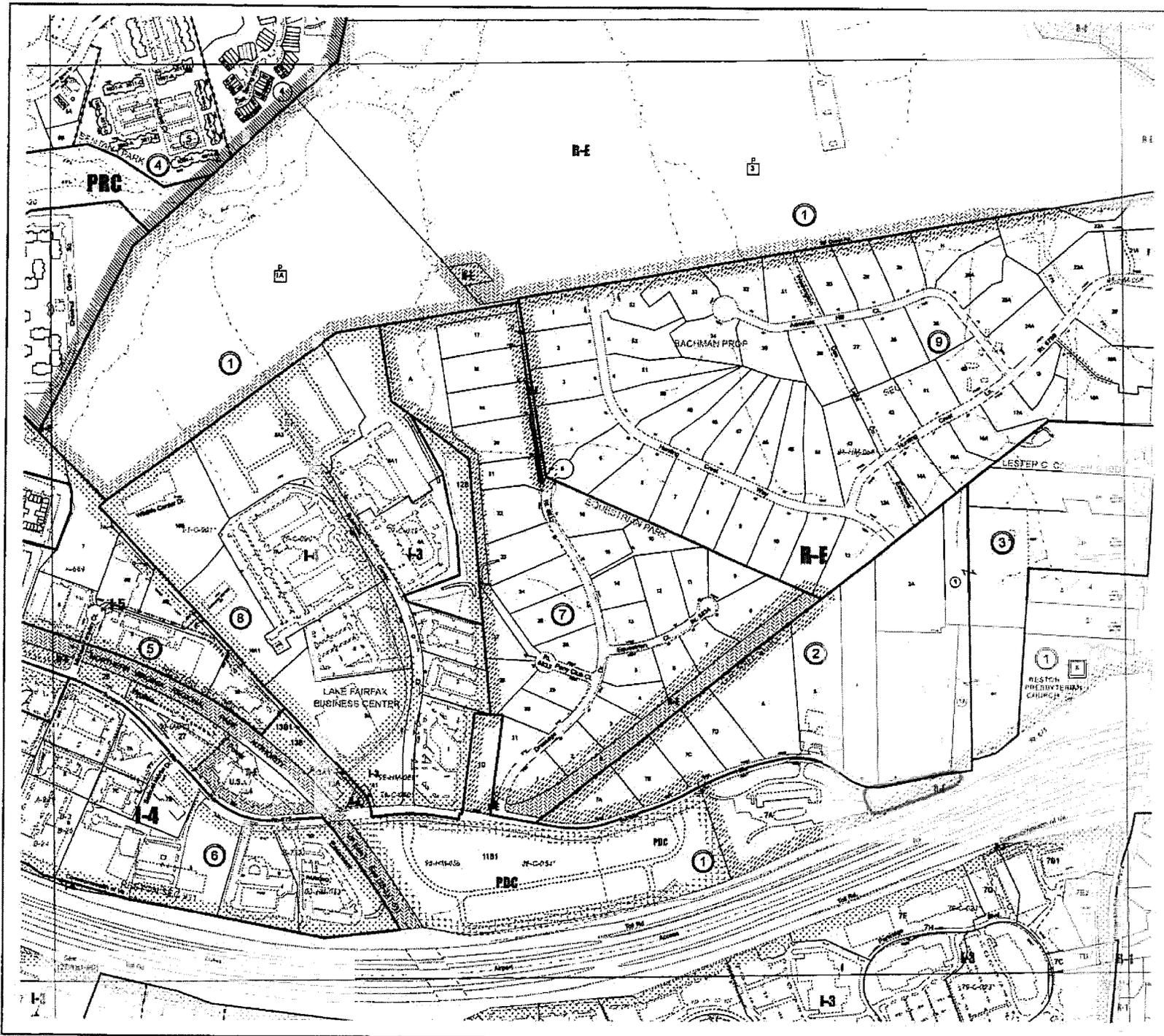
**Fairfax County
Department of Planning and Zoning, Planning Division
Herrity Building
12055 Government Center Parkway, Suite 730
Fairfax, Virginia 22035**



FOR STAFF USE ONLY	
Date application received:	<u>2/17/11</u>
By:	<u></u>
Additional information requested to complete application:	
Date application accepted:	<u>2/17/11</u>
By:	<u></u>

**PROPERTY IDENTIFICATION TABLE FOR DULLES CORRIDOR METRORAIL PROJECT
TPSS 10
SECTION 2232 APPLICATION**

TAX MAP NUMBER	PROPOSED USE	MAGISTERIAL DISTRICT	PLANNED USE	ZONING	PROPERTY OWNER	PROPERTY LOCATION	OWNER ADDRESS
18-3		Hunter Mill			Commonwealth of Virginia	North side of Dulles Toll Road west of Hunter Mill Road and south of Sunset Hills Road	



Scale: 1" = 100 Feet
 This map is for general information only and does not constitute a contract. The user should verify all information on this map with the appropriate authorities. The user should also verify all information on this map with the appropriate authorities. The user should also verify all information on this map with the appropriate authorities.

GENERAL NOTES

1. This map is for general information only and does not constitute a contract. The user should verify all information on this map with the appropriate authorities. The user should also verify all information on this map with the appropriate authorities. The user should also verify all information on this map with the appropriate authorities.



ADMINISTRATIVE BLOCKS

17-2	18-1	18-2
17-4	18-3	18-4
20-2	27-1	27-2

SHEET INDEX

PROPERTY MAP ZONING
18-3

Revised to 01 - 01 - 2011

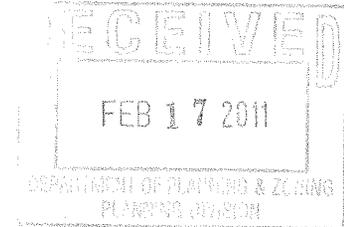
FAIRFAX COUNTY
 OFFICE OF THE COUNTY ENGINEER
 11000 Lee Highway, Suite 100
 Fairfax, Virginia 22031
 (703) 246-1000

**Metropolitan Washington Airports Authority in coordination with the
Virginia Department of Rail and Public Transportation on behalf of
Washington Metropolitan Area Transit Authority (WMATA)**

Section 15.2-2232 Application

STATEMENT OF JUSTIFICATION

February 15, 2011



1. Description of Proposed Use

The proposed public use is a traction power substation (TPSS) associated with the extension of Metrorail service through Fairfax County. There will also be a train control room/communications room within the facility. This 2232 determination request is essentially an amendment to shift the previously approved location, which was part of the system-wide 2232 approval on January 18, 2007 for the tracks and ancillary facilities, to a different location, approximately 700 east of the approved location. Both the previously approved location and the proposed location are on land owned by the Commonwealth of Virginia between Sunset Hills Road and the Dulles Airport Access Road on Tax Map 18-3. Land on the north side of Sunset Hills Road is zoned R-E. The Edlin School, Reston Presbyterian Church and single family detached dwellings on large lots are located on the north side of Sunset Hills Road. The property to the west is developed with a VDOT maintenance yard.

Traction Power Substations transform alternating current electrical power into direct current needed to power Metrorail train cars. TPSS facilities are spaced approximately one mile apart to maintain a minimum voltage level available to train cars under certain emergency conditions. Emergency conditions include events such as an adjacent substation becoming unavailable due to maintenance or a power failure. Normal operating conditions address factors including headway requirements between trains and the number of cars per train. A train control room and communications equipment are co-located with the TPSS to control operation of Metrorail trains.

The TPSS will be a pre-constructed modular building 40 feet by 72 feet in size and 12 feet in height. The TPSS has an enclosed courtyard at one end containing three transformers. The size of this courtyard is 40 feet by 40 feet. The train control room/communications room will be located on the opposite side of the courtyard from the TPSS in a pre-constructed modular building 40 feet by 12.5 feet and 11 feet in height. A 15 foot high brick façade will be constructed as a screen wall 5 feet out around the entire perimeter of the modular buildings and the courtyard. This brick façade is consistent with the treatment of other ancillary facilities approved as part of the original 2232. The facility will operate, unmanned, 24 hours a day, 7 days a week. It is anticipated that a maintenance vehicle will visit the site once a week.

2. Requirements for Proposed Use

TPSS 10 is strategically located within the chain of TPSS facilities along the Metrorail extension for the reasons stated above. Relocation of TPSS 10 to a location farther to the east is necessary to resolve a conflict with a salt dome installed by VDOT in the area of the approved location. Included with this submission is a preliminary engineering drawing, which was included in the original 2232 staff report, showing the approved location. Also included in this submission is an aerial photograph showing the approved location, the proposed location and one other location which was assessed. The proposed location will afford better access to the facility by eliminating the need for crossing through the active VDOT maintenance yard. At the proposed location, there is an existing access point from Sunset Hills Road to a cell tower which can be shared, thus eliminating construction of an additional access point from Sunset Hills Road.

An alternative location was explored in the northwestern portion of the VDOT property but for several reasons, this location was not feasible. These reasons are discussed below under Alternatives.

3. Anticipated Impacts on Adjoining Properties and On- and Off-site Environmental Features

Traffic Impacts

Relocation of the TPSS will not change the anticipated traffic impact, and as indicated above, it was previously determined that the once a week maintenance visit would not create an adverse traffic impact to the neighborhood.

It is noted that the location, orientation and area for landscaping for the proposed location has been designed so as to allow for the future widening of Sunset Hills Road to a 4-lane urban section as shown on the Comprehensive Plan. The building has been located so that a substantial area, large enough for canopy and lower level evergreens, will remain if and when an additional traffic lane is added on the south side of Sunset Hills Road.

Noise and Light Impacts

Low-level noise will be generated at the traction power substation from ventilation fans. The low-level hum from transformers will be attenuated by the screening wall enclosing the entire facility. The building will have lights above secured doorways.

Impacts on Air and Water Quality

The operation of the equipment within the TPSS will not create any impact to air quality. Stormwater runoff for the facility will be handled within a basin designed to accommodate the runoff similar to the other TPSS facilities in the system.

Impacts on Environmental Features of Site

There will not be adverse impact to any other environmental features of the property.

Visual Impacts

The TPSS building will be surrounded by an architectural brick wall, specifically for screening purposes. To allow room for landscaping and screening, the structure has been positioned as far from Sunset Hills Road as practical along the slope down to the Dulles Airport Access Road. The proposed landscaping consists of canopy trees and hollies to create multi-level screening.

The building, itself, cannot be shifted farther out on the slope because a larger and more substantial retaining wall would be required to support the building foundation. If the height of the retaining wall were increased, tie-backs would be needed to support the wall and those tie-backs would interfere with the grounding grid located beneath the TPSS. Shifting the building elevation down the slope would create issues with access to the building for maintenance purposes. For future replacement of equipment, WMATA requires tractor-trailer access to the rollup door on the western end of the facility. If the facility were located farther from Sunset Hills Road, the traction power manholes for ductbanks running from the tracks to the TPSS would likely interfere with the drainage ditch running along the shoulder of the Dulles Toll Road.

4. Alternative Sites Considered

The only other area offered as an option by VDOT for relocation of the TPSS was a wooded area in the northwest corner of the property. This location is depicted on the attached aerial photograph showing the approved, proposed and alternative locations.

The alternative location was deemed not feasible for several reasons, primarily because of the distance between this site and tracks in the median of the Dulles Airport Access Road. The traction power ductbanks and cables that run between the tracks and the TPSS would need to be several hundred feet longer than for the

proposed location, which adds expense but also results in additional voltage drop. To address the voltage drop, additional ductbanks and cables would be required. These ductbanks and cables would be routed through the VDOT maintenance yard, and it is likely that existing underground utilities and drainage lines on the VDOT property would be impacted. An additional disadvantage is that the track area would not be visible from the TPSS, which is a desirable feature for Metrorail operations. Further, this area is within the perimeter fence for the VDOT maintenance yard and access to a TPSS in this location would cause security concerns, as was the case for the approved location.

The alternative location would require removal of approximately one acre of mature vegetation, and the facility would be located across Sunset Hills Road from three established residences that sit fairly close to the road. This location did not offer improvement in the impact to surrounding residences compared to the proposed location.

For these reasons, it was determined that the area in the northwestern portion of the VDOT property was not feasible for location of the TPSS.

5. **Property Identification on Fairfax County Tax Map** – included in submission
6. **Proposed Facility Plan** – included in submission
7. **Reduced Copy of Plans** – included in submission, as well a full size set of plans.

Conclusion

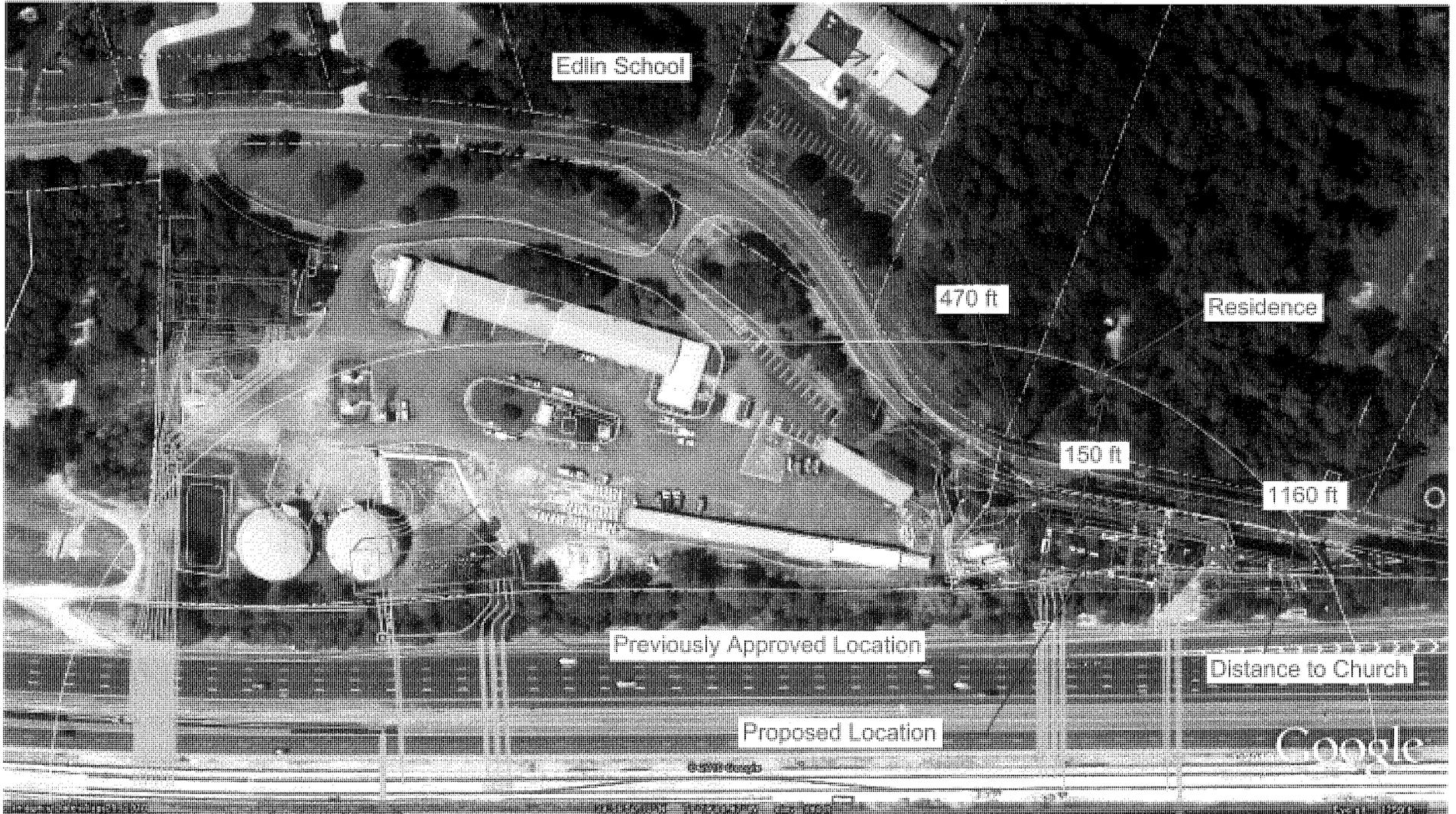
The following is offered in addition to the foregoing information to specifically to address the standards under Va. Code Sec. 15.202232, as amended:

Location: The shift of the location of the proposed location of the traction power substation does not hinder the existing determination that its location is consistent with the recommendations of the Comprehensive Plan. Further, the location of the proposed facility is such that Sunset Hills Road can be widened in accordance with the recommendations of the Comprehensive Plan. The Policy Plan goal of reducing dependence on the automobile by increasing the efficiency and productivity of the mass transit system will also be furthered by providing this support service facility.

Character: The character of the facility has not changed with its new location. Actually, it is anticipated that the facility will be shorter in height than that previously approved and, in the new location, landscaping and screening will be provided to screen the facility.

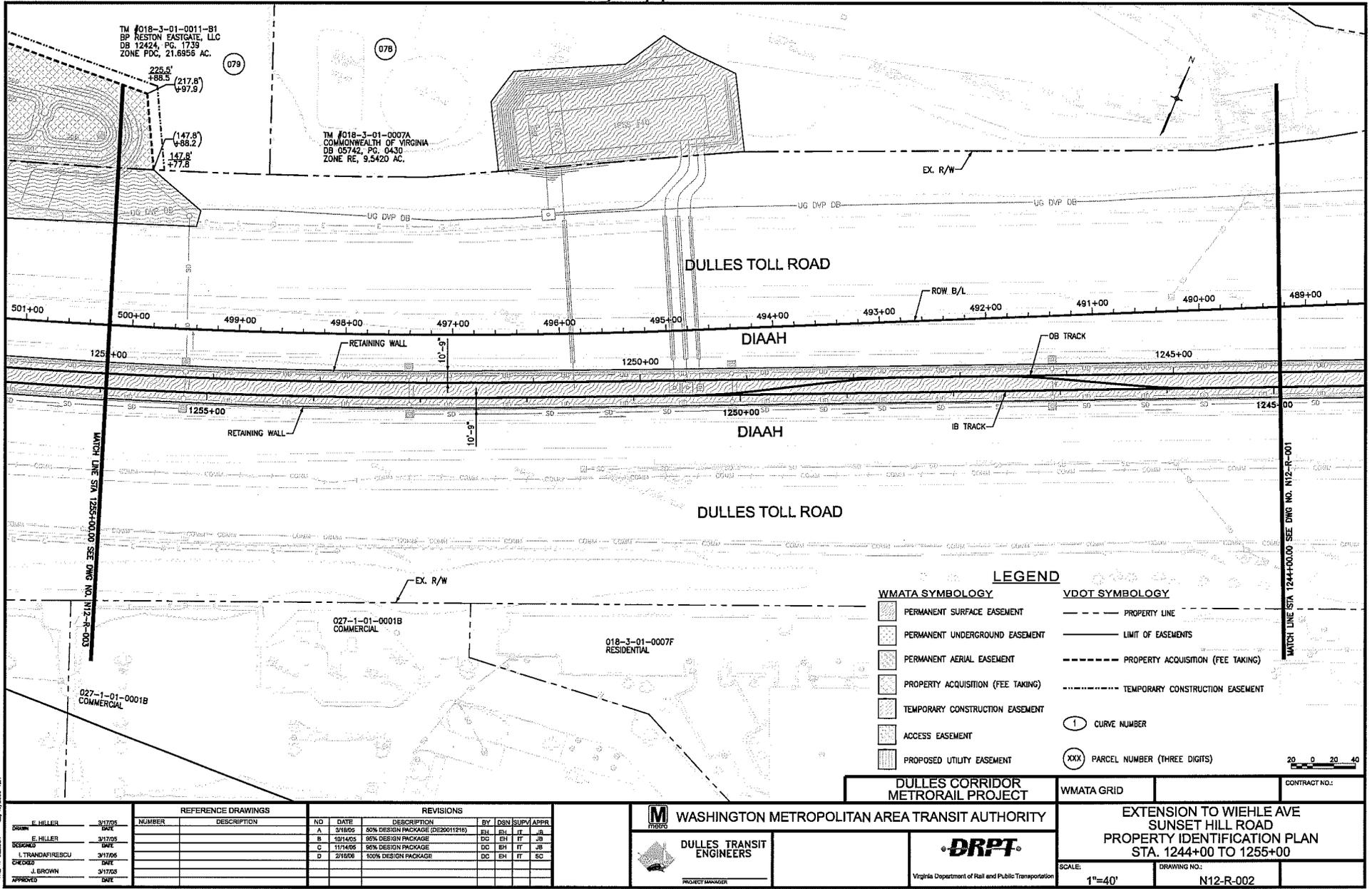
Extent: The extent of the facility has not changed with its new location.

The proposed location for the TPSS meets the location, character and extent of the Comprehensive Plan and therefore, the Applicant respectfully requests support of this 2232 application by County Staff and the Planning Commission.



RECEIVED
APR 12 2011

Previously Approved Location



TM #018-3-01-0011-B1
BP RESTON EASTGATE, LLC
DB 12424, PG. 1739
ZONE FDC, 21.6956 AC.

TM #018-3-01-0007A
COMMONWEALTH OF VIRGINIA
DB 05742, PG. 0430
ZONE RE, 9.5420 AC.

027-1-01-0001B
COMMERCIAL

018-3-01-0007F
RESIDENTIAL

027-1-01-0001B
COMMERCIAL

LEGEND

WMATA SYMBOLOGY

- PERMANENT SURFACE EASEMENT
- PERMANENT UNDERGROUND EASEMENT
- PERMANENT AERIAL EASEMENT
- PROPERTY ACQUISITION (FEE TAKING)
- TEMPORARY CONSTRUCTION EASEMENT
- ACCESS EASEMENT
- PROPOSED UTILITY EASEMENT

VDOT SYMBOLOGY

- PROPERTY LINE
- LIMIT OF EASEMENTS
- PROPERTY ACQUISITION (FEE TAKING)
- TEMPORARY CONSTRUCTION EASEMENT
- CURVE NUMBER
- PARCEL NUMBER (THREE DIGITS)

DULLES CORRIDOR
METRORAIL PROJECT

WMATA GRID

CONTRACT NO.:



WASHINGTON METROPOLITAN AREA TRANSIT AUTHORITY



DULLES TRANSIT
ENGINEERS



Virginia Department of Rail and Public Transportation

EXTENSION TO WIEHLE AVE
SUNSET HILL ROAD
PROPERTY IDENTIFICATION PLAN
STA. 1244+00 TO 1255+00

SCALE:

1"=40'

DRAWING NO.:

N12-R-002

REFERENCE DRAWINGS

NUMBER	DESCRIPTION

REVISIONS

NO	DATE	DESCRIPTION	BY	DSN	SUPV	APPR
A	3/18/05	65% DESIGN PACKAGE (DE28011218)	EH	EH	IT	JB
B	10/14/05	85% DESIGN PACKAGE	DC	EH	IT	JB
C	11/14/05	95% DESIGN PACKAGE	DC	EH	IT	JB
D	2/18/06	100% DESIGN PACKAGE	DC	EH	IT	SC

K12-3-00000N 02/16/2006 EST

Dulles Corridor Metrorail Project

General Description of Traction Power Substations/Train Control Rooms

Traction Power Substations

The purpose of a traction power substation is to transform alternating current electrical power into direct current needed to power Metrorail train cars. Traction power substations are fed via cables from the Dominion Virginia Power utility network. Incoming AC power at 34.5 kV is transformed by the substation equipment to 750 volts DC. This DC voltage is then fed through cables to the "third rail" running alongside the track. Positive voltage is fed to the third rail and negative voltage is collected from the track rails. Train cars collect the power through a sliding shoe that rides on the third rail.

Traction power substations generally are spaced one mile apart. Ideally, substations are located at passenger stations, since the trains draw heavy current when they start moving from a stop. Additional traction power substations are provided when there are long distances between passenger stations. Substation locations are determined in order to maintain a minimum voltage level available to train cars under certain emergency conditions.

The exact location of a substation and its capacity are determined by a Load Flow Simulation Computer Analysis. This analysis ensures that the traction power supplied to the third rail meets the operational requirements of Metrorail under normal and emergency conditions. Emergency conditions include events such as an adjacent substation becoming unavailable due to maintenance or a power failure. Normal operating conditions address factors including headway requirements between trains and the number of cars per train.

Traction power substations vary in size depending on the equipment included and whether one is co-located with a tie breaker station or remote train control room. The typical building enclosure for a traction power substation located between passenger stations will be 40-foot wide by 150-foot in length with a wall height of 23-feet. Building exteriors will be finished in a neutral color brick similar to existing Metrorail facilities on the Orange Line. Where there are adjacent residential properties, landscaping will be provided with plantings including native shade trees and evergreen trees.

Low-level noise will be generated at a traction power substation from ventilation fans. The low-level hum from transformers will be attenuated by the masonry walls enclosing the equipment. There will be no rotating equipment and no vibration. Lighting will be provided at the primary entry doors with no other area/site lighting. The buildings will be locked and equipped with intrusion detection systems to deter vandalism. Access to

each substation will be provided by 18-foot wide paved driveways to accommodate lift trucks. Where possible and for safety reasons, access will be provided from local streets rather than from pullouts from the Dulles Connector Road or the Dulles Toll Road.

Traction power substations will generate approximately one visit per week from maintenance staff. Vehicles visiting the facility for routine maintenance will be heavy duty pickup trucks.

Remote Train Control Room

Remote train control rooms house the equipment needed for safe operation of Metrorail trains. This includes equipment for automatic control of the trains, communications to collect and transmit data to the Metrorail Operations Center, and backup electrical power for the control and communications systems. Remote train control rooms are typically located in passenger stations where the equipment is readily accessible for maintenance. Remote train control rooms are necessary between passenger stations if the distance between stations exceeds the maximum length for control equipment to be effective. One is also located near the tie-in of the Dulles Extension to the existing Metrorail Orange Line.

Remote train control rooms are also used to house equipment needed to operate Metrorail in situations when there is a failure or blockage of a track. In these circumstances Metrorail operations may continue by using emergency crossovers to switch trains from one track to another. Safe operation using crossovers involves track interlocking which is performed by signaling and switch equipment. The control of this equipment must be located near the track crossover point and remote train control rooms are used if crossovers are not near passenger stations.

Remote train control rooms and accompanying communications rooms are co-located with either a traction power substation or a tie breaker station, except for one facility located at the tie-in point to the existing Metrorail Orange Line. This remote train control room will be located at the existing Metrorail Fisher Avenue Substation in a new building approximately 14-foot wide by 43-foot length with a wall height of 16-feet. The building exterior will be finished in a neutral color brick similar to the existing Metrorail facilities at that location, and will be landscaped and screened in a similar manner.

No noise will be generated at a remote train control room and there will be no vibration. Lighting will be provided at the primary entry doors with no other area/site lighting. The buildings are locked and equipped with intrusion detection systems to deter vandalism.

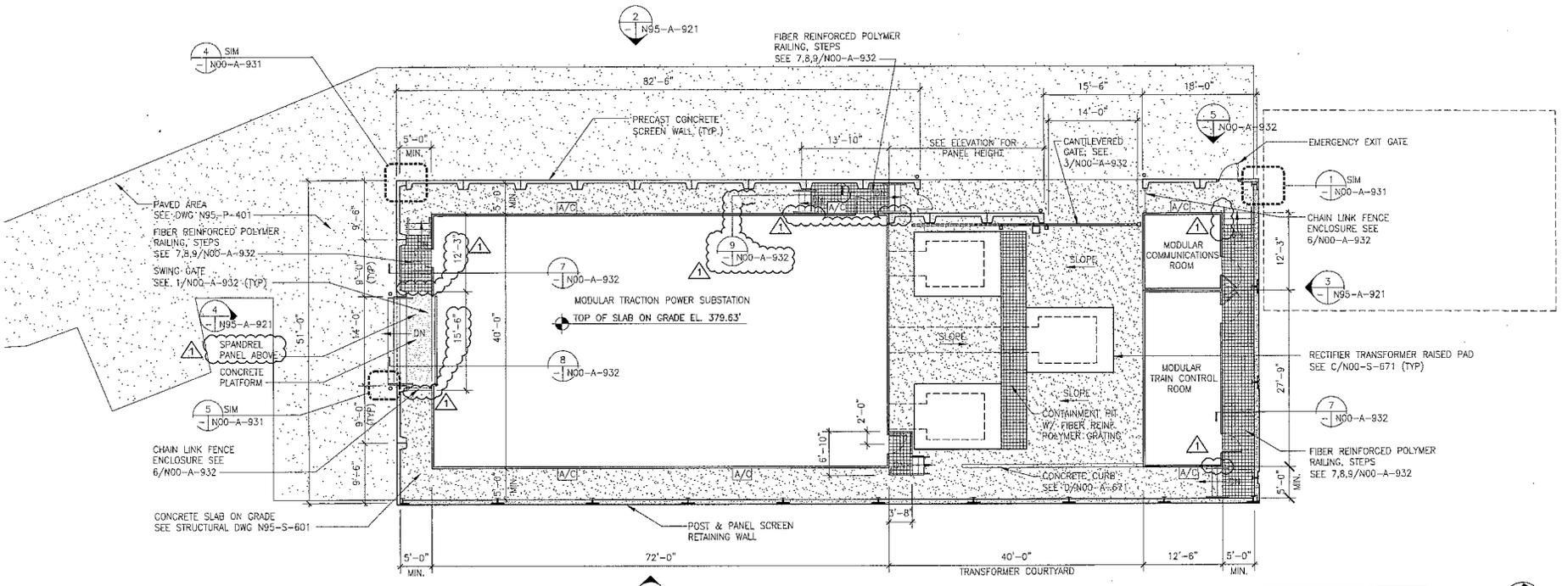
Stand alone remote train control rooms will generate approximately one visit per week from maintenance staff. Vehicles visiting the facility for routine maintenance will be heavy duty pickup trucks.

RECEIVED

FEB 17 2011

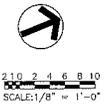
DEPARTMENT OF PLANNING & ZONING
PLANNING DIVISION

SUNSET HILLS ROAD



2 TPSS # 10 FLOOR PLAN
SCALE: 1/8" = 1'-0"

NOTE:
WIDTH OF THE PC CONCRETE PANELS IS TO BE ADJUSTED BY THE PC PANEL MANUFACTURER, SO THAT THE MIN REQUIRED EGRESS WIDTH OF 18" AT DOOR'S SWING IS MAINTAINED. SEE 5/N00-A-930

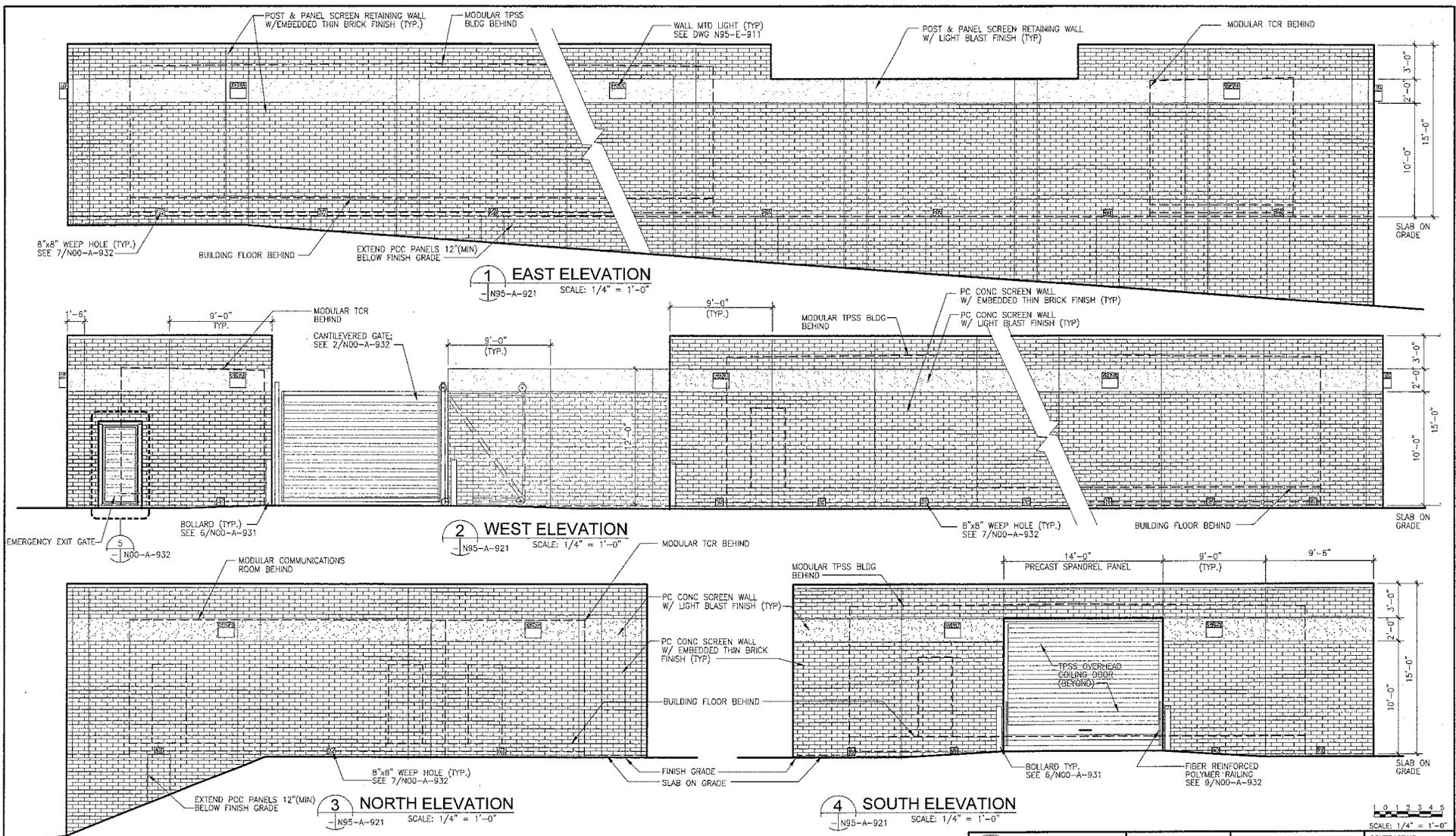


ORIGINATORS H. SHAWK 05-06-09 L. GORI 05-08-09 N. HEMAN-ACKAN 05-08-09 R. WERNER 05-08-09		REFERENCE DRAWINGS NUMBER DESCRIPTION NO. DATE DESCRIPTION 0 05-11-10 ISSUED FOR PERMIT 1 09-22-10 REV'D AS NTD & REISSUED FOR PERMIT		REVISIONS ORIG. CHKD. SUPP. APPR. LG. ALR. MHA. RS. AS. LG. MHA. RS. LG. MHA. RS.				METROPOLITAN WASHINGTON AIRPORTS AUTHORITY DULLES TRANSIT PARTNERS, LLC 1590 SPRING HILL RD., STE. 600 VIENNA, VA 22182				CONTRACT NO.: WMATA GRID	
DESIGNED N. HEMAN-ACKAN 05-08-09		CHECKED R. WERNER 05-08-09		APPROVED R. WERNER 05-08-09		D395 PROJECT IDENTIFICATION CODE: 505-08505-690				EXTENSION TO WIEHLE AVE WAYSIDE BUILDINGS SUNSET HILLS ROAD TPSS # 10 - FLOOR PLAN			
								SCALE: 1/8" = 1'-0"		DRAWING NO.: N95-A-911			

N95-A-911.dwg, 9/22/2010 2:18:50 PM

Electronic copy: your printer may not reproduce all information contained in this drawing. Refer to the electronic document for complete information.

Electronic copies shall be provided as indicated and may become external. Refer to the electronic documents in accordance for current revisions.



ORIGINATORS	
DESIGN	HSHARGH 05-08-09
DESIGNED	L.GORI 05-08-09
CHECKED	H.HEMAN-ACKAKI 05-08-09
APPROVED	R.A.WERNER 05-08-09

REFERENCE DRAWINGS		REVISIONS	
NUMBER	DESCRIPTION	NO.	DATE
		0	05-11-10



METROPOLITAN WASHINGTON AIRPORTS AUTHORITY

DULLES TRANSIT PARTNERS, LLC
1295 SPRING HILL RD, STE 500
VIENNA, VA 22182

WASHINGTON METROPOLITAN AREA TRANSIT AUTHORITY

DGS PROJECT IDENTIFICATION CODE:
505-08505-690

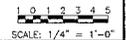
DULLES CORRIDOR METRO RAIL PROJECT

WMATA GRID

**EXTENSION TO WIEHLE AVE
WAYSIDE BUILDINGS
SUNSET HILLS ROAD
TPSS #10 - ELEVATIONS**

SCALE: 1/4"=1'-0"
DRAWING NO.: N95-A-921

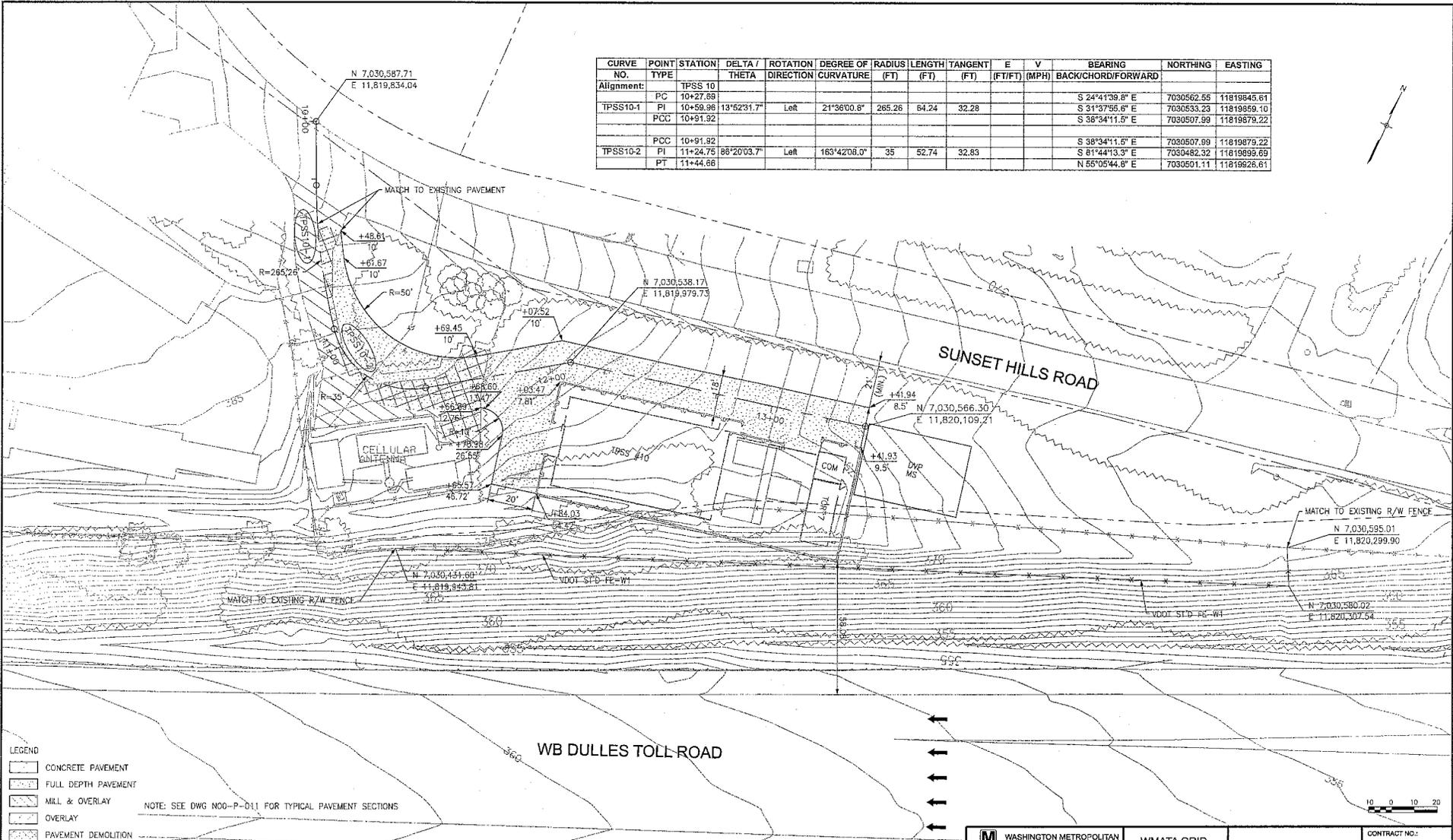
CONTRACT NO.:



10/24/09 10:47 AM 27 2/2/10 2:11:28 PM

Electronic copies were printed via UNCONTROLLED and may become outdated. Refer to the electronic documents in INFOWORKS for current revisions.

CURVE NO.	POINT TYPE	STATION	DELTA / THETA	ROTATION DIRECTION	DEGREE OF CURVATURE	RADIUS (FT)	LENGTH (FT)	TANGENT (FT)	E (FT/FT)	V (MPH)	BEARING	NORTHING	EASTING
Alignment:													
	PC	10+27.59									S 24°41'39.8" E	7030562.55	11819845.61
TPSS10-1	PI	10+59.96	13°52'31.7"	LeR	21°36'00.8"	265.26	64.24	32.28			S 31°37'55.6" E	7030533.23	11819859.10
	PCC	10+91.92									S 38°34'11.5" E	7030507.99	11819879.22
	PCC	10+91.92									S 38°34'11.5" E	7030507.99	11819879.22
TPSS10-2	PI	11+24.75	88°20'03.7"	Left	163°42'08.0"	35	52.74	32.83			S 81°44'13.3" E	7030482.32	11819895.65
	PT	11+44.66									N 55°05'44.8" E	7030501.11	11819926.61



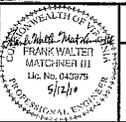
- LEGEND**
- CONCRETE PAVEMENT
 - FULL DEPTH PAVEMENT
 - MILL & OVERLAY
 - OVERLAY
 - PAVEMENT DEMOLITION

NOTE: SEE DWG NOG-P-011 FOR TYPICAL PAVEMENT SECTIONS



5/12/2010 9:00 N95-P-401.dgn

ORIGINATORS		REFERENCE DRAWINGS		REVISIONS			
DATE	DESCRIPTION	NO.	DATE	DESCRIPTION	ORIG	CHKD	SUPV
12/10/08	R. BOWERS						
12/10/08	R. JAKMINICH						
12/10/08	F. MATCHNER						
12/10/08	S. SIEBERT						

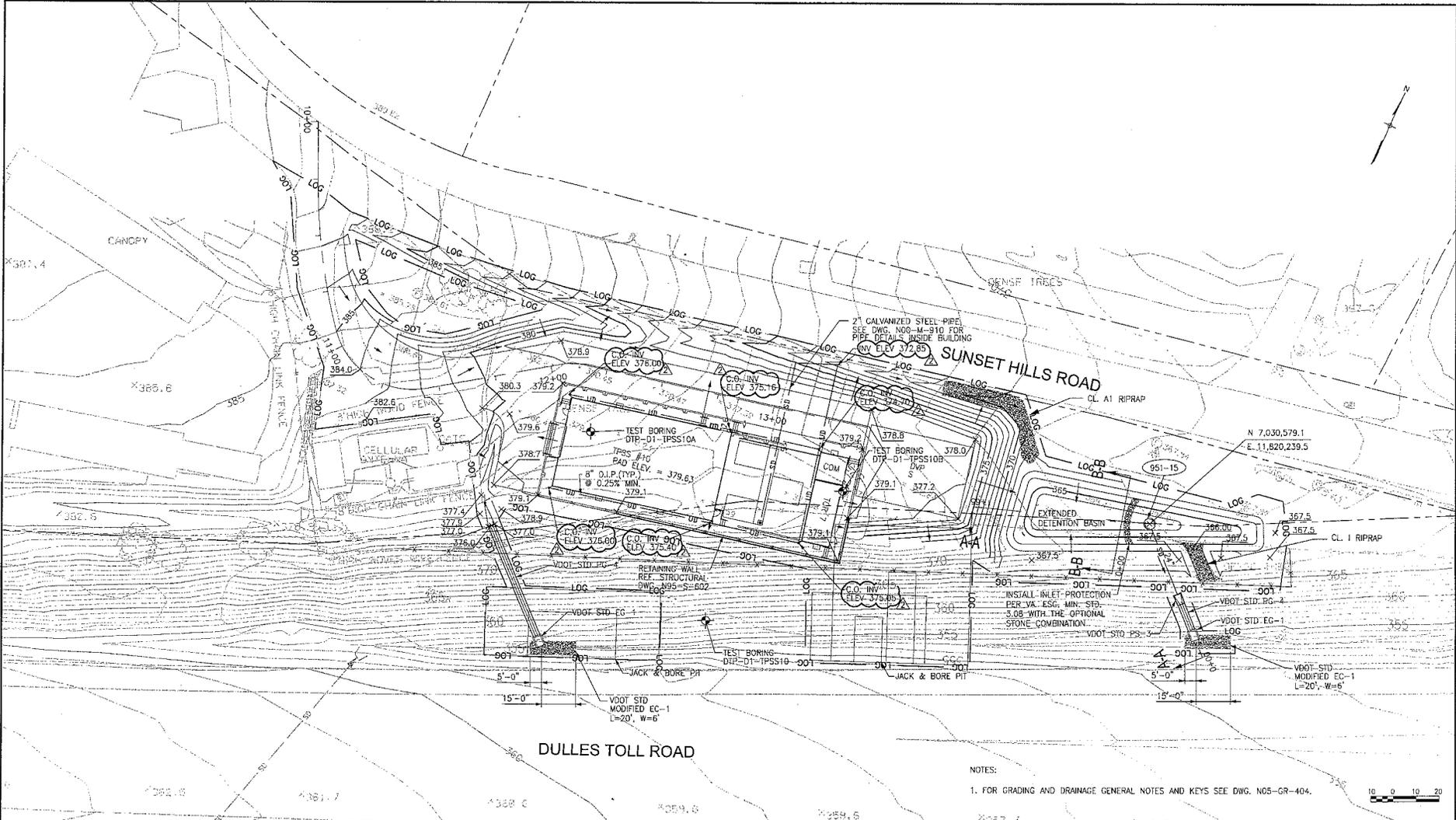


METROPOLITAN WASHINGTON AIRPORTS AUTHORITY

DULLES TRANSIT PARTNERS, LLC
1595 SPRING HILL RD, STE 600
VIENNA, VA 22182

<p>WASHINGTON METROPOLITAN AREA TRANSIT AUTHORITY</p> <p>DGS PROJECT IDENTIFICATION CODE: 505-08505-690</p>	<p>WMATA GRID</p>	<p>CONTRACT NO.:</p>
<p>DULLES CORRIDOR METRO RAIL PROJECT</p>	<p>SCALE: 1"=20'</p>	<p>DRAWING NO.: N95-P-401</p>

Electronic copies were printed and uncontrolled and may become outdated. Refer to the electronic documents in MATWORKS for current revisions.



NOTES:
 1. FOR GRADING AND DRAINAGE GENERAL NOTES AND KEYS SEE DWG. N05-GR-404.



9/24/2010 3:48 N95-GR-401.dgn

ORIGINATORS	
DRAWN	P. BOWERS 01/12/09
DESIGNED	A. KARADAG 01/12/09
CHECKED	F. MATCHNER 01/12/09
APPROVE	S. SIEBERT 01/12/09

REFERENCE DRAWINGS	
NUMBER	DESCRIPTION
N95-GR-411	BASIN DETAILS
N95-C-341	SITE CLEARING & DEMOLITION PLAN
N95-F-401	ROADWAY PLAN
N95-GR-401	GRADING AND DRAINAGE PLAN
N95-R-001	PROPERTY IDENTIFICATION PLAN

REVISIONS		
NO.	DATE	DESCRIPTION
0	6/12/10	ISSUED FOR PERMIT
1	6/4/10	REVISED TEST BORINGS
2	8/24/10	INCORPORATED DGS COMMENTS



METROPOLITAN WASHINGTON AIRPORTS AUTHORITY

DULLES TRANSIT PARTNERS, LLC
 1588 BRAND HILL RD, STE 600
 WINDY HILL, VA 22183

WASHINGTON METROPOLITAN AREA TRANSIT AUTHORITY

DGS PROJECT IDENTIFICATION CODE:
505-08505-690

WMATA GRID

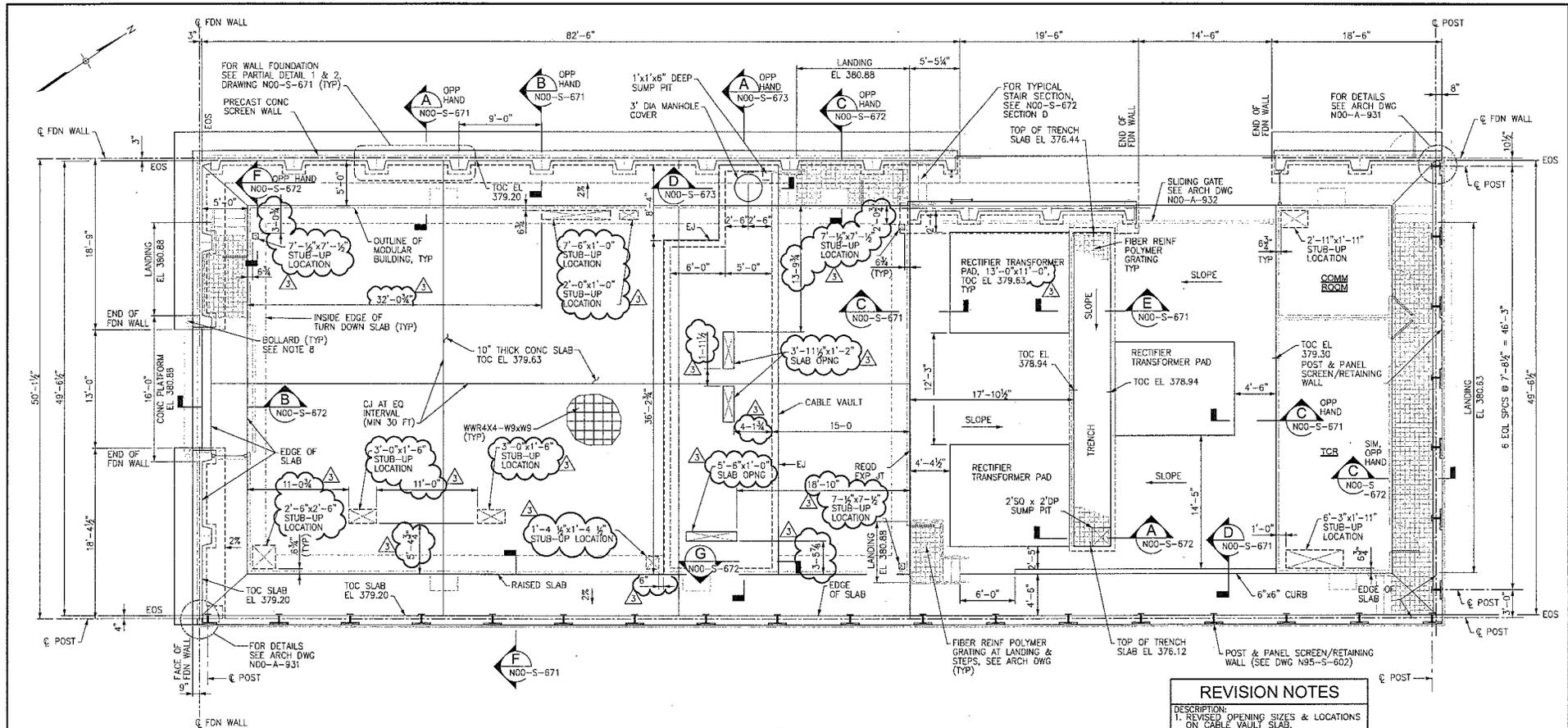
**EXTENSION TO WIEHLE AVE
 SUNSET HILL ROAD
 GRADING AND DRAINAGE PLAN
 TPSS #10/TCR #7**

SCALE: **1"=20'**

DRAWING NO.: **N95-GR-401**

CONTRACT NO.:

DESIGNER'S SIGNATURE AND SEAL ARE UNCONTROLLED AND MAY BECOME OBSOLETE. REFER TO THE ELECTRONIC APPROVALS IN IMPROVED FOR CURRENT PROBLEMS.



- NOTES:
- FOR GENERAL NOTES SEE DWGS. N00-S-021, S-022 & S-023.
 - FOR TYPICAL ABBREVIATIONS SEE DWGS. N00-S-031 & S-032.
 - MINIMUM COMPRESSIVE STRENGTH AT 28 DAYS, FOR ALL FOUNDATIONS AND CONC STRUCTURES ON THIS DRAWING, $f_c = 4,000$ PSI.
 - SUBGRADE SOIL BEARING CAPACITY SHALL BE REVIEWED BY CONTRACTOR'S GEOTECHNICAL ENGINEER. UNSUITABLE SOIL SHALL BE REPLACED AS DIRECTED. DESIGN BEARING CAPACITY $\leq 2,500$ PSF.
 - FOR REINFORCEMENT NEAR SLAB OPENINGS, SEE DETAIL 2 ON DWG N00-S-672.
 - GATE VENDOR TO PROVIDE SHOP DRAWINGS FOR GATE POST FOOTING DETAILS, SIGNED & SEALED BY A PROFESSIONAL ENGINEER REGISTERED IN COMMONWEALTH OF VIRGINIA, FOR BUILDING ANCHORING DETAIL, SEE DETAIL 1 ON DWG N00-S-672.
 - BOLLARD FOUNDATION TO TERMINATE JUST SHORT OF THE TOP OF THE WALL FOOTING, SEE DETAIL 6 ON DWG N00-A-631.

TPSS #10 FOUNDATION PLAN

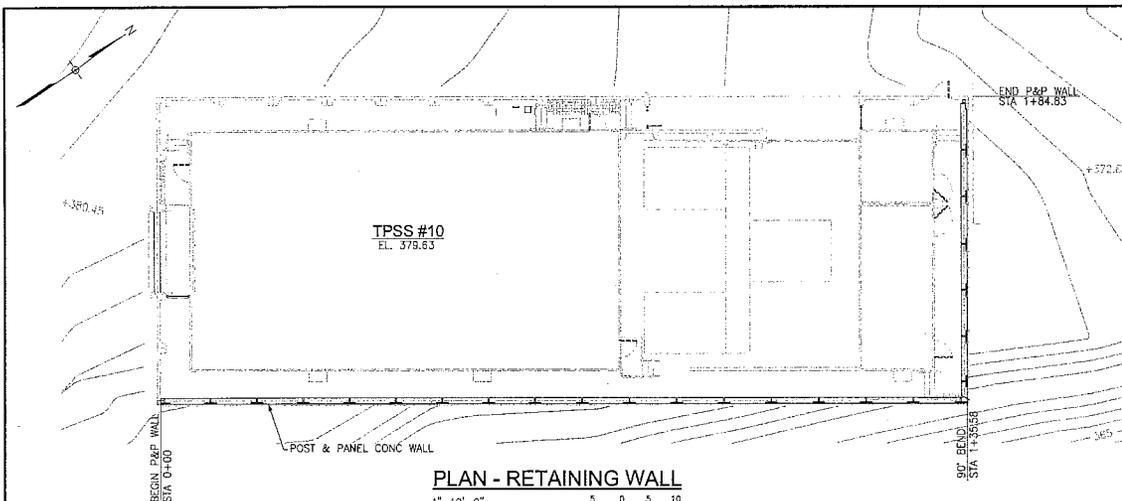
REVISION NOTES	
DESCRIPTION:	1. REVISED OPENING SIZES & LOCATIONS ON CABLE VAULT SLAB.
	2. REVISED SLAB OPENINGS TO STUB UP LOCATIONS WITH MINOR ADJUSTMENT TO SIZES.
	3. INCREASED TRANSFORMER PAD WIDTH BY 9".
CHANGE DOCUMENTS:	1. NONE.
COMMENTS:	1. ADJUSTMENTS TO ACCOMMODATE LATEST OPENINGS SHOWN ON POWER DWGS.
	2. STUB UP LOCATIONS NEED TO BE SHOWN AS CABLES FROM DUCT BANKS WILL BE INSTALLED PRIOR TO POURING SLAB.
	3. PROVIDE TOLERANCE TO INSTALL TRANSFORMER.

2/9/2011 2:07

N95-S-001.dwg

ORIGINATORS J.L. HART 0205/09 G. HE 0205/09 M. SIRCAR 0205/09 S. SIEBERT 0205/09		REFERENCE DRAWINGS N00-S-671 TPSS FOUNDATION - SECTIONS & DETAILS N00-A-671 TPSS #10 - ARCH FLOOR PLAN N00-P-671 TPSS #10 - ROADWAY PLAN N00-GR-40 TPSS #10 - GRADING & DRAINAGE PLAN N00-TP-301 TPSS #10 - EIGHT LAYOUT		REVISIONS NO. DATE DESCRIPTION 0 05/13/10 ISSUED FOR PERMIT 1 06/03/10 REM'D HOLD & REV'D AS NTD 2 08/22/10 REVISED AS NOTED 3 02/06/11 REVISED AS NOTED		WASHINGTON METROPOLITAN AREA TRANSIT AUTHORITY DGS PROJECT IDENTIFICATION CODE: 505-08505-690		CONTRACT NO.: EXTENSION TO WIEHLE AVE SUNSET HILLS ROAD STRUCTURAL - WAYSIDE BUILDING TPSS #10 FOUNDATION LAYOUT PLAN	
METROPOLITAN WASHINGTON AIRPORTS AUTHORITY DULLES TRANSIT PARTNERS, LLC 1205 SPRING HILL RD, STE 600 VIENNA, VA 22184				DULLES CORRIDOR METRORAIL PROJECT		SCALE: 3/16"=1'-0"		DRAWING NO.: N95-S-601	

Electronic copies were printed and are UNCONTROLLED and may become outdated. Refer to the electronic documents in AEPROFILES for current revisions.

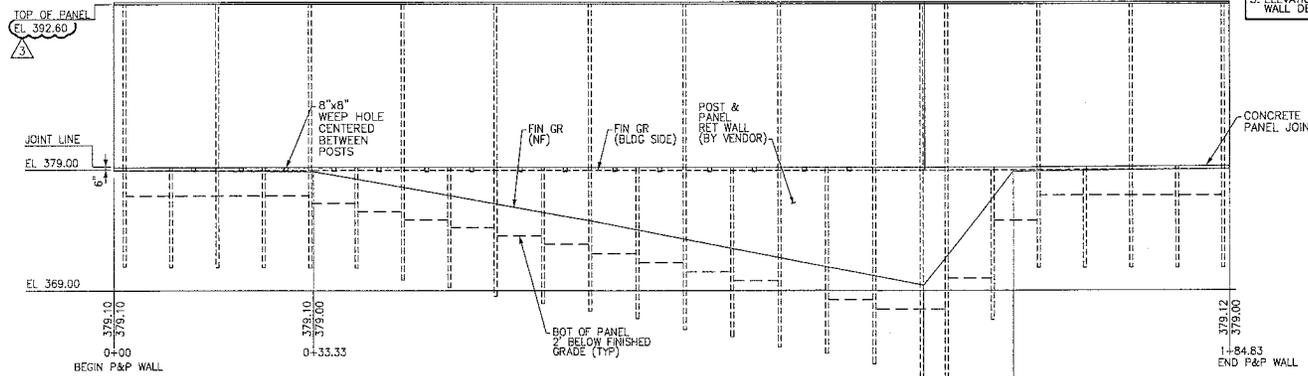


PLAN - RETAINING WALL

1"=10'-0"

NOTES:

1. FOR STATIONS & FACILITY GENERAL NOTES, SEE DWGS. N00-S-021, S-022 & S-023.
2. FOR ABBREVIATIONS SEE DWGS. N00-S-031 AND N00-S-032.
3. FOR STRUCTURAL GENERAL NOTES - RETAINING WALLS, SEE DWG N00-S-501.
4. DESIGN BEARING CAPACITY OF FOUNDATION IS 2,500 PSF.
5. FOR SURFACE WATER DRAINAGE, SEE CIVIL GRADING AND DRAINAGE PLANS.
6. FOR EXISTING AND PROPOSED UTILITIES, SEE UTILITY PLANS.
7. SITE CONTRACTOR TO PROVIDE ALL NECESSARY APPROVALS & PERMITS INCLUDING, BUT NOT LIMITED TO MAINTENANCE OF TRAFFIC MEASURES.
8. DESIGN OF POST & PANEL WALLS ARE PROVIDED BY WEIDLINGER ASSOCIATES, INC., SEE SHOP DWGS S-40 TO S-45, S-47 AND S-48 FOR DETAILS.
9. DESIGN OF POST & PANEL RETAINING WALL, WOODEN SCREEN WALL, CHAIN LINK FENCE AND ALL MOUNTING DETAILS (SHOWN AS GUIDANCE) TO BE PROVIDED BY SUB-CONTRACTOR SIGNED AND SEALED BY A PROFESSIONAL ENGINEER REGISTERED IN COMMONWEALTH OF VIRGINIA. POST & PANEL WALL TO BE DESIGNED IN ACCORDANCE WITH TECHNICAL SPECIFICATION 02493.
10. POST SPACINGS AND PANEL EMBEDMENTS IN THIS DWG ARE SHOWN FOR GENERAL GUIDANCE ONLY. POSTS SHOULD BE SPACED IN REGULAR INTERVALS NOT EXCEEDING 8.0 FEET WHEN RETAINING EARTH. PANELS TO BE EMBEDDED A MINIMUM OF 2.0 FEET BELOW FINISHED GRADE.



ELEVATION - RETAINING WALL

(LOOKING NORTHWEST)

H: 1"=10'-0"
V: 1"=20'-0"

LEGEND:

- FINISHED GRADE ELEVATION (FF)
- FINISHED GRADE ELEVATION (NF)
- STATION

REVISION NOTES

DESCRIPTION:

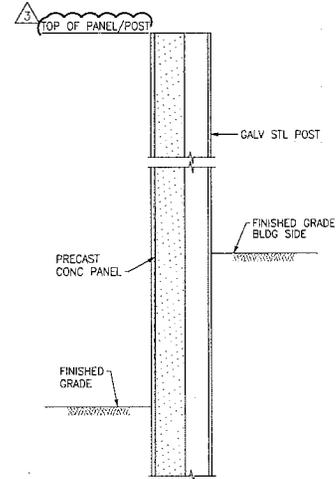
1. REMOVED HOLD FROM WALL ELEVATION.
2. REVISED NOTE #8.
3. TOP OF PANEL ELEVATION PROVIDED

CHANGE DOCUMENTS:

1. NONE

COMMENTS:

1. INCLUDED P&P WALL DESIGN ELEMENTS
2. NOTE #8 WAS REDUNDANT & HENCE DELETED. ADDED NEW NOTE TO DIRECT TO WALL SHOP DWGS. FOR DETAILS.
3. ELEVATION SHOWN PER P&P WALL DESIGN.



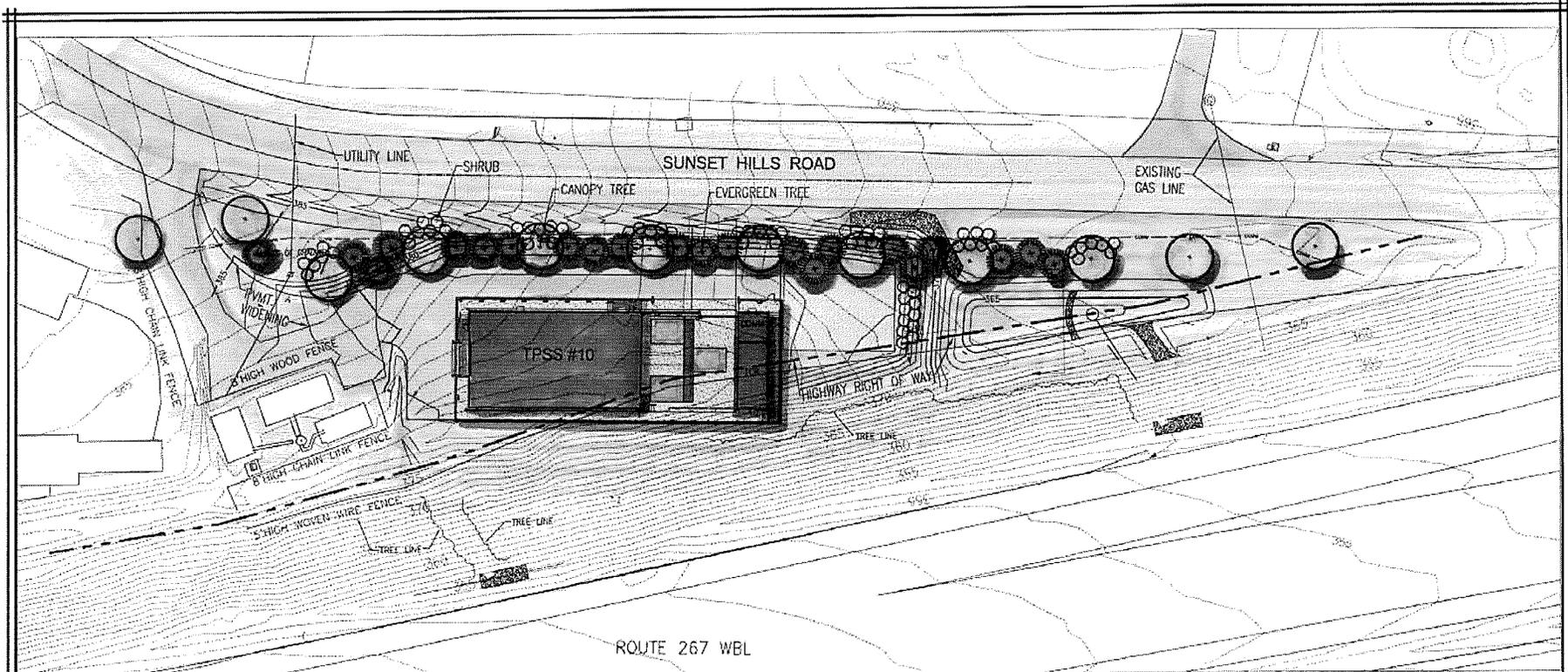
TYPICAL WALL SECTION
NOT TO SCALE

2/9/2011 2:05

N95-S-602.dgn

ORIGINATORS J.L. HART 02/05/09 Q. HE 02/06/09 M. SIRCAR 02/05/09 S. SEIBERT 02/05/09		REFERENCE DRAWINGS <table border="1"> <tr> <th>NUMBER</th> <th>DESCRIPTION</th> <th>NO.</th> <th>DATE</th> <th>DESCRIPTION</th> <th>ORIG</th> <th>CHK</th> <th>SUPV</th> <th>APPR</th> </tr> <tr> <td>N95-S-601</td> <td>TPSS#10 - FSN LAYOUT PLAN</td> <td>0</td> <td>06/13/10</td> <td>ISSUED FOR PERMIT</td> <td>DPR</td> <td>MS</td> <td>EA</td> <td>KS</td> </tr> <tr> <td>N95-S-611</td> <td>TPSS#10 - ARCH. FLY PLAN</td> <td>1</td> <td>09/16/10</td> <td>REV'D AS NOTED</td> <td>DPR</td> <td>MS</td> <td>EA</td> <td>KS</td> </tr> <tr> <td>N95-P-401</td> <td>TPSS#10 - ROADWAY PLAN</td> <td>2</td> <td>05/26/10</td> <td>REV'D AS NOTED</td> <td>DPR</td> <td>MS</td> <td>EA</td> <td>KS</td> </tr> <tr> <td>N95-GR-401</td> <td>TPSS#10 - GRADINGS & DRAINAGE PLAN</td> <td>3</td> <td>02/08/11</td> <td>REV'D AS NOTED-REISSUED FOR PERMIT</td> <td>DPR</td> <td>SP</td> <td>MS</td> <td>RP</td> </tr> <tr> <td>N95-TP-301</td> <td>TPSS#10 EQPT. LAYOUT</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> </table>		NUMBER	DESCRIPTION	NO.	DATE	DESCRIPTION	ORIG	CHK	SUPV	APPR	N95-S-601	TPSS#10 - FSN LAYOUT PLAN	0	06/13/10	ISSUED FOR PERMIT	DPR	MS	EA	KS	N95-S-611	TPSS#10 - ARCH. FLY PLAN	1	09/16/10	REV'D AS NOTED	DPR	MS	EA	KS	N95-P-401	TPSS#10 - ROADWAY PLAN	2	05/26/10	REV'D AS NOTED	DPR	MS	EA	KS	N95-GR-401	TPSS#10 - GRADINGS & DRAINAGE PLAN	3	02/08/11	REV'D AS NOTED-REISSUED FOR PERMIT	DPR	SP	MS	RP	N95-TP-301	TPSS#10 EQPT. LAYOUT								REVISIONS <table border="1"> <tr> <th>NO.</th> <th>DATE</th> <th>DESCRIPTION</th> <th>ORIG</th> <th>CHK</th> <th>SUPV</th> <th>APPR</th> </tr> <tr> <td>0</td> <td>06/13/10</td> <td>ISSUED FOR PERMIT</td> <td>DPR</td> <td>MS</td> <td>EA</td> <td>KS</td> </tr> <tr> <td>1</td> <td>09/16/10</td> <td>REV'D AS NOTED</td> <td>DPR</td> <td>MS</td> <td>EA</td> <td>KS</td> </tr> <tr> <td>2</td> <td>05/26/10</td> <td>REV'D AS NOTED</td> <td>DPR</td> <td>MS</td> <td>EA</td> <td>KS</td> </tr> <tr> <td>3</td> <td>02/08/11</td> <td>REV'D AS NOTED-REISSUED FOR PERMIT</td> <td>DPR</td> <td>SP</td> <td>MS</td> <td>RP</td> </tr> </table>		NO.	DATE	DESCRIPTION	ORIG	CHK	SUPV	APPR	0	06/13/10	ISSUED FOR PERMIT	DPR	MS	EA	KS	1	09/16/10	REV'D AS NOTED	DPR	MS	EA	KS	2	05/26/10	REV'D AS NOTED	DPR	MS	EA	KS	3	02/08/11	REV'D AS NOTED-REISSUED FOR PERMIT	DPR	SP	MS	RP			METROPOLITAN WASHINGTON AIRPORTS AUTHORITY 				CONTRACT NO.: 	
NUMBER	DESCRIPTION	NO.	DATE	DESCRIPTION	ORIG	CHK	SUPV	APPR																																																																																														
N95-S-601	TPSS#10 - FSN LAYOUT PLAN	0	06/13/10	ISSUED FOR PERMIT	DPR	MS	EA	KS																																																																																														
N95-S-611	TPSS#10 - ARCH. FLY PLAN	1	09/16/10	REV'D AS NOTED	DPR	MS	EA	KS																																																																																														
N95-P-401	TPSS#10 - ROADWAY PLAN	2	05/26/10	REV'D AS NOTED	DPR	MS	EA	KS																																																																																														
N95-GR-401	TPSS#10 - GRADINGS & DRAINAGE PLAN	3	02/08/11	REV'D AS NOTED-REISSUED FOR PERMIT	DPR	SP	MS	RP																																																																																														
N95-TP-301	TPSS#10 EQPT. LAYOUT																																																																																																					
NO.	DATE	DESCRIPTION	ORIG	CHK	SUPV	APPR																																																																																																
0	06/13/10	ISSUED FOR PERMIT	DPR	MS	EA	KS																																																																																																
1	09/16/10	REV'D AS NOTED	DPR	MS	EA	KS																																																																																																
2	05/26/10	REV'D AS NOTED	DPR	MS	EA	KS																																																																																																
3	02/08/11	REV'D AS NOTED-REISSUED FOR PERMIT	DPR	SP	MS	RP																																																																																																
DESIGNER M. SIRCAR		DATE 02/05/09		PROJECT IDENTIFICATION CODE: 505-08505-690		EXTENSION TO WIEHLE AVE SUNSET HILLS ROAD WAYSIDE BUILDING TPSS #10 RETAINING WALL PLAN & ELEVATION		SCALE: AS NOTED		DRAWING NO.: N95-S-602																																																																																												
CHECKER S. SEIBERT		DATE 02/05/09		DULLES CORRIDOR METRO RAIL PROJECT																																																																																																		
APPROVED 																																																																																																						

PROPOSED LOCATION LANDSCAPE PLAN



SUNSET HILLS ROAD EXISTING CONDITION

PROPOSED PLANTS

-  CANOPY TREE: WILLOW OAK
-  EVERGREEN TREES: FOSTERS HOLLIES AND AMERICAN HOLLIES
-  SHRUBS: NORTHERN BAYBERRY AND SCARLETTA LEUCOTHOE



METROPOLITAN WASHINGTON AIRPORTS AUTHORITY
 DULLES TRANSIT PARTNERS, LLC
TRANSIT PARTNERS, INC. 1000 WOODBURN AVENUE, SUITE 1000, WASHINGTON, DC 20004

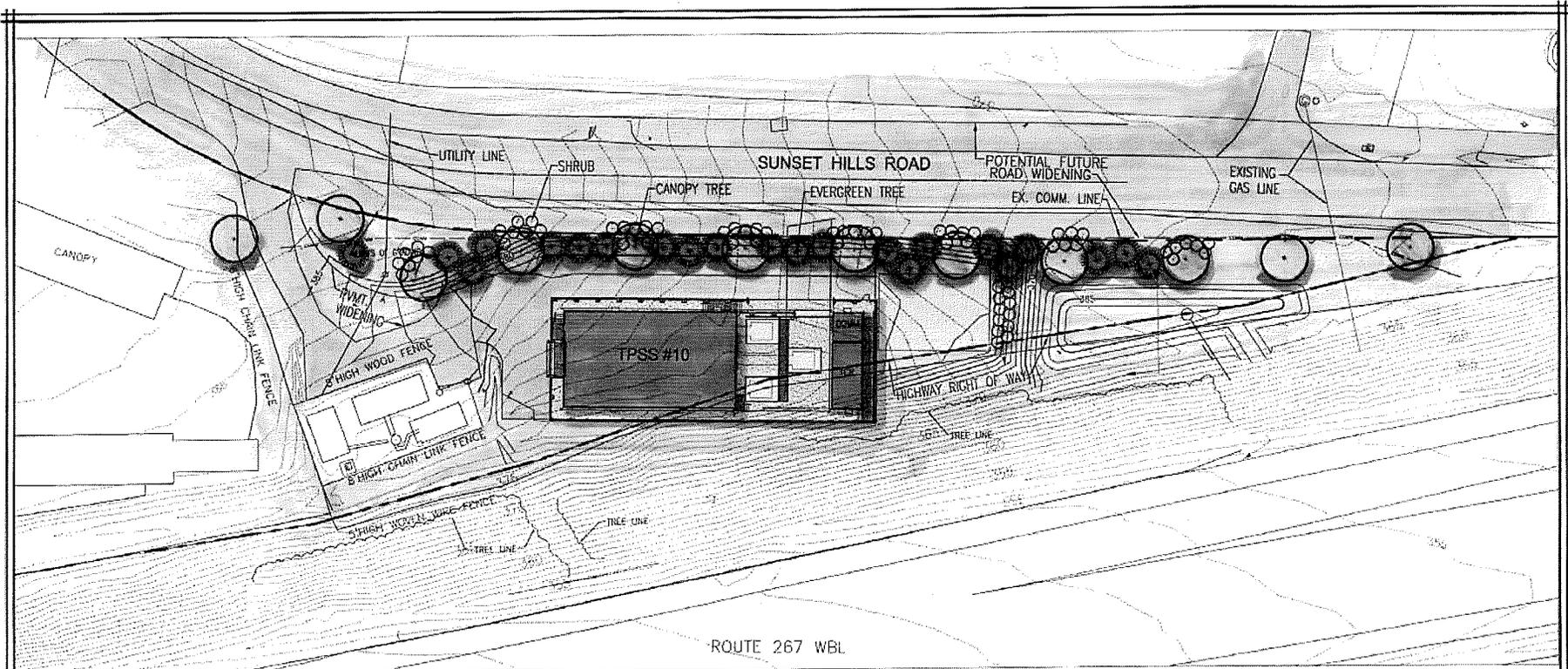


LANDSCAPE PLAN

EXTENSION TO WIEHLE AVE
 WAYSIDE BUILDING PACKAGE
 TPSS 010

JACOBS
 FEBRUARY 7, 2011
 SHEET 8

PROPOSED LOCATION DEPICTING ADDITIONAL TRAFFIC LANE ON SUNSET HILLS ROAD



SUNSET HILLS ROAD WIDENED

PROPOSED PLANTS

-  CANOPY TREE: WILLOW OAK
-  EVERGREEN TREES: FOSTERS HOLLIES AND AMERICAN HOLLIES
-  SHRUBS: NORTHERN BAYBERRY AND SCARLETTA LEUCOTHOE



Maier, Constance A.

From: Greenlief, Lori R. [lgreenlief@mcguirewoods.com]
Sent: Sunday, March 20, 2011 7:38 PM
To: Maier, Constance A.
Cc: Turpin, Frank; Hsu, Nancy; VanZee, James; Rak, Jonathan P.
Subject: TPSS#10
Attachments: Active_30074287_1_TPSS#10 graphic showing alternative location.PDF

Connie -

Per your email the following answers are provided for TPSS#10:

1. Photo Simulations: The photo simulations are being prepared and will be forwarded as soon as they are ready.
2. Environmental Impact: The facility will occupy approximately 0.5 acres of previously wooded area. This consists of a triangular piece of land adjacent to an existing cell tower and between the Toll Road and Sunset Hills Road. Stormwater runoff from the site currently runs to a drainage ditch along the north side of the Toll Road. Once the traction power substation is constructed, site drainage will flow to a detention basin before entering the Toll Road drainage system.
3. Residential Impact: One residence is located on the north side of Sunset Hills Road approximately 150 feet from the closest point of the screen wall surrounding proposed facility. The next closest residence is 900 feet to the northwest. The proposed facility will use an existing driveway for access to Sunset Hills Road. This existing driveway is currently used for access to a cellular phone monopole tower.
4. Graphic: Attached is an aerial photograph that was submitted with the application. The eastern red lined area is the proposed location. The western red lined area is the only other location offered by VDOT as an alternative. The yellow lined area in the middle of the two is the approved location. As stated in the Written Statement, the alternative was deemed not feasible because of the extended distance between the location and the tracks in the median of the DAAR, the need to run ductbanks and cables through the VDOT yard and potentially impact existing underground utilities and drainage lines, the lack of visibility from the location to the tracks, and the fact that the location is within VDOT's yard causing security concerns.

Please let me know if there is anything else you need to prepare your staff report.

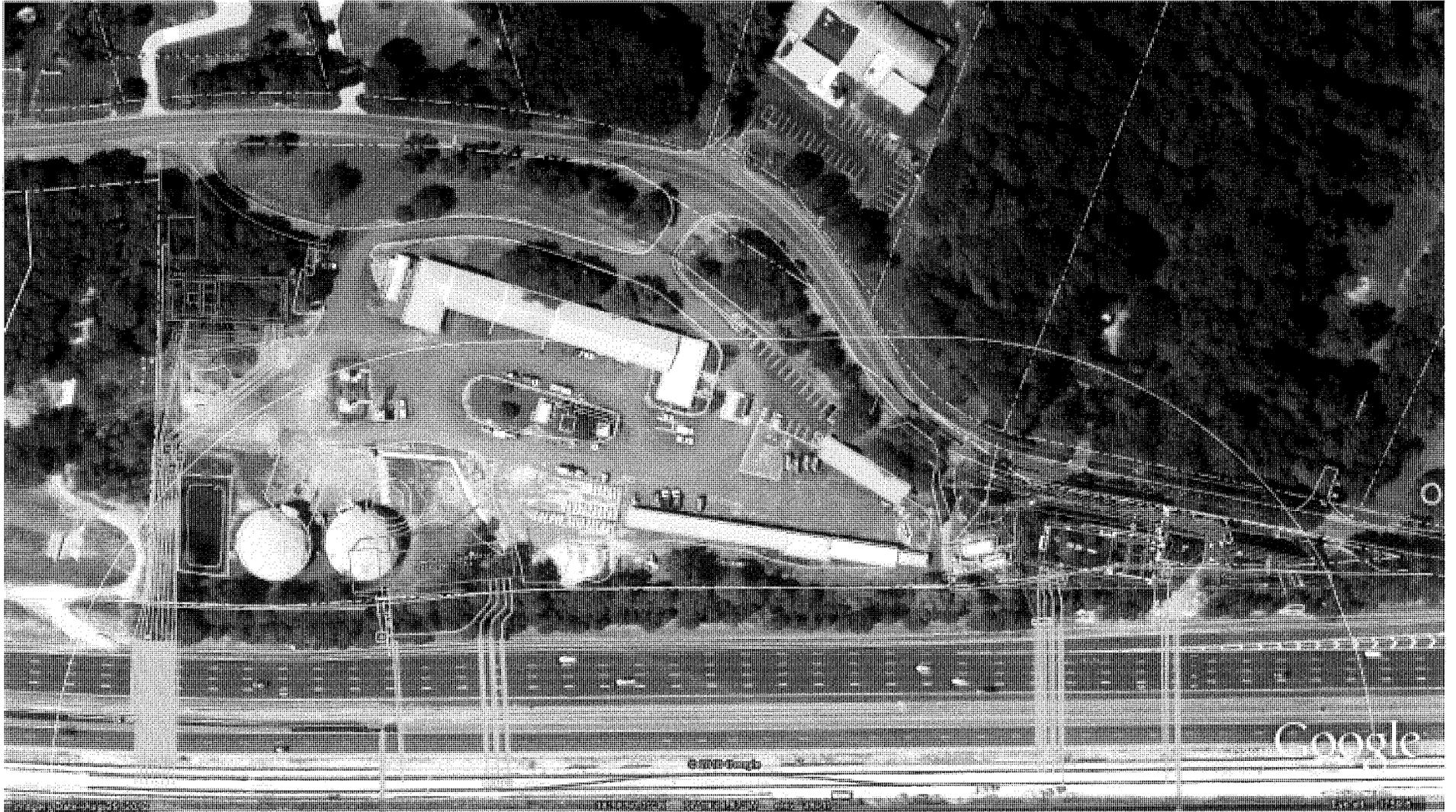
Lori

Lori R. Greenlief
Land Use Planner
McGuireWoods LLP

1750 Tysons Boulevard
Suite 1800
McLean, VA 22102-4215
703.712.5433 (Direct Line)
703.712.5050 (Direct FAX)
lgreenlief@mcguirewoods.com
<http://www.mcguirewoods.com>

This e-mail may contain confidential or privileged information. If you are not the intended recipient, please advise by return e-mail and delete immediately without reading or forwarding to others.

3/21/2011



RECEIVED
MAR 20 2011

MEMORANDUM

Fairfax County Department of Planning and Zoning
Planning Division, Suite 730
12055 Government Center Parkway, Fairfax, Virginia 22035-5505

TO: Distribution Below

DATE: February 28, 2011

FROM: Connie Maier, Planner
Facilities Planning Branch, Planning Division, DPZ

SUBJECT: 2232 Review Application
Application 2232A-MD06-10-1 TAX MAP: 18-3

Attached for your Review and Comment is a 2232 Review Application submitted by
MWATA requesting that the Fairfax County Planning Commission make a determination, pursuant to
Va. Code Section 15.2-2232, whether this proposal to change the location of a previously approved Traction
Power Substation and Train Control Room located on property between Sunset Hills Road and the Dulles
Airport Access road associated with the extension of Metrorail service through the Dulles Corridor at tax map
18-3 pt. is substantially in accord with provisions of the adopted Comprehensive Plan. To be considered in the
staff analysis, send, fax (703-324-3056), or e-mail (constance.maier@fairfaxcounty.gov) your comments to
(tel. 703-324-1263), Planning Division, DPZ by March 11, 2011.

Distribution:

- DPWES /
LDS - Environmental & Site Review Div.
LDS - Urban Forestry
SWM - Stormwater Planning Div.
WM - Wastewater Plan. & Monitoring Div.
DPZ /
Planning Div. - Env. & Dev. Review
Planning Div. - Historic Preservation
Zoning Administration Div. - Ord. Admin.
Zoning Evaluation Div. - RZ & SE Eval.
FCPA / Planning & Development Div.
FW / Planning & Engineering Div.
FCPS /
Property Management.
Facilities Planning
FCDOT / Transportation Planning Div.
Fire & Rescue / Strategic Planning
VDOT / District Land Development Mgr.



ZAD: Permitted in accordance with Section 2-516 of the Zoning Ordinance. The property is zoned
R-E District and the proposed facility meets Sect. 2-516.

Lorrie Kirst 2/28/11



County of Fairfax, Virginia

MEMORANDUM

DATE: 21 March 2011

TO: Connie Maier, Planner, Facilities Planning Branch

FROM: Linda Cornish Blank
Historic Preservation Planner *LCB*

SUBJECT: 2232A-MD06-10-1, between Sunset Hills Road & Dulles Airport Access Road, Tax Map 18-3 ((6)) 27; relocate a previously approved Traction Power Station and Train Control Room associated with Metrorail service.

Finding: The subject property is not included within the boundaries of a Fairfax County Historic Overlay District, is not listed on the Fairfax County Inventory of Historic Sites or the National Register of Historic Places or documented in the historic structures survey file. There are no properties in the immediate vicinity of the property which is the subject of this application that are within the boundaries of a Fairfax County Historic Overlay District, listed on the Fairfax County Inventory of Historic Sites or the National Register of Historic Places or documented in the historic structures survey file that would be negatively impacted by the proposed location of a traction power station and train control room.

Recommendation: No action concerning heritage resources is required.

Department of Planning and Zoning
 Planning Division
 12055 Government Center Parkway, Suite 730
 Fairfax, Virginia 22035-5509
 Phone 703-324-1380
 Fax 703-324-3056
www.fairfaxcounty.gov/dpz/



Maier, Constance A.

From: Bell, John
Sent: Wednesday, March 09, 2011 2:13 PM
To: Maier, Constance A.
Cc: Nee, Pamela
Subject: 2232A-MD06-10-1 Metrorail Traction power substation

Connie,

I've reviewed the above-noted 2232 application and there do not appear to be any significant environmental issues associated with the proposed use at this location.

Feel free to contact me should any questions, comments or concerns arise regarding this application.

*John R. Bell, Planner III
Fairfax County
Department of Planning & Zoning
(703) 324-1278*

MEMORANDUM

Fairfax County Department of Planning and Zoning

Planning Division, Suite 730

12055 Government Center Parkway, Fairfax, Virginia 22035-5505

TO: Distribution Below

DATE: February 28, 2011

FROM: Connie Maier, Planner
Facilities Planning Branch, Planning Division, DPZ

SUBJECT: 2232 Review Application
Application 2232A-MD06-10-1 TAX MAP: 18-3

Attached for your Review and Comment is a 2232 Review Application submitted by MWATA requesting that the Fairfax County Planning Commission make a determination, pursuant to Va. Code Section 15.2-2232, whether this proposal to change the location of a previously approved Traction Power Substation and Train Control Room located on property between Sunset Hills Road and the Dulles Airport Access road associated with the extension of Metrorail service through the Dulles Corridor at tax map 18-3 pt. is substantially in accord with provisions of the adopted Comprehensive Plan. To be considered in the staff analysis, send, fax (703-324-3056), or e-mail (constance.maier@fairfaxcounty.gov) your comments to (tel. 703-324-1263), Planning Division, DPZ by March 11, 2011.

Distribution:

- DPWES /
- LDS - Environmental & Site Review Div.
- LDS - Urban Forestry
- SWM - Stormwater Planning Div.
- WM - Wastewater Plan. & Monitoring Div.
- FW / Planning & Engineering Div.
- FCPS /
- Property Management.
- Facilities Planning
- DPZ /
- Planning Div. - Env. & Dev. Review
- Planning Div. - Historic Preservation
- Zoning Administration Div. - Ord. Admin.
- Zoning Evaluation Div. - RZ & SE Eval.
- FCPA / Planning & Development Div.
- FCDOT / Transportation Planning Div.
- Fire & Rescue / Strategic Planning
- VDOT / District Land Development Mgr.

Not subject to special exception. KG 3/9/11

- ZAD: Permitted in accordance with Section 2-516 of the Zoning Ordinance.** The property is zoned R-E District and the proposed facility meets Sect. 2-516.

Lorrie Kirst 2/28/11

RECEIVED
Department of Planning & Zoning
MAR 07 2011
Zoning Evaluation Division

Maier, Constance A.

From: Regmi, Sharad
Sent: Tuesday, March 22, 2011 1:59 PM
To: Maier, Constance A.
Cc: Nassimbeni, Bruce G.; Baig, Shahab; Stonefield, Jerry
Subject: 2232 Review Application # 2232A-MD06-10-1 for WMATA : Tax Map # 18-3-((01))-0007A : 6511 - ZONA-001-1

Connie,

Proposed construction is within a Commonwealth of Virginia parcel. Typically, we do not receive construction plan for review within a state owned property.

If you need further information, please contact me.

Sharad Regmi, P.E.
Stormwater and Geotechnical Section
DPWES - LDS
X324-7179



County of Fairfax, Virginia

MEMORANDUM

March 22, 2011

TO: Connie Maier, Planner
Planning Division, DPZ

FROM: Hugh Whitehead, Urban Forester II *HW*
Forest Conservation Branch, DPWES

SUBJECT: Dulles Corridor Metrorail Project, MWATA, 2232A-MD06-10-1_TAX

I have reviewed the above referenced 2232 Application, stamped as received by the Planning Division, DPZ on February 17, 2011. The following comments are based on this review and a site visit conducted on March 22, 2011.

- Comment:** The proposed landscape plan shows northern bayberry and scarlet leucothoe for shrub planting in the transitional screening yard along Sunset Hills Road. Northern bayberry requires full sun. Scarlet leucothoe grows to only about 2 to 3 feet tall, so is not effective as a screen. Shrubs between Sunset Hills Road and proposed trees will need to be tolerant of shade to perform well beneath proposed large deciduous trees and grow to at least 5-6 feet tall to effectively screen the site. Shrubs could be planted on the site side of proposed deciduous trees where they would get more sun and would still be in a position to screen the substation.

Recommendation: Request selection of shade tolerant evergreen or semi-evergreen shrubs that will grow to 5-6 feet tall. Suggested species include Schipkaensis cherry laurel (*Prunus laurocerasus*, 'Schipkaensis'), and glossy abelia (*Abelia x grandiflora*). If desired, use less shade tolerant species on the site side of the screening yard.

- Comment:** Potential Sunset Hills Road widening would result in the loss of a significant number of the shrubs proposed for the screening yard. Appropriate shrubs could be planted further from the road and retained with potential road widening.

Recommendation: Show proposed shrubs located further from the road. Shrubs could also be planted on both sides of the screening yard as suggested above.

If there are any questions, please contact me at (703)324-1770.

HCW/
UFMID #: 159580

cc: RA File
DPZ File

Department of Public Works and Environmental Services
Land Development Services, Urban Forest Management Division
12055 Government Center Parkway, Suite 518
Fairfax, Virginia 22035-5503
Phone 703-324-1770, TTY: 703-324-1877, Fax: 703-803-7769
www.fairfaxcounty.gov/dpwes



Maier, Constance A.

From: Stone, Laurie A.
Sent: Friday, March 11, 2011 10:52 AM
To: Maier, Constance A.
Subject: MWATA 2232 Application

Constance,

FRD has no issues with the WMATA 2232 application, 2232A-MD06-10-1.

Laurie Stone
Strategic Planner
Fairfax County Fire and Rescue Department
4100 Chain Bridge Road
Fairfax, VA 22030
Phone: 703-246-3889 Fax: 703-273-1049

3/11/2011



County of Fairfax, Virginia

MEMORANDUM

DATE: March 16, 2011

TO: Chris Caperton, Chief
Facilities Planning Branch, DPZ

FROM: Leonard Wolfenstein, Chief
Transportation Planning Section
Department of Transportation *L.W.*

FILE: 10-5

SUBJECT: Application for 15.2-2232 determination – 2232A-MD06-10-1 MWAA relocation of a previously approved Traction Power Substation and Train Control Room located on the south side of Sunset Hills Road between the WO&D Trail and Hunter Mill Road.

The Fairfax County Department of Transportation (FCDOT) has reviewed the above 2232 application and has the following comments:

The Fairfax County Transportation Plan Map shows a planned widening of Sunset Hills Road, to a 4-lane section, in the vicinity of the planned Traction Power Substation and Train Control Room. However, this application states, and the included site plans appear to show, that the site has been designed to provide adequate right-of-way to allow for this planned widening.

The Fairfax County Transportation Plan Map also shows a full interchange improvement for the Hunter Mill Road/Dulles Toll Road interchange. In 2000, a number of conceptual interchange designs were developed for this improvement. Several community meetings were held on these design options. Based on this community outreach, a preferred option was identified and is referenced in the Comprehensive Plan as follows:

“The preferred Option A is to slightly modify the current Sunset Hills Road intersection by moving it opposite the Dulles Toll Road westbound off-ramp and relocate the Toll Road westbound on-ramp to begin at Sunset Hills Road west of Hunter Mill Road and east of the existing VDOT Maintenance Facility” (p. 170).

It appears that the proposed location of the Traction Power Substation would require a modification to this design. It should be noted that there were other feasible design concepts for the interchange among those developed in 2000. At this time, there is no funding or prospect of funding in the near or medium term for an interchange improvement. However, given the projected growth in the corridor, an interchange improvement will be needed in the longer term.

Fairfax County Department of Transportation
4050 Legato Road, Suite 400
Fairfax, VA 22033-2895
Phone: (703) 877-5600 TTY: 711
Fax: (703) 877 5723
www.fairfaxcounty.gov/fcdot



March 16, 2011
Page 2 of 2

The Countywide Trails Plan shows a minor paved trail along the south side of Sunset Hills Road. Adequate right-of-way should be preserved to allow for the implementation of this trail in the future. The construction of the Sunset Hills Road Traction Power Substation and Train Control Room is expected to have minimal impact on the surrounding road network. Sunset Hills Road, which provides the only vehicular access to the site, has capacity to handle any increase in trips associated with the proposed use. Given the unmanned nature of Traction Power Substations and Train Control Rooms, Fairfax Connector bus service to the site is unnecessary.

Due to the overall nature of Traction Power Substations and Train Control Rooms, there appears to be no other significant traffic impacts resulting from the proposed use for the site. Additionally, the proposed Traction Power Substation and Train Control Room does not have any design features associated with it that would involve any VDOT road design standards.

If you have any questions please feel free to contact Kris Morley-Nikfar (tel. 703-877-5672) Kris.Morley-Nikfar@fairfaxcounty.gov.

Cc: Constance Maier, DPZ
Angela Rodeheaver, FCDOT
Karyn Moreland, FCDOT
Charlie Strunk, FCDOT
Kris Morley-Nikfar, FCDOT
Jaak Pedak, FCDOT
Randy White, FCDOT
Chris Wells, FCDOT



FAIRFAX COUNTY WATER AUTHORITY
8560 Arlington Boulevard, Fairfax, Virginia 22031
www.fairfaxwater.org

PLANNING & ENGINEERING
DIVISION
Jamie Bain Hedges, P.E.
Director
(703) 289-6325
Fax (703) 289-6382

March 14, 2011

Ms. Connie Maier
Planner - Facilities Planning Branch
Fairfax County Department of Planning & Zoning
12055 Government Center Parkway, Suite 730
Fairfax, Virginia 22035

Re: Application No. 2232A-MD06-10-1
Traction Power Substations
Tax Map: 18-3

Dear Ms. Maier:

The following information is submitted in response to your request for comments on the above referenced application:

1. The proposed site can be served by Fairfax Water.
2. Adequate domestic water service and fire protection is available at the site from an existing 12-inch diameter water main in Sunset Hills Road. See the enclosed water system map.
3. Depending upon the configuration of the on-site water mains, additional water main extensions may be necessary to satisfy fire flow requirements and accommodate water quality concerns.

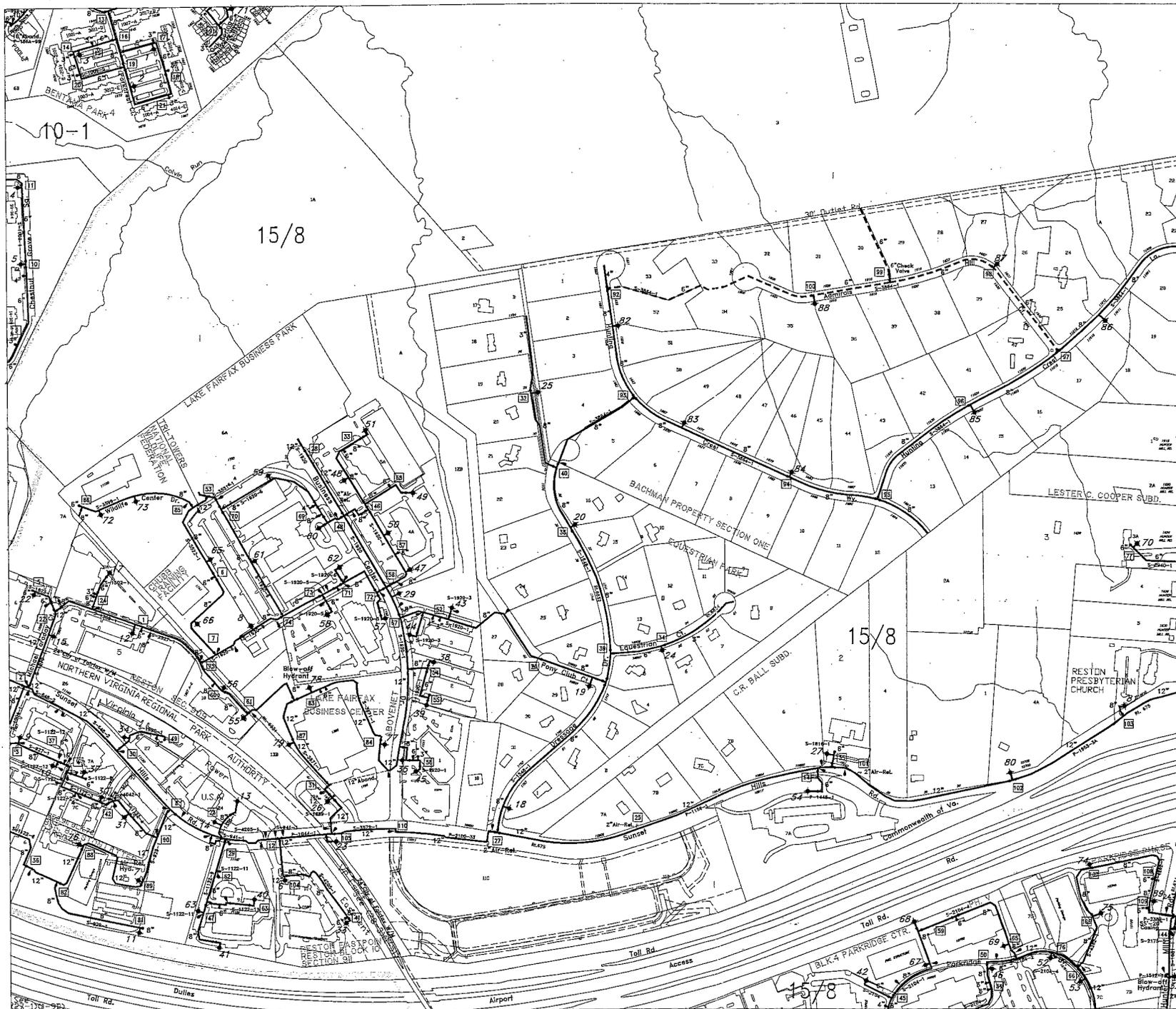
If you have any questions regarding this information please contact Dave Guerra at (703) 289-6343.

Sincerely,

A handwritten signature in cursive script that reads "Traci K. Goldberg".

Traci K. Goldberg, P.E.
Manager, Planning

Enclosure

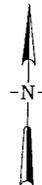


Fairfax Water
Planning and Engineering Division
Technical Services Branch

PIPE MATERIAL LEGEND

- A.C.P.
- C.I.P.
- D.I.P.
- P.C.C.P.
- STEEL
- COPPER
- PLASTIC
- GALVANIZED
- - - APPROVED CONST.

- XX AS-BUILT COMPLETE
- XX AS-BUILT INCOMPLETE
- XX AS-BUILT NOT DRAWN



CONTOUR INTERVAL 5-FOOT

17-2	18-1	18-2
17-4	18-3	18-4
26-2	27-1	27-2

FAIRFAX COUNTY
SECTION
18-3



The information contained on this page is NOT to be construed or used as a "legal description". Fairfax Water does not provide any guaranty of accuracy or completeness regarding the map information. Any errors or omissions should be reported to the Technical Services Branch of the Planning and Engineering Division. In no event will Fairfax Water be liable for any damages, including but not limited to loss of data, lost profits, business interruption, loss of business information or any other pecuniary loss that might arise from the use of this map or information it contains.

MARCH 2011

§ 15.2-2232. Legal status of plan.

A. Whenever a local planning commission recommends a comprehensive plan or part thereof for the locality and such plan has been approved and adopted by the governing body, it shall control the general or approximate location, character and extent of each feature shown on the plan. Thereafter, unless a feature is already shown on the adopted master plan or part thereof or is deemed so under subsection D, no street or connection to an existing street, park or other public area, public building or public structure, public utility facility or public service corporation facility other than a railroad facility or an underground natural gas or underground electric distribution facility of a public utility as defined in subdivision (b) of § 56-265.1 within its certificated service territory, whether publicly or privately owned, shall be constructed, established or authorized, unless and until the general location or approximate location, character, and extent thereof has been submitted to and approved by the commission as being substantially in accord with the adopted comprehensive plan or part thereof. In connection with any such determination, the commission may, and at the direction of the governing body shall, hold a public hearing, after notice as required by § 15.2-2204. Following the adoption of the Statewide Transportation Plan by the Commonwealth Transportation Board pursuant to § 33.1-23.03 and written notification to the affected local governments, each local government through which one or more of the designated corridors of statewide significance traverses, shall, at a minimum, note such corridor or corridors on the transportation plan map included in its comprehensive plan for information purposes at the next regular update of the transportation plan map. Prior to the next regular update of the transportation plan map, the local government shall acknowledge the existence of corridors of statewide significance within its boundaries.

B. The commission shall communicate its findings to the governing body, indicating its approval or disapproval with written reasons therefor. The governing body may overrule the action of the commission by a vote of a majority of its membership. Failure of the commission to act within sixty days of a submission, unless the time is extended by the governing body, shall be deemed approval. The owner or owners or their agents may appeal the decision of the commission to the governing body within ten days after the decision of the commission. The appeal shall be by written petition to the governing body setting forth the reasons for the appeal. The appeal shall be heard and determined within sixty days from its filing. A majority vote of the governing body shall overrule the commission.

~~C. Widening, narrowing, extension, enlargement, vacation or change of use of streets or public areas shall likewise be submitted for approval, but paving, repair, reconstruction, improvement, drainage or similar work and normal service extensions of public utilities or public service corporations shall not require approval unless such work involves a change in location or extent of a street or public area.~~

D. Any public area, facility or use as set forth in subsection A which is identified within, but not the entire subject of, a submission under either § 15.2-2258 for subdivision or subdivision A 8 of § 15.2-2286 for development or both may be deemed a feature already shown on the adopted master plan, and, therefore, excepted from the requirement for submittal to and approval by the commission or the governing body; provided, that the governing body has by ordinance or resolution defined standards governing the construction, establishment or authorization of such public area, facility or use or has approved it through acceptance of a proffer made pursuant to § 15.2-2303.

E. Approval and funding of a public telecommunications facility by the Virginia Public Broadcasting Board pursuant to Article 12 (§ 2.2-2426 et seq.) of Chapter 24 of Title 2.2 shall be deemed to satisfy the requirements of this section and local zoning ordinances with respect to such facility with the exception of television and radio towers and structures not necessary to house electronic apparatus. The exemption provided for in this subsection shall not apply to facilities existing or approved by the Virginia Public Telecommunications Board prior to July 1, 1990. The Virginia Public Broadcasting Board shall notify the governing body of the locality in advance of any meeting where approval of any such facility shall be acted upon.

F. On any application for a telecommunications facility, the commission's decision shall comply with the requirements of the Federal Telecommunications Act of 1996. Failure of the commission to act on any such application for a telecommunications facility under subsection A submitted on or after July 1, 1998, within ninety days of such submission shall be deemed approval of the application by the commission unless the governing body has authorized an extension of time for consideration or the applicant has agreed to an extension of time. The governing body may extend the time required for action by the local commission by no more than sixty additional days. If the commission has not acted on the application by the end of the extension, or by the end of such longer period as may be agreed to by the applicant, the application is deemed approved by the commission.

(Code 1950, §§ 15-909, 15-923, 15-964.10; 1958, c. 389; 1960, c. 567; 1962, c. 407, § 15.1-456; 1964, c. 528; 1966, c. 596; 1968, c. 290; 1975, c. 641; 1976, c. 291; 1978, c. 584; 1982, c. 39; 1987, c. 312; 1989, c. 532; 1990, c. 633; 1997, cc. 587, 858; 1998, c. 683; 2007, c. 801; 2009, cc. 670, 690.)