

1 REPLACE: Fairfax County Comprehensive Plan, 2017 Edition, Area II Volume, McLean Planning
2 District, amended through 7-31-2018, McLean Community Business Center, with the following:

3
4 **MCLEAN COMMUNITY BUSINESS CENTER**

5
6 **LOCATION AND CHARACTER**

7
8 The McLean Community Business Center (CBC) is approximately 230 acres in size. The
9 CBC is centered around the intersection of two major roadways, Chain Bridge Road and Old
10 Dominion Drive, as shown on the Locator Map, Figure 1. The triangular land area bounded by Old
11 Dominion Drive, Dolley Madison Boulevard, and Chain Bridge Road is in the M4 – Balls Hill
12 Community Planning Sector, while the rest of the CBC is in the M3 – Kirby Community Planning
13 Sector.

14
15 The CBC contains several neighborhood-serving shopping centers that are accessible from
16 Chain Bridge Road or Old Dominion Drive. Dispersed between these centers are commercial uses
17 that include automobile service stations, banks, restaurants, and former residences converted to
18 professional offices or small retail establishments. The converted offices or retail uses are located
19 in the west and southwest portions of the CBC, primarily along Ingleside Avenue, the north side of
20 Chain Bridge Road between Buena Vista Avenue and Pathfinder Lane, and the south side of
21 Whittier Avenue. A major concentration of professional offices is located along Elm Street and
22 Beverly Road, and along Lowell and Whittier Avenues, between Laughlin Avenue and Old
23 Dominion Drive. Professional office complexes are also located at Curran Street and Chain Bridge
24 Road and Old McLean Village Drive and Chain Bridge Road. The McLean Professional Park is
25 located in the southwestern portion of the CBC at Chain Bridge Road and Tennyson Drive.

26
27 Existing residential uses include mid- to high-rise multifamily, townhouses, and single-
28 family detached residences. Multi-family residential developments are located along Fleetwood
29 Road, Beverly Road, Laughlin Avenue, and Lowell Avenues. Residential townhome communities
30 are generally located along the edges of the CBC. Single-family residential uses and parkland
31 surround the CBC and include the McLean Central Park, Lewinsville Park, Bryn Mawr Park, and
32 Salona Park.

33
34 The CBC is located within two miles of the Tysons Urban Center (Tysons). In order to
35 preserve the McLean CBC’s identity as a community-serving business district, it is planned to
36 provide for the needs of the immediate surrounding community and not the regional needs at the
37 scale found in Tysons. Community-serving uses such as retail, commercial, and medical and
38 professional offices should continue to be accommodated under the CBC Plan. In addition, the
39 CBC is expected to include a variety of housing types. Mixed-use development is envisioned
40 towards the center of the CBC, with lower density development towards the edges that are of a
41 compatible scale to existing neighborhoods and other low intensity uses.

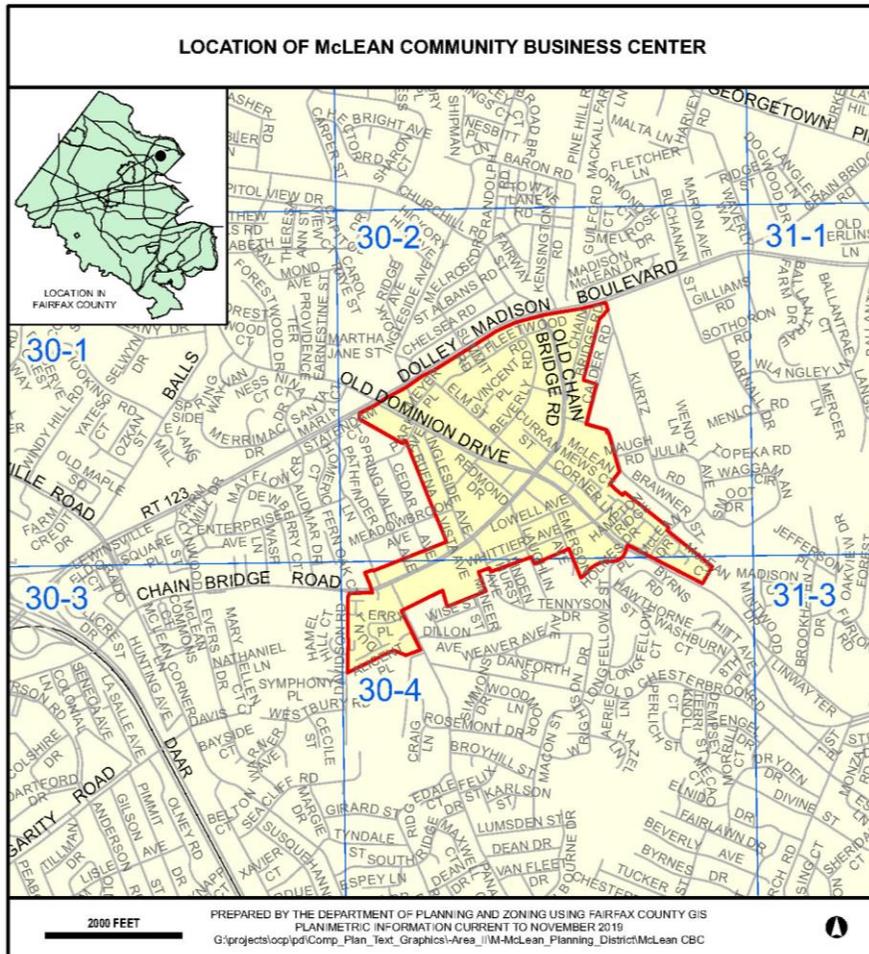


FIGURE 1

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 45
 46 The proximity of Tysons to the CBC causes some spillover traffic in the CBC and in adjacent
 47 residential neighborhoods, particularly during peak hours. Access from the CBC to Dolley
 48 Madison Boulevard during peak hours is hindered. At present, most internal traffic within the CBC
 49 must use the central intersection of Old Dominion Drive, Chain Bridge Road, and Elm Street. The
 50 capacity of this intersection is limited. While some improvements can be made by providing
 51 alternative routes that reduce the number of cars that must use that intersection, no alternative exists
 52 for through-traffic. Additional anticipated growth in Tysons is likely to add to this traffic problem.

53 A clear distinction between the character and scale of development in the CBC and Tysons is not
54 only consistent with the vision of the CBC as a community center, but also recognizes the need to
55 not overload the traffic circulation network.

56
57 **REVITALIZATION CONSIDERATIONS**

58
59 The Board’s revitalization policy supports programs and initiatives that seek to sustain the
60 economic vitality and quality of life in older commercial centers and adjacent neighborhoods. The
61 goal is to improve the economic climate and encourage private and public investment and
62 reinvestment in these areas. On October 12, 1998, the Board of Supervisors (Board) designated
63 the McLean Commercial Revitalization District (CRD). The boundary of the CRD coincides with
64 the boundary of the CBC. A CRD is a zoning overlay district that provides specific regulations that
65 are designed to facilitate the continued viability and redevelopment of designated areas. The
66 districts provide additional flexibilities for development while also providing for urban design
67 measures such as streetscape and landscaping.

68
69 **PLAN HISTORY**

70
71 The first McLean Central Area Plan was adopted in 1970 to provide an attractive shopping,
72 working, and living environment. The objective of the Plan was to encourage the stability of
73 surrounding residential neighborhoods while promoting the success of downtown McLean as a
74 community shopping district.

75
76 In 1988, the McLean Central Business District Study was completed by consultants for
77 Fairfax County and the McLean Planning Committee. The primary purpose of the study was to
78 establish parameters for new development; to identify ways to continue the existing service
79 functions of downtown McLean; and, to address the perception that McLean lacks an identity and
80 attractiveness which sets it apart as a community. The study’s recommendations were incorporated
81 into the Comprehensive Plan as part of the Fairfax Planning Horizons process in 1991 at which
82 time the study area became known as the McLean Community Business Center (CBC). The study’s
83 public space and building design guidance became part of the Plan by reference.

84
85 In 1997, a series of public meetings and design charettes were sponsored by the county and
86 the McLean Planning Committee to review the goals and objectives of the local residents,
87 landowners, and business owners for the purpose of revitalizing the McLean CBC. This process
88 culminated in a series of plans and recommendations identified in the document entitled “McLean,
89 A Vision for the Future.”

90
91 On October 27, 1997, the Board authorized a McLean CBC Special Study to consider changes
92 to the Comprehensive Plan that would support community revitalization efforts. This effort
93 resulted in the development of a vision plan. As part of the implementation of the Plan, the McLean
94 Revitalization Corporation was formed in 1998 to facilitate public, private, and volunteer efforts in
95 the development and execution of the Plan. A Comprehensive Plan amendment was adopted in
96 1998.

97
98 The most recent land use planning effort began in 2018, when the Board authorized a
99 Comprehensive Plan amendment to review the recommendations of the McLean CBC and consider
100 land use, transportation, and urban design alternatives. Staff worked with a consultant and a

101 community task force to develop a vision for the McLean CBC and to review and develop new land
102 use, transportation, parks and open space, and urban design recommendations. The
103 recommendations include concentrating the most intensity in the center of McLean, the
104 introduction of additional residential uses in the CBC, a form-based approach to development in
105 most of the CBC, guidance for an urban park network, and conceptual multimodal street cross-
106 sections.

107
108 **CONCEPT FOR FUTURE DEVELOPMENT**

109
110 The countywide Concept for Future Development defines CBCs as older community-
111 serving commercial areas that emerged along major roadways. Redevelopment in CBCs is
112 recommended to include a higher intensity mix of uses focused in a defined core area such as a
113 town center or a main street. Site design in CBCs should prioritize the pedestrian experience which
114 includes the provision of active ground floor uses such as retail, and achieving the recommended
115 streetscape guidance, and block sizes that foster a walkable environment.

116
117 Transitions in intensity and compatible land uses should protect surrounding stable single
118 family detached residential neighborhoods. Redevelopment and revitalization efforts are
119 recommended to sustain the economic vitality of these commercial centers. These efforts should
120 also seek reinvestment in strategic locations and aim to foster a sense of place.

121
122 **VISION AND GUIDING PLANNING PRINCIPLES**

123
124 The community’s vision for the McLean CBC is to sustain and enhance its legacy function
125 as a community-serving business area, ~~by while~~ encouraging quality mixed-use redevelopment and
126 other revitalization efforts that support vibrancy, walkability, public infrastructure, open space and
127 public parks, and other improvements. The plan incentivizes redevelopment by offering flexibility
128 in land uses and intensity guided by a form-based plan for the majority of the CBC. Planned non-
129 residential intensities and residential densities along the edges of the CBC will provide transitions
130 in scale, mass, and height to adjacent single-family residential neighborhoods. The vision
131 contemplates vibrant places and a diversity of land uses with inviting street level facades primarily
132 in the form of mid-to-high-rise buildings concentrated mostly towards the central portion of the
133 CBC. Buildings particularly in this portion of the CBC are envisioned to support a pedestrian-
134 oriented environment by being located close to the sidewalk, with little to no surface parking
135 between buildings and the street. In some circumstances, redevelopment may incorporate long-
136 standing commercial uses, including some surface parking, especially to serve retail uses. A
137 signature urban park is expected to be a major placemaking element in the center of the CBC.

138
139 **Planning Principles**

140
141 In addition to the guidance provided above, the following planning principles are intended to
142 guide future development in the McLean CBC.

- 143
144 • Encourage revitalization and redevelopment that creates attractive community-serving
145 commercial and mixed-use areas, ~~with the inclusion of residential uses in mixed-use areas.~~

Commented [GJ1]: Staff is drafting an additional principle about parking in response to task force discussion and community feedback related to parking recommendations

- 146 • Common urban design elements should be incorporated for sites that have frontage along the
147 same street to provide a sense of continuity and cohesion.

- 148 • Design streets to provide safe, convenient, and attractive travel for pedestrians and bicyclists,
149 and streetscapes should have a unified theme and appearance.

- 150 • Locate a signature urban park that can support community events in the center of the CBC and
151 provide other urban parks throughout the CBC.

- 152 • Encourage public art in public spaces and as part of redevelopment efforts to help foster a sense
153 of place and community identity.

- 154 • Create a sense of place ~~through focused redevelopment in the center of~~ throughout the CBC.

- 155 • Preserve the stability of adjacent single-family detached residential areas by establishing well-
156 designed transitional areas at the edges of the CBC.

- 157 • Provide housing affordable to a range of income levels.

- 158 • Encourage the retention of existing local businesses.

- 159 • Minimize adverse impacts to the natural environment and water and air quality by using best
160 practices in stormwater management, natural resource conservation, and site design.

- 161 • Identify heritage resources through surveys and research and consider mitigation of impacts on
162 resources during redevelopment.

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173 Zones

174
 175 The land use plan for the McLean CBC is organized into the three zones - Center, General,
 176 and Edge, as show in Figure 2. The purpose of the zones is to establish a framework for
 177 development that identifies distinguishing characteristics for each zone in terms of building
 178 intensity, form and character as well as for land uses and site design. The highest intensity and
 179 tallest buildings are expected in the Center Zone; development is expected to be primarily mid-rise
 180 buildings in the General Zone; and the existing character and uses in the Edge Zone are expected
 181 to be retained.

182
 183 Each zone is divided into land units, as described in the Land Units section. For each land
 184 unit, the preferred vision is recommended as an optional level of development above the base plan.
 185 The preferred vision in the Center and General Zones employs a form-based approach that
 186 encourages flexibility in the mix of uses within a total amount of development potential for those
 187 zones, as shown in Figure 3. The preferred vision for the Edge Zone employs dwelling units per
 188 acre (du/ac) or floor area ratio (FAR) recommendations.

189
 190 While the zones themselves establish a tiered development pattern, transitions between the
 191 zones should be considered, and special care taken to ensure that appropriate transitions are made
 192 when properties abut the boundary of the CBC and for adjacent single-family ~~detached~~ residential
 193 uses. Appropriate transitions may include the use of building setbacks and step-backs, screening
 194 and buffering, and other techniques that achieve appropriate transitions.

195
 196 *Center Zone and General Zone Form-Based Approach*

197
 198 Implementation of the Plan options in the Center and General Zones follow a form-based
 199 approach that uses building form, design, and height to inform the development potential of
 200 properties in lieu of a maximum FAR or du/ac. A maximum total development potential for these
 201 zones is recommended in terms of residential units and non-residential square feet as shown in
 202 Figure 3. The maximum building heights for the Center and General Zones are shown in Figure 4.
 203 The Plan allows for flexibility among the square footage for types of non-residential uses in the
 204 Center and General Zones, provided that the total amount of recommended non-residential use is
 205 not exceeded. The form-based recommendations provide flexibility for individual proposals so they
 206 may best achieve the vision of the community and respond to the market. A development at an
 207 intensity of 3.0 FAR may be achievable based on parcel consolidation and height as guided by this Plan.

208
 209
 210 Figure 3: Planned Development Potential for the Center and General Zones

Land Use Category	Maximum Plan Potential
Residential	3,150 dwelling units
Non-residential	2,705,000 square feet

211
 212
 213 *Building Heights*

214
 215 Recommended Maximum building heights are guided by the number of stories rather than
 216 by prescribing specific building heights in order to provide flexibility to respond to changing market
 217 conditions and the needs of different uses. However, specific ranges of floor-to-~~ceiling~~-floor
 218 heights for different types of land uses are provided to inform potential building heights. All

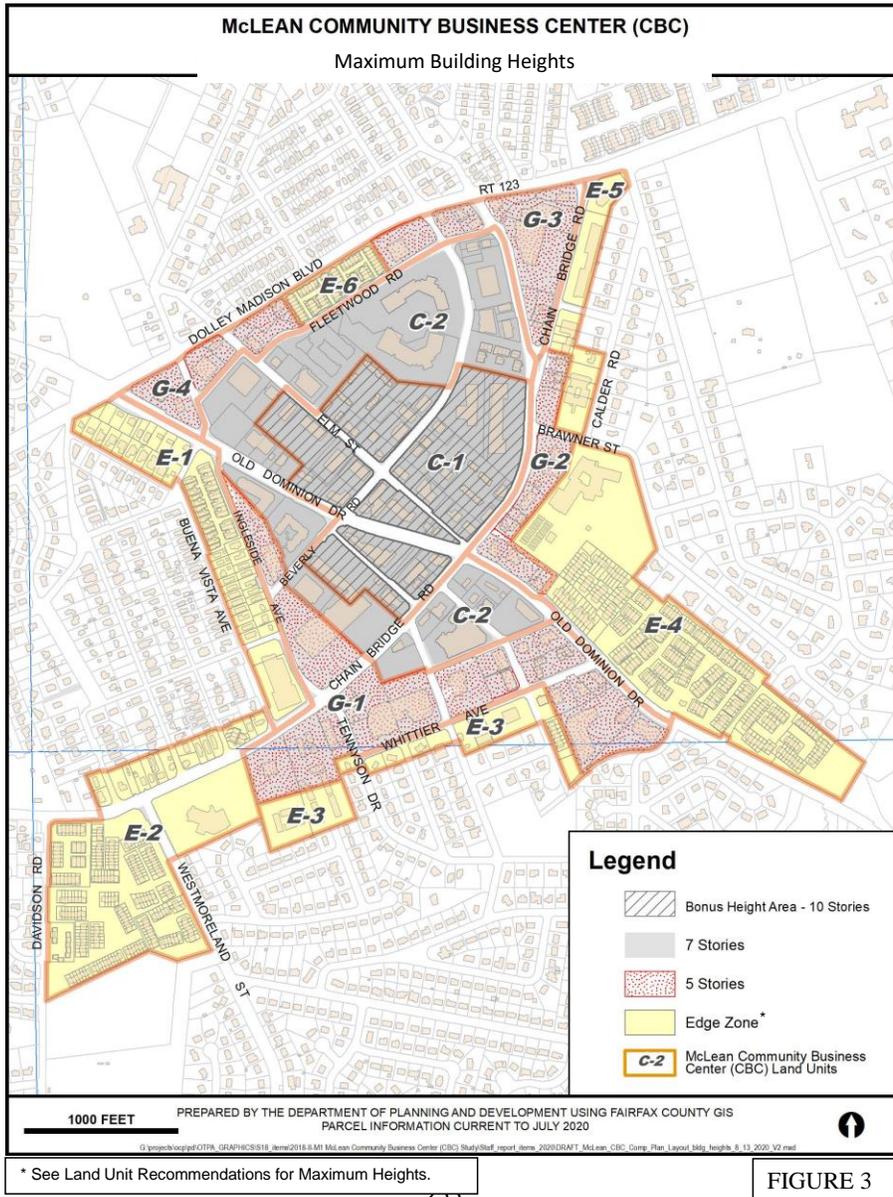
219 ground floors of buildings may have a floor-to-~~ceiling~~ floor height from 16 to 20 feet, regardless
220 of the land use. Above the ground floor, residential uses may have floor-to-~~ceiling~~ floor heights of
221 10 to 12 feet and office or hotel uses may have floor-to-~~ceiling~~ floor heights of 10 to 15 feet. Height
222 limits do not include mechanical penthouses, architectural elements, or features affixed to buildings
223 which are part of energy technology such as solar panels, provided that these features do not exceed
224 20 feet or 25 percent of the overall building height, whichever is less. Except for architectural
225 elements, these features should be effectively screened from adjoining uses. Height maximums are
226 exclusive of any additional stories that could result from meeting the Affordable Dwelling Unit
227 (ADU) Ordinance in the Zoning Ordinance or the Guidelines for the Provision of Workforce
228 Housing (WDU) in the Policy Plan.

229
230 Parcels that are split by two zones should be evaluated on a case-by-case basis to allow for
231 careful consideration of transitions.
232

Commented [GJ2]: Staff will continue to work on language for situations where a parcel is included in two zones. Draft statement a starting point for consideration.

233 Figure 4 depicts the recommended maximum building heights. The following
234 recommendations are also provided regarding building height within the McLean CBC:

- 235
- 236 • A building height of up to ten stories in the Bonus Height Area is achievable under certain
237 conditions outlined for the Center Zone.
- 238 • Buildings may be oriented to maximize their view potential, but their location and
239 orientation should take into consideration planned uses in the immediate vicinity.
- 240
- 241 • Step-backs in height should be considered ~~as a transition between zones, and~~ for properties
242 that abut the boundary of the CBC and for properties that abut single-family residential uses.
- 243 • If podium parking is part of a development, it is counted towards the total number of stories
244 recommended in each respective zone.
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257 Center Zone

258
259 The Center Zone is approximately 75 acres in size and is planned for the highest intensity
260 development and for the tallest building heights of the three zones. Building heights are
261 recommended to range from five stories to a maximum of seven stories; however, included within
262 the Center Zone is a smaller Bonus Height Area in which a single consolidation of four to six acres
263 may be developed with building heights up to ten stories. The development in the Bonus Height
264 Area may achieve the maximum ten story height if it provides a vibrant, mixed-use, pedestrian-
265 oriented place with a signature urban park. If a consolidation between four and less than five acres
266 is provided, a minimum 1/2 acre public park is recommended. If a consolidation between five and
267 six acres is provided, a minimum 2/3 acre public park is recommended. ~~(TASK FORCE)~~ Active
268 ground floor uses should surround the park. If a signature urban park exists at the time the
269 consolidated development is proposed, the development may provide an alternative urban park
270 configuration such as a common green, a civic plaza, or a recreation-focused urban park.
271 Contributions towards the purchase of a larger park and/or towards amenities in existing parks in
272 the CBC may also be considered in this instance.

273
274 A mix of uses is recommended to optimize vibrancy and increase opportunities for activity
275 throughout the day. Residential uses should be multi-family and are expected as part of mixed use
276 development. Single-family detached and attached residential units are discouraged, as they are not
277 consistent with the desired character in the Center Zone. A limited number of stacked townhomes
278 (“two-over-two” townhomes) may be appropriate as a transition generally along the edge of this
279 zone, so long as the site layout, height, and design reflect the desired character of the Center Zone.
280 Development is expected to yield public benefits such as new public parks, and improved access
281 and mobility.

282
283 Buildings should be located close to the sidewalk in a manner that creates a pedestrian-
284 oriented environment, unless there is an outdoor café, public plaza space or similar use located
285 between the building and the sidewalk. A particular focus on providing a high-quality pedestrian
286 experience is expected, including active ground floor uses such as retail, continuous sidewalks, and
287 block sizes that create a walkable environment. Where a smaller block size cannot be achieved,
288 mid-block crossings for pedestrians should be provided to help create a better scaled block and
289 improved pedestrian network. Proposed streetscapes are expected to meet the guidance contained
290 in this plan.

291
292 ~~Most parking is expected to be accommodated in structures or placed underground. On~~
293 ~~street parking is encouraged where appropriate. Surface parking is expected to be limited with~~
294 ~~redevelopment in the Center Zone.~~

295
296 General Zone

297
298 The General Zone is approximately 54 acres in size. Planned low to mid-rise development
299 in this zone is intended to provide a transition from the Center Zone to the Edge Zone, and in some
300 cases to single-family neighborhoods outside of the CBC. Building heights are recommended to
301 range from three stories to a maximum of five stories to effectuate appropriate transitions. A mix
302 of uses is recommended in the General Zone. For mixed-use development where pedestrian activity
303 is desired, active ground floor uses such as retail should be considered. Development is expected
304 to yield public benefits such as new public parks, and improved access and mobility. Single-family
305 attached or stacked townhomes (“two-over-two” townhomes) may be considered along the

Commented [GJ3]: STAFF RECOMMENDATION: The development in the Bonus Height Area may achieve the maximum ten story height if it provides a vibrant, mixed-use, pedestrian-oriented place with a signature urban park that is a minimum of 2/3 acres in size.

306 periphery of the General Zone as a transition between zones or to neighborhoods outside of the
307 CBC. ~~Structured parking is encouraged in the General Zone, especially for office and multifamily~~
308 ~~residential uses. Parking should support neighborhood-serving retail including some surface teaser~~
309 ~~parking in front of a building. ~~weaker~~However, it is generally which may include surface parking~~
310 ~~generally preferably to locate surface parking located to the side or rear of a building in the General~~
311 ~~Zone, but may include some teaser parking in the front of a building.~~

312
313 *Edge Zone*

314
315 The Edge Zone is approximately 85 acres in size and is comprised of primarily residential
316 uses along the outer boundary of the CBC. This zone is planned for the lowest density and intensity
317 development within the CBC. The Edge Zone provides a buffer between the CBC and single family
318 detached residential neighborhoods and is generally not envisioned for intensification of existing
319 uses, although redevelopment in conformance with the Plan is permitted. In addition to residential
320 development, the Edge Zone includes some commercial and institutional uses, and the Franklin
321 Sherman Elementary School. The preservation of small-scale commercial and community-serving
322 retail uses is encouraged. Building heights are recommended to be consistent with the
323 predominately lower intensity development. In some cases, land units have recommended building
324 height maximums with development options. The form-based approach does not apply to the Edge
325 Zone. Instead, recommendations are provided as a residential density range (du/ac) and/or square
326 feet of non-residential use or FAR for each land unit in the Edge Zone.

327
328 **HOUSING**

329
330 Fairfax County housing policies encourage the provision of housing affordable at a range
331 of income levels located close to employment opportunities including mixed-use areas. The
332 McLean CBC is well situated to provide a diversity of housing types to support households with a
333 range of ages, income levels, and abilities.

334
335 In the McLean CBC, development projects with a residential component are expected to
336 provide housing for a variety of income levels in accordance with the Affordable Dwelling Unit
337 (ADU) Ordinance contained in the Zoning Ordinance, and the Guidelines for the Provision of
338 Workforce Housing (WDUs) in the Policy Plan. The development potential for the CBC does not
339 include bonus density associated with the ADU Ordinance or with the WDU Policy. ADUs or
340 WDUs are expected to be provided on-site, or as an alternative, on another site within the McLean
341 CBC. The units should accommodate households of a variety of sizes, ages, and abilities.
342 Consideration may be given to deviations from the total number of ADUs or WDUs that should be
343 provided if the units meet additional housing needs that have been identified. Examples may
344 include a higher proportion of ADUs or WDUs for the lowest income tiers or units with more
345 bedrooms than would otherwise be expected.

346
347 A list of existing assisted housing in the McLean Planning District is contained in the
348 McLean District Overview Section, District-Wide Recommendations, Housing, Area II Volume of
349 the Comprehensive Plan.

350

351 PARKS AND RECREATION

352
353 As the McLean CBC redevelops, the need for publicly accessible parks and recreation
354 facilities will increase. Much of the recommended redevelopment is for multifamily residential
355 units which will not have private yards; as such, the provision of public park spaces is critical. Parks
356 and recreational opportunities provide significant benefits: they promote health and fitness, social
357 connections, and community building; support placemaking efforts that attract residents,
358 businesses, employees, and customers; improve air quality; and, capture stormwater runoff.

359
360 *Current Conditions*

361
362 Much of the development in the Center and General Zones in the McLean CBC is
363 dominated by retail and office uses. The long-standing commercial nature in these portions of the
364 CBC has resulted in a lack of a community-serving and centrally located public park. A majority
365 of the existing public parks that serve current residents are located outside of the CBC, including
366 McLean Central Park, Lewinsville Park, Salona Park, and Bryn Mawr Park. Franklin Sherman
367 Elementary School in the Edge Zone contains a diamond field and a playground that are available
368 for community use.

369
370 *Urban Parks Framework*

371
372 The Urban Parks Framework found in the Parks and Recreation Element of the Policy Plan
373 recommends minimum park acreage standards for residents and employees to estimate park needs
374 generated by development proposals; these standards are applied to all redevelopment projects. The
375 Urban Parks Framework describes five types of urban parks: pocket parks, common greens, civic
376 plazas, recreation-focused parks, and linear parks. These park types span a continuum of purposes,
377 uses, sizes, and features that can accommodate a broad spectrum of activities. Publicly accessible
378 parks can be publicly owned, privately owned, or provided through public-private partnerships.
379 Privately-owned public park spaces should remain open to the public at all times through public
380 access easements. Publicly accessible urban parks should be integrated with development projects
381 to provide for the diverse needs of the community. Active recreation needs should be provided
382 through a combination of on-site improvements, providing new recreation facilities, or funding
383 improvements at existing parks and at other sites within the service area.

384
385 *Parks, Recreation, and Connectivity Concept*

386
387 The concept for a park system is for a comprehensive network of well-distributed and
388 connected publicly accessible park spaces throughout and near the CBC, consisting of a new
389 signature urban park in the Center Zone, new well-distributed smaller urban park spaces and nearby
390 existing parks. The Conceptual Parks, Recreation, and Connectivity Map, Figure 5, shows the
391 general location of the signature urban park space and potential general locations of a series of
392 smaller urban parks. These locations should not preclude alternative or additional park spaces.
393 Pedestrian connectivity between the McLean Central Park and the Franklin Sherman Elementary
394 School is envisioned; this can be achieved by as a series of urban park spaces with pedestrian and
395 bicycle facilities along the length of roadway(s) that connect these destinations. The concept plan

396 also indicates other connections to link future parks in the CBC. As applicable, developments
397 should include connections between the urban parks and the public realm, and safe pedestrian and
398 bicycle-friendly pathways throughout the CBC and to the surrounding residential neighborhoods.
399 Opportunities to protect, connect to, and enhance existing park facilities in and near the McLean
400 area are also encouraged as part of the overall park network serving the CBC.
401

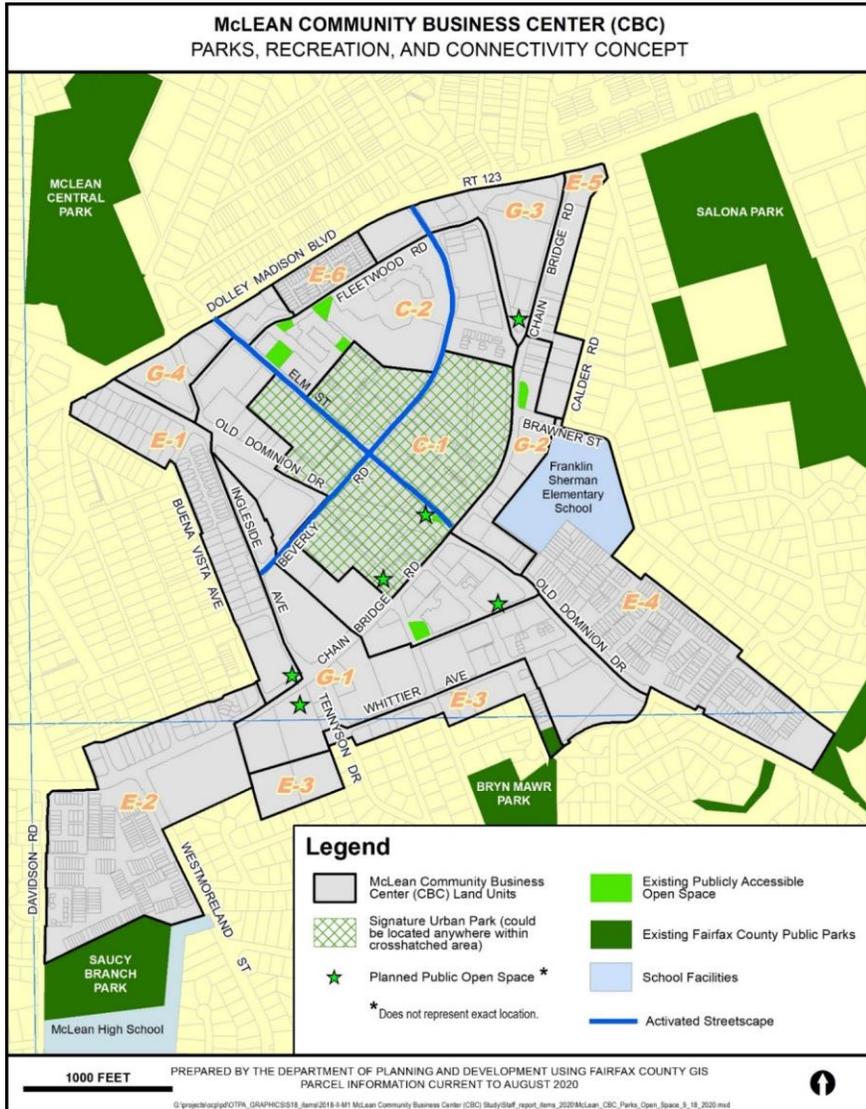


FIGURE 5

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 403
 404

405 Center Zone Signature Urban Park

406
407 A signature urban park space in the Center Zone is a major feature of the parks, recreation,
408 and connectivity concept for the McLean CBC. A single consolidated area between four and less
409 than five acres within the Bonus Height Area of the Center Zone is expected to provide a minimum
410 1/2-acre signature urban park. A single five- to six-acre consolidated development within the bonus
411 height area of the Center Zone is expected to provide a minimum 2/3-acre signature urban park
412 ~~(TASK FORCE)~~. If the development is phased, at a minimum, a substantial portion of the park
413 should be delivered with the initial phase of development.

Commented [GJ4]: STAFF RECOMMENDATION: A single four- to six-acre consolidated redevelopment within the Bonus Height Area of the Center Zone is expected provide the signature urban park. The signature urban park space should be a minimum of 2/3 of an acre in size.

414
415 The signature urban park should provide opportunities for both active and passive activities.
416 The design of the surrounding land uses and streetscape should ensure the public space is inviting
417 for park users of a variety of ages and abilities. The park space should:

- 418 • Be visible from the public realm, and accessible for users of a variety of ages and abilities;
- 419
- 420
- 421 • Provide connections to surrounding pedestrian and bicycle infrastructure, with access
- 422 supported by wayfinding signage as needed;
- 423
- 424 • Contain a variety of seating options and shade elements;
- 425
- 426 • Include an area designed for community gatherings such as farmer’s markets, art
- 427 exhibitions, festivals, concerts, fitness classes, and other events;
- 428
- 429 • Designate space(s) for unscheduled uses such as unstructured play;
- 430
- 431 • Incorporate a focal point which may include but is not limited to water features or public
- 432 art; and
- 433
- 434 • Include interactive elements for users to engage in physical activity to engage park users.
- 435 Examples of these types of elements include climbing/interactive art, an interactive musical
- 436 element, a splash pad, or a bocce court.
- 437

438 Parks and recreation facilities should be located to best serve the overall needs of the
439 residents, visitors, and employees in McLean. If a development is under consideration that is
440 adjacent to a previously approved application, or if two or more applications are under review at
441 the same time, the development(s) under consideration should demonstrate how their urban park
442 spaces can connect to, expand, or enhance the previously approved or proposed urban park spaces.

443
444 Creative solutions to providing parks and recreation facilities in the McLean CBC may be
445 pursued. ~~Developments are encouraged to demonstrate resourcefulness in their provision of~~
446 ~~recreation spaces which could include non-traditional locations. Creative initiatives~~ to meet a portion
447 of recreational facility needs in non-traditional locations, may include including the use of rooftops
448 for recreational uses such as sport courts or the provision of unique programming areas at nearby

449 existing parks or schools. These types of approaches should be limited to situations where ~~the~~
450 provision of at-grade public park or recreation facilities is not feasible. The alternative approaches
451 are not expected to be the primary means of providing public park and recreational facilities under
452 the optional level of development. With any of these approaches, visual and physical accessibility
453 to the public is essential.

454
455 PUBLIC FACILITIES

456
457 The existing public facilities and those anticipated to be needed to accommodate the future
458 growth of the McLean CBC are described in the following section. Since implementation of the Plan
459 will vary over time, development approvals should be monitored so that infrastructure capacity is
460 phased with new development. Regardless of the rate of growth, commitments of the land needed for
461 public facilities and/or identification of additional resources to support the provision of public facilities
462 should be completed in advance of the estimated need. Tables listing existing public facilities for the
463 Planning District are provided in the Overview section of the McLean Planning District in the Area II
464 volume of the Comprehensive Plan.

465
466 Schools

467
468 The McLean CBC is served by four public schools: Franklin Sherman Elementary School,
469 Kent Gardens Elementary School, Longfellow Middle School, and McLean High School. Using
470 attendance areas for School Year 2019 - 2020. Kent Gardens Elementary School, Longfellow
471 Middle School, and McLean High School had capacity deficits which may continue to exist through
472 School Year 2024-2025. A modular addition is planned for McLean High School to help mitigate
473 its capacity deficit prior to School Year 2024-2025. Student membership projections and individual
474 school capacity evaluations are based on five-year increments and updated annually, while the
475 Comprehensive Plan considers a 20-year horizon. To address the shorter-term student and school
476 capacity projections while also considering student needs over the longer-term planning horizon,
477 numerous strategies may be considered to ensure appropriate improvements are phased with new
478 development.

479
480 During the development review process, impacts generated by a development on public
481 schools should be mitigated. A variety of measures to mitigate the impacts of a new development
482 on school capacity should be considered, provided that the objectives and policies for public
483 schools within the Public Facilities Element of the Policy Plan are followed. Property owners and
484 developers in the McLean CBC should collaborate with Fairfax County Public Schools (FCPS) to
485 identify appropriate strategies to address school impacts, preferably in advance of approval of
486 applications for new residential developments, to maintain and improve the county's standards for
487 educational facilities and levels of service.

488
489 A new site or building that allows flexibility for school facility types may be needed to
490 support additional residential development. School facilities may include a traditional school or a
491 location for vocational training, academy programs, adult learning centers, and/or other support
492 functions. Contributions to the provision of these facilities should be made by developers proposing
493 new residential uses. Contributions might include dedicated land or buildings; or innovative

494 solutions such as repurposing buildings, locating school facilities with parks, or collocating within
495 commercial or residential buildings. For reuse of a building(s), the applicant in coordination with
496 FCPS may select a building(s) that provides access, safety, security, and meets play space
497 requirements. If FCPS determines that a site or building for a school facility is required to support
498 additional residential development, a fair share commitment should be identified in collaboration
499 with FCPS preferably in advance of approval of any application for residential development.
500 Alternatively, developers could make contributions toward land acquisition and school construction
501 based on a contribution formula determined by FCPS and Fairfax County. FCPS also may evaluate
502 other possible “in-kind” school impact mitigation strategies.

503
504 The FCPS’ Capital Improvement Program (FCPS CIP) contains detailed information on
505 student membership and facilities. The FCPS CIP is updated annually with data and contains
506 strategies for addressing schools where capacity is needed through capital projects and other
507 proposed solutions to alleviate a capacity need. Examples include additions to existing facilities,
508 interior facility modifications, uses of temporary classrooms to accommodate short-term capacity
509 deficits, program changes, reassigning instructional spaces within a school, utilizing existing space
510 on a school site used by non-school programs, repurposing existing inventory of school facilities
511 not currently being used as schools, building a school facility, and/or potential boundary
512 adjustments with schools having a capacity surplus can also be pursued by FCPS.

513
514 Libraries

515
516 There are currently no community or regional libraries within the boundaries of the McLean
517 CBC; however, two Fairfax County public libraries are within close proximity to the CBC: Dolley
518 Madison Library just outside of the CBC, and Tysons-Pimmit Regional Library, in the southern
519 part of the McLean Planning District. These libraries will be able to serve additional residents that
520 could result from the growth recommended by the Comprehensive Plan. Fairfax County Public
521 Libraries (FCPL) does not anticipate the need for additional library facilities in or near the McLean
522 CBC.

523
524 Police and Fire and Rescue

525
526 The McLean CBC is served by the McLean District Police Station, co-located with the
527 Dranesville District Supervisor’s Office at the McLean Governmental Center. The McLean District
528 Police Station also provides service to Tysons, Pimmit Hills, West Falls Church, Dunn Loring, and
529 portions of Great Falls and Merrifield. A new District Police Station is planned for Tysons in
530 response to the projected growth in the area.

531
532 Emergency and other fire and rescue services are primarily provided by the McLean Fire
533 and Rescue Station 1 located within the McLean CBC. Several other fire and rescue stations
534 provide service to the McLean CBC, including the Tysons Corner Fire and Rescue Station 29 and
535 Dunn Loring Fire and Rescue Station 13. The new Scotts Run Fire and Rescue Station 44 is planned
536 to be constructed on a site along Old Meadow Lane in Tysons. There are plans to relocate the
537 existing Tysons Corner Fire and Rescue Station 29 to a larger facility on the site of the Tysons

538 Transit site by 2025. The existing and planned facilities will continue to provide sufficient coverage
539 as redevelopment occurs in the CBC.

540
541 Wastewater Management

542
543 Wastewater generated in the McLean CBC is treated at Blue Plains treatment plant, a
544 regional facility located in Washington, DC. The service agreement that Fairfax County has with
545 Blue Plains is not adequate to handle the projected sewage flow beyond 2040. To alleviate the
546 future treatment deficit for the Blue Plains service area, the county has purchased treatment capacity
547 from Loudoun Water. The county is also rehabilitating the Difficult Run Pump station to allow the
548 pumping of excess flow from the Blue Plain service area to the Norman M. Cole Jr. Pollution
549 Control Plant. The McLean CBC is served by the Dead Run Sanitary Sewer Pump Station which
550 will require an upgrade along with the sewer lines serving the CBC for adequate capacity to
551 accommodate the development potential recommended by the Comprehensive Plan. Coordination
552 with county staff by applicants proposing new developments is recommended to address
553 wastewater planning needs.

554
555 Fairfax Water

556
557 The McLean CBC is served by transmission water mains ranging in size from 4 to 30 inches
558 in diameter. The existing facilities are sized appropriately to meet the proposed increase in demand
559 that could result from new development. Distribution water main sizing and alignments,
560 distribution network improvements, and fire flow requirements will be evaluated concurrently with
561 the review of development proposals.

562
563 Undergrounding of Utilities

564
565 Utility lines are expected to be placed underground and coordinated with future roadway
566 and sidewalk improvements to promote a pedestrian-friendly and visually pleasing environment.

567
568 **IMPLEMENTATION**

569
570 Successful implementation of the Plan for the Mclean CBC will require a commitment to
571 the overall vision set forth in the Plan. Key components for the vision include a multimodal street
572 network that is responsive to the needs of pedestrians and bicyclists as well as automobiles, an
573 appropriate mix of uses, activated streetscapes, building height maximums, compatible transitions,
574 and public parks. Implementation will occur primarily through the rezoning process, where reviews
575 are conducted to evaluate the extent to which a development proposal achieves the Plan
576 recommendations and whether a development's impacts are adequately addressed. As part of the
577 development review process, the provisions of the McLean Commercial Revitalization Overlay
578 District contained in the Zoning Ordinance and the "McLean CBC Open Space Design Standards"
579 are expected to be implemented.

580
581 Mechanisms to address development impacts include commitments to high quality design
582 and other Plan objectives, and contributions towards transportation and/or public facility

583 improvements that are needed to support new development. The initial projects that seek
584 redevelopment should establish a sound framework that sets the stage for future developments.

585
586 Community Involvement

587
588 The continued involvement of community groups is necessary to implement the McLean CBC
589 Plan. Organizations such as For example, the McLean Planning Committee, composed of
590 representatives from the McLean Citizens Association, the McLean Chamber of Commerce, the
591 McLean Landowners Association, and the surrounding citizens' associations, is an organization that
592 has have long been involved in planning activities within the CBC. The McLean Planning Committee
593 reviews development proposals prior to public hearings and issues recommendations for consideration
594 by the Dranesville District Supervisor. Other community groups in McLean also provide feedback on
595 development proposals for consideration by the Planning Commission and/or Board of Supervisors.
596 Efforts of community groups to encourage redevelopment and implementation of the Plan may include
597 facilitating community-enhancing development through innovative partnerships among the private,
598 public, and volunteer sectors; and leveraging available funds and generating new funding sources
599 through grants and fund raising from the business and government communities.

- 600
601 ~~• Facilitating community enhancing development through innovative partnerships among the~~
602 ~~private, public, and volunteer sectors;~~
- 603
604 ~~• Leveraging available funds and generating new funding sources through grants and fund~~
605 ~~raising from the business and government communities;~~
- 606
607 ~~• Implement the above within the context of the Policy and Area Plans and under the policy~~
608 ~~guidance of the McLean Planning Committee; and~~
- 609
610 ~~• Providing initiative, feedback, and advice to the McLean Planning Committee on~~
611 ~~development planning, legal, marketing, public relations, real estate, and land development~~
612 ~~opportunities.~~
- 613
614 ~~• Reviewing development proposals for consistency with the guidance provided for the~~
615 ~~McLean CBC in the Comprehensive Plan.~~

616
617 Flexibility for Non-Residential Uses

618 The Plan recommends a maximum development potential for both residential and non-
619 residential uses in the Center and General Zones. The Plan allows for flexibility among the square
620 footage for types of non-residential uses, provided that the total amount of recommended non-
621 residential use is not exceeded and that development proposals achieve the recommendations for
622 multimodal connectivity, publicly accessible parks or open space, building heights and transitions,
623 mix of uses, and pedestrian-oriented and active streets at the ground-floor level. Monitoring
624 approved rezonings and building permits is expected to track the amount of development that is
625 implemented under the overall maximum development potential.

626
627 Discouraged Uses

628
629 Standalone, single-use auto-oriented uses and uses with drive-through lanes are not
630 preferred ~~envisioned~~ as part of the long-term vision for the CBC. These uses may be acceptable
631 ~~only~~ when they can be incorporated and are consistent with the desired building form ~~and~~

Commented [GJ5]: Requesting confirmation from the task force regarding the retention or deletion of Discouraged Uses section, as a decision was not clear from the meeting recording.

632 character of the area. The location and design of such uses and any associated drive-through lanes
633 should not impede the flow of pedestrian or vehicular traffic, compromise safety, disrupt the
634 existing and planned interior circulation system of the site, or impede the achievement of the long-
635 term vision of the Comprehensive Plan. Other uses that may not be consistent with revitalization
636 goals such as standalone industrial and self- storage facilities are strongly discouraged.
637

638 Phasing with Public Facilities

639 Development in the McLean CBC will occur incrementally. Each development proposal
640 or phase of a development proposal will be evaluated for its public facility impacts and is expected
641 to construct and/or commit to the provision of public facilities appropriate for the phase of
642 development so that infrastructure and public facilities are balanced with growth.
643

644 Transportation Infrastructure

645
646 All development proposals should include the planned road improvements as described in
647 the Transportation or Urban Street Network sections. For new streets not built to their ultimate
648 cross-section, right-of-way should be provided to allow for the future construction of the ultimate
649 cross-section as identified in the Plan. Additional street segments identified through future analysis
650 that are necessary to maintain acceptable traffic circulation should be provided by that
651 development.
652

653 Transportation Pilot Project

654
655 A pilot project to create a more bicycle and pedestrian friendly transportation network
656 should be explored by Fairfax County. Specifically, a pilot project is suggested along Old Dominion
657 Drive from Beverly Road to Corner Lane, and Chain Bridge Road from the intersection with Old
658 Chain Bridge Road southwest to the Tennyson/Ingleside intersection. Cross-sections should be
659 designed and constructed as described in this Plan, including for areas that are part of a pilot project.
660 Where implementation of the ultimate cross-sections as part of a pilot project would have
661 significant impacts on existing land uses, a modified interim cross-section is acceptable as long as
662 it provides continuous pedestrian and bicycle facilities. In these cases, the future construction of
663 the ultimate cross-section can be achieved through redevelopment rather than the pilot project.
664

665 Interim Development Conditions

666
667 Achieving the Plan’s long-term vision can take many years and can occur incrementally. In the
668 meantime, reinvestment or development may occur that does not achieve the ultimate Plan vision.
669 Furthermore, in some instances, development that will ultimately achieve the vision may take place in
670 phases resulting in interim site conditions during those phases. Development proposals may be
671 considered interim development under the following four scenarios:
672

- 673 1. Temporary conditions that are created when a project is constructed in phases and the
674 development plan is not fully realized;
675

- 676 2. Developments, generally those smaller in scale and potentially for a limited duration, that do
677 not strictly conform to the ultimate vision in the Plan;
678
679 ~~3.~~ Minor improvements to existing uses ~~that do not strictly conform to the vision in the~~
680 ~~4.3. Plan~~; and
681
682 ~~5.4.~~ Temporary place-making efforts that can contribute to the vitality of the area on a short-
683 term basis such as public art or pop-up commercial uses.
684

685 Interim development conditions should ~~mitigate ensure that~~ any adverse impacts associated with
686 an interim state of redevelopment ~~to a degree that is~~ mitigated as reasonable and appropriate to the
687 extent of the redevelopment or improvement. Additional guidance on interim conditions can be found
688 in the Guidelines for Interim Improvement of Commercial Establishments, Appendix 6 of the Land Use
689 Element of the Policy Plan and in Chapter 7 of *Volume I: Urban Design Guidelines for Fairfax County*
690 *Commercial Revitalization Districts and Areas.* ~~Any~~ ~~interim proposals, should,~~ as applicable, may be
691 expected to:
692

- 693 1. Design buildings for the ultimate street cross-sections by siting them to be compatible with
694 the alignment of the street network. As appropriate, provide façade articulation to each
695 building face and treatments to ensure compatible transitions, and incorporate appropriately
696 scaled entrances;
697
698 2. Include a pedestrian plan that provides interim or permanent pedestrian connections and
699 streetscape improvements to facilities such as retail uses, parks within the site and on
700 adjacent sites;
701
702 ~~3. Provide streetscape improvements that conform to Plan recommendations and achieve~~
703 ~~continuity of the streetscape design;~~
704
705 ~~4.3.~~ Demonstrate how interim parking adheres to parking design and phasing goals;
706
707 ~~5.4.~~ Show how stormwater facilities will be incorporated and address the impacts of interim
708 development conditions;
709
710 ~~6.5.~~ Provide landscaping improvements to enhance the aesthetics and functionality of spaces
711 that are in transition; and
712
713 ~~7.6.~~ Demonstrate how the proposed development will not preclude future redevelopment of the
714 site or adjacent sites in conformance with the Plan.
715

716 For a phased project, interim conditions that enhance the urban character and contribute to
717 place-making are encouraged for portions that will not be built until later phases. Examples include
718 pop-up parks, interim recreational facilities, or low intensity temporary uses. It may also be acceptable
719 to maintain existing uses in lieu of an interim use as long as they do not preclude the achievement of
720 other priorities and Plan goals.
721

722 Parcel Consolidation

723 Parcel consolidation is encouraged to achieve the vision of the McLean CBC plan, and
724 should be in conformance with any areawide and site-specific recommendations of the
725 Comprehensive Plan. Should the Plan text not specifically address consolidation, then any proposed
726 parcel consolidation should further the integration of the development with adjacent parcels. Parcel
727 consolidation is expected to be logical and of sufficient size to allow projects to function in a well-
728 designed, efficient manner, and should not preclude nearby properties from developing as
729 recommended by the Comprehensive Plan.
730

731 ENVIRONMENT

732
733 The McLean CBC is located at the headwaters of the Dead Run and Pimmit Run watersheds.
734 The county completed numerous stormwater improvement projects in these two watersheds around
735 McLean between 2007-2017. Redevelopment presents opportunities for continued watershed
736 improvement through the use of modern stormwater management controls. Development should
737 also seek to restore and enhance other environmental elements such as tree cover and landscaping
738 to promote environmental stewardship and encourage the incorporation of the natural environment
739 into the McLean CBC. Implementation of other county environmental policy objectives related to
740 green building practices, transportation generated noise and tree preservation should be
741 incorporated with any redevelopment proposal.

742
743
744 Stormwater Management
745

746 Most of the existing development in the McLean CBC was constructed in the mid-1900s prior
747 to the stormwater management requirements that are expected today. The CBC contains a
748 significant amount of impervious surfaces including parking lots, roads, sidewalks and buildings
749 which has resulted in flooding and stream degradation issues, as the impervious surfaces do not
750 allow for the infiltration of rainwater into the ground. This results in the flow of large quantities of
751 runoff directly into streams.

752
753 Receiving waters downstream of the McLean CBC should be protected by reducing runoff
754 from impervious surfaces. The primary means to achieve this goal is through the inclusion of
755 stormwater management measures that ~~produce optimal results to manage~~ reduce with
756 ~~development. Stormwater runoff is expected to be managed in a manner that reduces~~ the volume
757 of stormwater runoff from sites and controls peak flows of remaining stormwater runoff that cannot
758 be captured on-site. The following guidelines are recommended for the optional level of
759 development in the Center and General Zones:

- 760
- 761 • Stormwater quantity and quality control measures should be provided with the goal of reducing
762 the total runoff volume and/or significantly delaying its entry into the stream system. The
763 emphasis should be on Green Stormwater Infrastructure (GSI). Examples of GSI include rain
764 gardens, vegetated swales, permeable pavements, and green roofs. GSI is designed to protect,
765 restore, or mimic nature. ~~GSI also and to evapotranspires water, filters water through vegetation~~
766 ~~and/or soil, returns water into the ground, or reuses it water. Examples of GSI include rain~~
767 ~~gardens, vegetated swales, permeable pavements, and green roofs.~~
 - 769 • At a minimum, the first inch of rainfall should be retained on-site through infiltration,
770 evapotranspiration, and/or reuse. If the retention of the first inch of rainfall is not fully
771 achievable on a given site, substantial efforts should be made to achieve partial retention as
772 close to the first inch of rainfall as possible.
 - 774 • Identifying partnership opportunities with Fairfax County is encouraged to provide additional
775 stormwater volume and water quality controls for proposed stormwater management facilities.

Commented [GJ6]: Edits will be distributed end of September

779 Residential and Other Noise-Sensitive Uses

780
781 The Environment ~~Section~~-Element of the Policy Plan provides guidance on minimizing
782 human exposure to unhealthful levels of transportation generated noise. For residential or other
783 noise sensitive uses proposed near Chain Bridge Road, Dolley Madison Boulevard, and Old
784 Dominion Drive, adequate measures to prevent negative impacts on noise sensitive uses, consistent
785 with ~~Objective 4 of the Environment element of the Policy Plan~~ those policies should be taken.

786
787 Green Building Practices

788
789 The Environment Element of the Policy Plan provides guidance for green building practices
790 and standards. Development should meet applicable green building standards in accordance with
791 the Policy Plan.

792
793
794 HERITAGE RESOURCES

795
796 The Overview section of the McLean Planning District includes a figure and map of
797 historically significant resources that are included in the Fairfax County Inventory of Historic Sites,
798 as well as countywide heritage resources policies. The heritage resources in the McLean CBC
799 included in the Inventory of Historic Sites are the McLean Baptist Church at 1437 Emerson
800 Avenue, the Sears-Roebuck House at 1506 Chain Bridge Road, and the former McLean Volunteer
801 Fire Department/Fairfax County Fire Station Number 1 at 4440 Chain Bridge Road, known as the
802 "Old Firehouse." ~~The "Old Firehouse" has been adaptively reused and should be maintained as a~~
803 ~~historic structure. The area around it is planned for public gathering space. All development should~~
804 ~~respect the historic integrity of the resource~~

805
806 Few historic buildings in the McLean CBC have been formally documented. A
807 reconnaissance level field survey conducted in 2019 identified several potential heritage resources
808 associated with the area's 20th century residential and commercial history. ~~The potential resources~~
809 ~~include residents residences built between 1900 and 1940 that remain unaltered; pre WWII~~
810 ~~commercial buildings; and post WWII buildings which potentially embody distinctive~~
811 ~~characteristics of a type, period, or method of construction. The potential resources identified by~~
812 ~~the reconnaissance level survey should be evaluated in further detail to determine the property's~~
813 ~~significance and whether it qualifies as a heritage resource.~~ Heritage resources staff in the
814 Department of Planning and Development should be contacted for information regarding resource
815 identification and ongoing survey efforts as directed by the Heritage Resource Management Plan
816 and the Comprehensive Plan Policy on Heritage Resources.

817
818 TRANSPORTATION

819
820 Multimodal Vision

821
822 The overall transportation vision for the McLean CBC is to encourage increased use of active
823 transportation modes and transit by developing high-quality bicycle networks, pedestrian facilities,
824 and transit services while continuing to accommodate vehicular needs. Redevelopment efforts
825 should focus on enhancing the pedestrian and bicycle experience through the implementation of
826 continuous walkways and multimodal connections that support local travel within and through the

Commented [OL7]: The task force voted to remove these three sentences. Staff recommends retaining the sentences.

Commented [GJ8]: The task force voted to remove these sentences. Staff recommends retaining the paragraph as originally proposed.

827 McLean CBC. It is important that multimodal transportation improvements not only promote
828 mobility, but improve connectivity, enhance safety, complement placemaking, and support
829 revitalization goals.

830
831 Transportation Improvements

832
833 The following sections provide transportation recommendations for the CBC.

834
835 *Street Network*

836 The street network should provide access to, through and within the McLean CBC. Several
837 modifications to the existing street network are recommended to achieve these goals. These
838 modifications are reflected on Figure X, Transportation Recommendations and are stated below:

839
840 • Improve Old Dominion Drive, Chain Bridge Road, and Old Chain Bridge Road, while
841 maintaining them as four-lane roads. Improvements may include but are not limited to
842 narrowing of vehicle lane widths, sidewalk or curb and gutter enhancements. Pending
843 further study, it may be appropriate to designate two travel lanes and two parking lanes on
844 portions of Old Dominion Drive and Chain Bridge Road during off-peak hours.

845
846 • Improve Elm Street and Beverly Road, while maintaining them as two-lane roads.
847 Improvements may include but are not limited to narrowing of vehicle travel lane widths,
848 sidewalk or curb and gutter enhancements.

849
850 • Prioritize pedestrians and bicyclists on Beverly Road and Elm Street. Treatments that
851 reduce vehicle operating speeds, minimize crossing distances, and shorten block lengths
852 should be implemented.

853
854 • ~~Close the intersection of Elm Street at Chain Bridge Road to vehicles to improve safety and~~
855 ~~operations at the Old Dominion Drive and Chain Bridge Road intersection. However, access~~
856 ~~to the southern portion of Elm Street should be maintained for pedestrians and bicyclists.~~
857 ~~The remaining portion of Elm Street from Beverly Road to Chain Bridge Road should be~~
858 ~~repurposed as a pedestrian-mall permitting with limited vehicular access. Closing this road~~
859 ~~segment should be coordinated with the property owners when redevelopment is proposed~~
860 ~~adjacent to this street segment. Impacts to adjacent businesses should be considered prior~~
861 ~~to implementation.~~

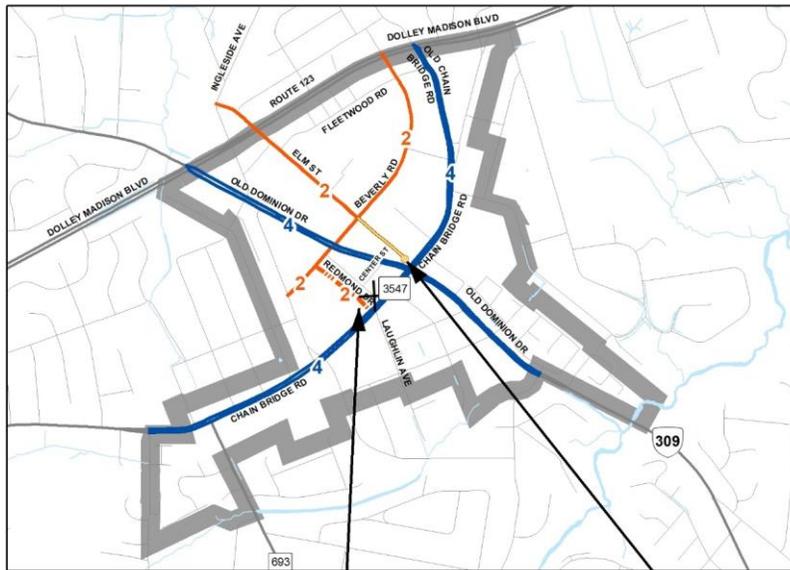
862
863 • Realign Redmond Drive to Laughlin Avenue at Chain Bridge Road to improve intersection
864 spacing and connectivity. Coordinate this improvement with adjacent property owners to
865 ensure that access is maintained as redevelopment occurs.

866
867 Close the intersection of Center Street and Old Dominion Drive or relocate Center Street
868 further from Chain Bridge Road to improve intersection spacing. Direct public pedestrian
869 and bicycle access between Redmond Drive and Old Dominion Drive should be maintained.
870 Coordinate this improvement with adjacent property owners to maintain access as
871 redevelopment occurs.

Commented [GJ9]: For task force – any proposed modifications to these roads will be coordinated with the property owners, VDOT and the County. Access needs to each property will be discussed as part of this process.

- 872 • Improve the intersection of Chain Bridge Road and Westmoreland Street to increase
873 vehicular capacity, and to provide safe crossings for all road users.

MCLEAN COMMUNITY BUSINESS CENTER

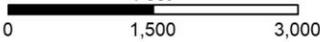


REALIGN REDMOND DRIVE

SEVER ELM STREET CONNECTION WITH CHAIN BRIDGE ROAD. CONSTRUCT AS A PEDESTRIAN MALL WITH LIMITED VEHICULAR ACCESS.



Feet



TRANSPORTATION RECOMMENDATIONS LEGEND

- | | | |
|-----------------|------------------------|--|
| ARTERIAL | COLLECTOR LOCAL | |
| | | WIDEN OR IMPROVE EXISTING ROADWAY |
| | | CONSTRUCT ROADWAY ON NEW LOCATION |
| | | TOTAL NUMBER OF LANES, INCLUDING HOV LANES (COLLECTOR/LOCAL CROSS SECTIONS TO BE FINALIZED DURING PROCESS OF REVIEWING PLANS FOR PROPOSED DEVELOPMENT) |
| EXISTING | PROPOSED | |
| | | METRO RAIL STATION |
| | | COMMUTER PARKING LOT |
| | | TRANSIT TRANSFER CENTER (NO PARKING) |
| | | COMMUTER RAIL STATION |
| | | RAIL STATION |
| | | POTENTIAL BUS RAPID TRANSIT (BRT) STATION |
| | | HIGH OCCUPANCY VEHICLE LANES |
| | | HIGH OCCUPANCY TOLL LANES |
| | | RAIL TRANSIT |

- FULL INTERCHANGE IMPROVEMENT (STUDY REQUIRED)
- PARTIAL INTERCHANGE IMPROVEMENT
- PROPOSED HIGHWAY OVERPASS
- PROPOSED HIGHWAY UNDERPASS
- PROPOSED CUL-DE-SAC
- BUS RAPID TRANSIT (BRT)
- PLANNING SECTOR OR DISTRICT OR DEVELOPMENT CENTER

NOTE: IMPROVEMENTS TO ARTERIAL FACILITIES SUBJECT TO COMPLETION OF CORRIDOR STUDIES. SEE DISCUSSION IN AREA PLAN OVERVIEW TEXT. FINAL ALIGNMENTS SUBJECT TO COMPLETION OF APPROPRIATE ENGINEERING STUDIES.

HOV LANES TO BE CONSIDERED IN PROJECT DEVELOPMENT. HOV LANES TO BE PROVIDED IF WARRANTED BASED ON DEMAND FORECASTS AND CORRIDOR STUDY.

875 *Transit Service*

876
877 High quality, frequent bus service should ~~continue to~~ provide access to and from regional
878 job centers, the McLean Metrorail Station, and the McLean CBC. High-quality bus stops with
879 amenities, such as benches, shelters, and/or other improvements, should be provided to enhance the
880 experience for bus riders and contribute to placemaking.

- 881
882 • Refer to the Fairfax County Department of Transportation Transit Development Plan (TDP)
883 for recommended transit improvements in the area.

884 *Pedestrian Facilities*

885
886 Pedestrian facilities within the McLean CBC should accommodate and attract users while
887 contributing to placemaking. Sidewalks should be constructed on both sides of all roadways to
888 provide a complete pedestrian network. The recommended network of pedestrian facilities is shown
889 in Figure X, Multimodal Network Map.

890
891 The following recommendations apply:

- 892
893 • Build and maintain pedestrian facilities with high levels of convenience, accessibility, and
894 comfort. This includes, but is not limited to, the provision of wayfinding signage,
895 minimized delay at intersections, minimized crossing distance at intersections, pedestrian
896 refuge areas (where crossing distances cannot be minimized), and appropriate treatments
897 where driveways cross sidewalks.
- 898
899 • Provide transitions between pedestrian facility types, such as from sidewalk to shared use
900 path, at intersections and not mid-block.
- 901
902 • Provide clearly marked crosswalks, along with pedestrian signals for all legs of signalized
903 intersections. Crosswalks should be provided at other intersection locations, such as at all-
904 way stops, as appropriate.
- 905
906 • Implement where feasible pedestrian walkway connections though and between
907 developments, including those that allow pedestrians to connect from one roadway to
908 another where there are large development blocks, especially those east and west of Old
909 Dominion Drive, north of Chain Bridge Road.
- 910
911 • Consider mid-block pedestrian crossings of collector and local streets that are part of large
912 blocks, such as along Beverly Road, Elm Street, Fleetwood Road, and Ingleside Avenue, if
913 they can be safely provided.
- 914
915 • Evaluate crossings of Dolley Madison Boulevard and Chain Bridge Road for improved
916 pedestrian access to the McLean Community Center, the Dolley Madison Library, the
917 McLean Central Park and Franklin Sherman Elementary.
- 918

919 *Bicycle/Trail Network*

920
921 A comfortable, well-marked, and well-connected bicycle and trail facility network should be
922 provided in the McLean CBC. Bicycle and trail facilities will provide important connections
923 through, to and from, the CBC. The facilities should be designed, maintained, and operated to a
924 standard that accommodates and attract users and contribute to place making. Recommendations
925 for bicycle facilities are shown on the Multimodal Network Map, Figure XX.

926
927 The following recommendations apply:

- 928 • Build and maintain convenient and comfortable facilities for bicyclists, including, but not
929 limited to, wayfinding, continuous and connected facilities, and minimized crossing
930 delays at intersections.
- 931 • Implement safety measures to increase separation of bicyclists and pedestrians from
932 vehicles and reduce conflicts at intersections.
- 933 • Locate transitions between bicycle facility types, such as from a cycle track to a shared
934 use path, only at intersections and not mid-block.
- 935 • Provide convenient access to secure bicycle parking facilities.
- 936 • Accommodate and/or install stations for bikeshare programs.

937
938
939
940
941
942
943 *Access Management*

944 Consolidation of access points is encouraged to enhance the walkability and bikability of
945 the CBC. The number of curb cuts and other driveway access points should be minimized, while
946 also taking into consideration the need to accommodate development. Reducing the number of
947 access points enhances safety and traffic flow and lessens conflicts among motorists, pedestrians,
948 and bicyclists. Curb cuts and driveway access points for the CBC should also be designed for
949 pedestrian and bicyclist safety and comfort.

950
951 The following recommendations apply:

- 952 • Reduce the number of curb cuts and other driveway access points, where feasible,
953 throughout the CBC, to minimize interruptions and safety conflicts where they cross
954 pedestrian facilities.
- 955 • Encourage coordinated access points and provide vehicular inter-parcel access wherever
956 possible.
- 957 • Locate off-street service and loading areas behind buildings and away from arterial
958 roadways to avoid conflicts with motorists, pedestrians, and bicyclists.

960
961
962
963

964 *Transportation Demand Management*

965
966 Transportation Demand Management (TDM) refers to a variety of strategies aimed at
967 reducing travel demand, especially for single-occupant vehicle trips during peak periods, and at
968 expanding the modal choices. Reduced traffic volumes contribute to improved vehicular
969 operations, more efficient use of the transportation system, and reduce negative impacts on
970 livability, bikability, and walkability. A systematic program of TDM strategies in the McLean CBC
971 can reduce peak period single-occupancy vehicle trips and increase the percentage of travelers
972 using transit and non-vehicular modes of transportation.

973

974 The following recommendations apply:

975

976

977

978

979

980

981

- Development proposals should commit to reduce vehicle trips during peak travel times though the use of TDM strategies per the Fairfax County Comprehensive Plan, Transportation Policy Element and Fairfax County TDM Guidelines.
- Residential and commercial property owners are strongly encouraged to coordinate TDM strategies with one another.

982 *Parking Management*

983

984 Parking strategies should be considered to avoid over-parking and maximize use of parking
985 spaces. These strategies, which may include shared parking and, timed parking, ~~and paid pay to~~
986 ~~park parking. These strategies can not only~~ may reduce the cost of providing parking, but also
987 encourage the use of active transportation modes and transit, and increase the turnover ~~and of~~
988 ~~availability of available~~ parking. ~~Parking management s~~ Strategies to reduce parking should consider
989 potential impacts to adjacent neighborhoods ~~to and~~ avoid overflow parking in those areas.

990 The following recommendations apply:

991

992 ~~Strongly encourage parking reductions and discourage excess parking volumes in excess of~~
993 ~~parking minimums above minimum requirements~~

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- Explore opportunities for consolidated or shared parking.
- Explore the potential to designate areas for off-peak, on-street parking (e.g. rush hour restricted parking) along Old Dominion Drive and Chain Bridge Road, in coordination with the Virginia Department of Transportation (VDOT).
- ~~Metered Provide on-street pay to park parking should be provided, in coordination with Fairfax County's parking program. Paid private Private pay to park parking may also be appropriate.~~
- ~~Encourage the designation of on-street loading zones on~~ Designate on-street loading zones to facilitate deliveries and drop-offs o On blocks where on-street parking is present and

1007 where loading and delivery areas cannot be accommodated on-site, ~~designate on street~~
 1008 ~~loading zones to facilitate deliveries and drop-offs.~~

1009
 1010 Functional Classification of Streets

1011
 1012 The Commonwealth of Virginia supports the goal of providing communities with a
 1013 multimodal transportation system. To advance this goal, the Virginia Department of Rail and Public
 1014 Transportation (DRPT), in collaboration with FCDOT, and other entities developed the Multimodal
 1015 System Design Guidelines (MMDG) in 2013. In urban or urbanizing areas those guidelines can be
 1016 used as an alternative to VDOT’s functional classification system, which is more applicable to
 1017 suburban and rural environments. Major features of the MMDG are alternative road classifications,
 1018 lesser intersection spacing standards, wider pedestrian and bicycle facility standards, and
 1019 designation of modal priorities for the corridor other than for vehicles, such as for transit and
 1020 pedestrian modes, or placemaking elements like landscaping. Use of the MMDG’s context sensitive
 1021 and multimodal approach is consistent with the vision planned for the McLean CBC. Figure X
 1022 provides a cross-reference between VDOT’s and MMDG’s classification methods.

1023
 1024 The recommended design of streets based on the MMDG classification includes
 1025 consideration for the roadway and the adjacent streetscape areas with the goal of achieving
 1026 “complete streets” and connected networks. Complete streets provide safe access and movement
 1027 for pedestrians, bicyclists, and transit riders of all ages and abilities, while networks ensure that
 1028 those users can travel and make connections throughout the area. As shown in Figure X, the
 1029 Multimodal Network Map, Avenues and Local Streets are proposed within the McLean CBC.
 1030 Additional or alternative connections that are not depicted on the Multimodal Network Map may
 1031 be necessary to improve or maintain pedestrian or bicycle connectivity and acceptable vehicular
 1032 and transit operations.

1033
 1034 **Figure #**
 1035 **Cross-Reference between Traditional Highway Classification and Multimodal Street Types**

Fairfax County Functional Classification (Design Speed)					
Multimodal Types (Design Speed)	Interstate, Freeway, or Expressway (50-70 mph)	Principal Arterial (30-60 mph)	Minor Arterial Type A or B (30-60 mph)	Collector (30-50 mph)	Local Street (20-30 mph)
		Multimodal Through Corridor (35-55 mph)			
		Transit Boulevard (30-35 mph)			
		Boulevard (30-35 mph)			
			Major Avenue (30-35 mph)		
			Avenue (25-30 mph)		
					Local Street (25 mph)

1036 Note: The cross-references shown in the table above are general in nature and some variations may occur. Design
 1037 speeds are depicted. There are no Multimodal Through Corridors, Transit Boulevards or Major Avenues proposed in
 1038 the McLean CBC.

1039 Source: Virginia Department of Rail and Public Transportation, Multimodal System Design Guidelines, Chapter 5
 1041 Multimodal Corridors, October 2013.

1042 Cross Sections and Streetscape Design

1043
1044 The typical cross-sections are depicted below with the understanding that flexibility will
1045 need to be applied in identifying the dimensions of some of the elements to respond to the particular
1046 circumstances of a location. The general right-of-way widths depicted do not include any additional
1047 turn lanes that may be needed to support new development, although creating new right turn lanes
1048 should be avoided, except where needed for safety or where other traffic impact mitigation
1049 strategies are not feasible.

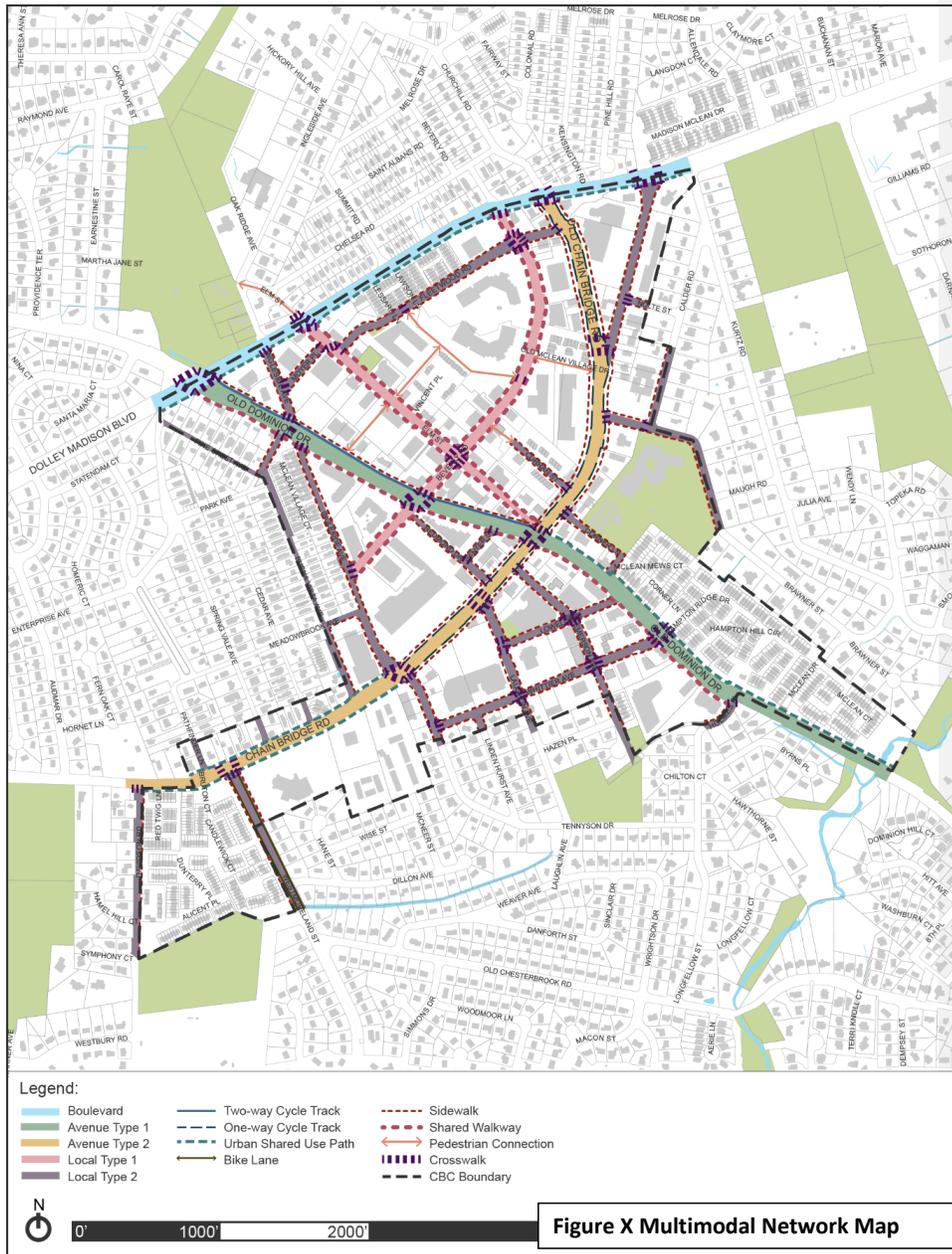
1050 The cross-sections include areas both within and outside of the right-of-way. The building
1051 zone (the area between the sidewalk and the face of the building) is the only area outside of the
1052 right-of-way. The width of the building zone is shown as consistently applied to all street types;
1053 however, the width is dependent upon the function of the adjacent land use. There is no required
1054 minimum building zone, provided that no portion of the building impedes upon the public right-
1055 of-way, including door swings.

1056
1057 In general, commercial development is recommended to provide a building zone 4 to 8 feet
1058 in width. When the ground-level use is retail, the building zone may be used for retail browsing or
1059 outdoor dining; a minimum of 8 feet is recommended to accommodate outdoor dining. Residential
1060 development is recommended to provide a building zone 8 to 12 feet in width to provide an effective
1061 transition and privacy between the public sidewalk and residences. When adjacent uses are
1062 residential, supplemental plantings (e.g. shade and flowering trees, shrubs, flowering plants, ground
1063 cover, and grasses) may also be located in the building zone. Ground-floor residences with
1064 individual entrances should be grade-separated from the public sidewalk to provide some privacy.
1065 When grade separation cannot be achieved, a landscaped building zone should be provided between
1066 the residence and the public sidewalk. Typically, the building zone should not exceed 12 feet in
1067 width. Exceptions to the building zone width may occur where plazas, urban parks, or spaces for
1068 public art are located. Upper levels of a building may be set back further than the ground floor to
1069 allow light and air to reach the street.

1070 Detailed guidance on other elements of the cross-sections, including street trees, landscape
1071 amenity panels, width and design to ensure adequate soil volume for shade trees, and building zone
1072 designs found in the *Volume I: Urban Design Guidelines for Commercial Revitalization Districts*
1073 *and Areas* should be consulted in the consideration of development proposals.

Commented [GJ10]: STAFF RECOMMENDATION: A building zone is expected to be provided with each development to support a high-quality pedestrian realm and to accommodate elements such as building entrances, outdoor dining, plantings and residential porches or stoops. No portion of the zone including door swings should impede upon the public right-of-way.

1076



1077 **Old Dominion Drive (Avenue Type 1) between Dolley Madison and Chain Bridge Road**

1078
1079 Old Dominion Drive is an arterial road (*Figure xx*) that connects local streets to higher-
1080 speed, higher-volume facilities, like Dolley Madison Boulevard. The existing and planned roadway
1081 condition is four lanes from Lowell Avenue to Dolley Madison Boulevard and two lanes from
1082 Lowell Avenue to the southeastern boundary of the CBC. Continuous bicycle facilities and
1083 sidewalks are recommended to accommodate bicycle and pedestrian travel. A median area may be
1084 necessary to provide a pedestrian refuge and/or allow for the provision of turn lane(s).

1085 For the section of roadway south of Chain Bridge Road, where the cross-section does not
1086 apply, the streetscape elements should transition to connect to existing trails and sidewalks as
1087 shown in Figure X Multimodal Network Map.

1088
1089 *Cross-section elements and dimensions for Old Dominion Drive between Dolley Madison*
1090 *Boulevard and Holmes Place (Arterial):*

1091
1092 Within the right-of-way (97-foot):

- 1093 • Median – A 12-foot typical median to accommodate vehicular turning movements,
1094 landscaping, or pedestrian refuge (the width may vary based on anticipated traffic volumes).
- 1095 • Drive Lanes – Two travel lanes per direction (10.5-foot width can be considered for some
1096 lane locations, but not wider than 11-feet). Pending further study, travel lanes near curb may
1097 be designated as parking lanes during off-peak hours.
- 1098 • Landscape Panel – A 6-foot wide panel for landscaping and amenity areas on both sides of
1099 the street.
- 1100 • Pedestrian and Bicycle Pathway – A 12-foot, off-road, flush facility delineated as 5-foot for
1101 cyclists, a 2-foot transition area, and a 5-foot sidewalk, inclusive of the VDOT-required 1-
1102 foot maintenance buffer, on both sides of the street, to accommodate varying pedestrian,
1103 bicycle and scooter traffic.

1104
1105 Outside of the right-of-way:

- 1106 • Building Zone – See building zone guidance under Cross-sections and Streetscape Design.

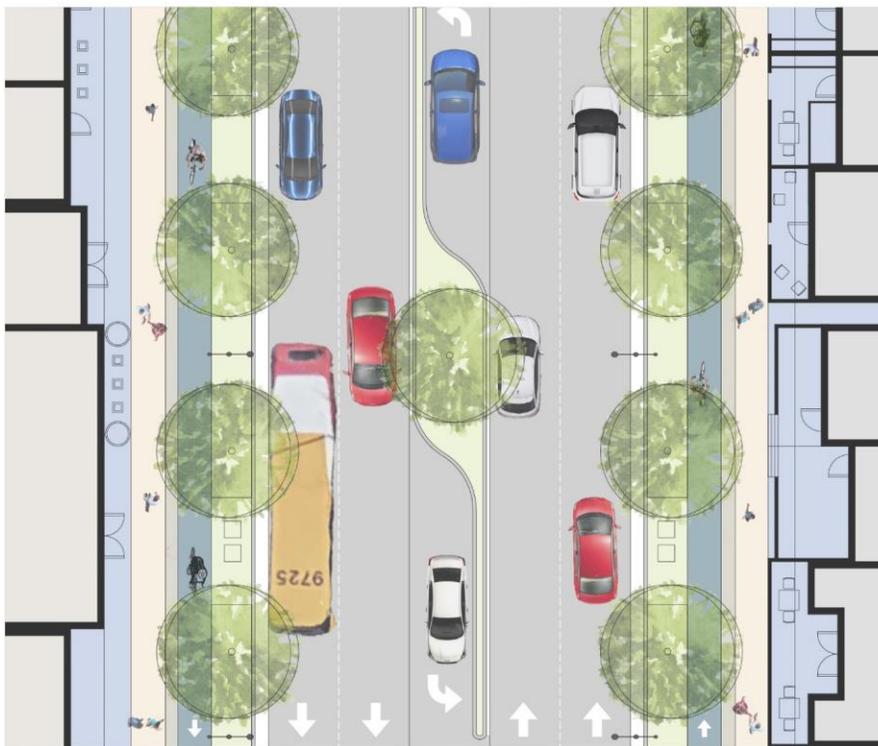
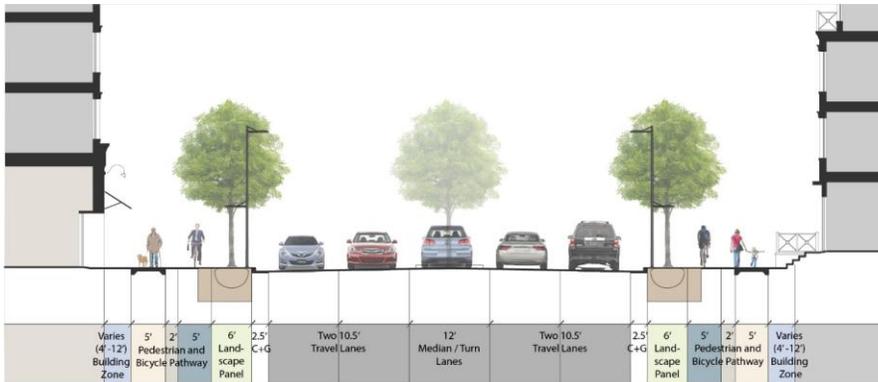
1107
1108 *Pedestrian and bicyclist facilities and dimensions for Old Dominion Drive between Holmes Place*
1109 *and the southeastern boundary of the CBC (Arterial):*

- 1110
1111 1) 8-10-foot Shared Use Path on both sides of the road.

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Commented [GJ11]: General task force support for new cross-section shown here. Formal vote was not taken.

1119 Figure XX Old Dominion Drive (Avenue Type 1) cross-section graphic between Dolley Madison
 1120 and Chain Bridge Road
 1121



1122

1123 **Chain Bridge Road (Minor Arterial)/Old Chain Bridge Road (Collector)**
1124

1125 Chain Bridge Road is a minor arterial (*Figure xx*) within the McLean CBC while Old Chain
1126 Bridge Road is a collector. Like Old Dominion Drive, Chain Bridge Road and Old Chain Bridge
1127 Road connects local streets to higher-speed, higher-volume facilities or other major streets. The
1128 existing and planned roadway condition is four lanes for Chain Bridge Road from Pathfinder Lane
1129 to Chain Bridge Road and for Old Chain Bridge Road from Chain Bridge Road to Dolley Madison
1130 Boulevard. Continuous bicycle facilities and sidewalks are recommended to accommodate bicycle
1131 and pedestrian travel. Medians may be necessary to provide a pedestrian refuge and/or turn lane(s).

1132 West of the intersection of Chain Bridge Road, Ingleside Avenue and Tennyson Drive,
1133 where the cross-section does not apply, the streetscape elements should transition to Urban Shared
1134 Use Paths and connect to existing trails and sidewalk as shown in Figure X Multimodal Network
1135 Map.

1136
1137 *Cross-section dimensions for Chain Bridge Road between Ingleside Avenue/Tennyson Drive and*
1138 *Chain Bridge Road (Minor Arterial) and for Old Chain Bridge between Chain Bridge Road and*
1139 *Dolley Madison Boulevard (Collector):*
1140

1141 Within the right-of-way (97-foot):

- 1142 • Median – A 12-foot typical median to accommodate vehicular turning movements or
1143 landscaping (the width may vary based on anticipated traffic volumes).
- 1144 • Drive Lanes – Two travel lanes per direction (10.5-foot width can be considered for some
1145 lane locations, but not wider than 11-feet). Pending further study, travel lanes adjacent to
1146 the curb may be designated as parking lanes during off-peak hours.
- 1147 • Landscape Panel – A 6-foot wide panel for landscaping and amenity areas on both sides of
1148 the street.
- 1149 • Cycle Track – A minimum 5-foot, off-road, one-way cycle track on each side of the road (a
1150 1-foot buffer should be provided to separate the cycle track from the sidewalk).
- 1151 • Sidewalk – A minimum 6-foot sidewalk, inclusive of the VDOT-required 1 -foot
1152 maintenance buffer, on both sides of the street.

1153
1154 Outside of the right-of-way:

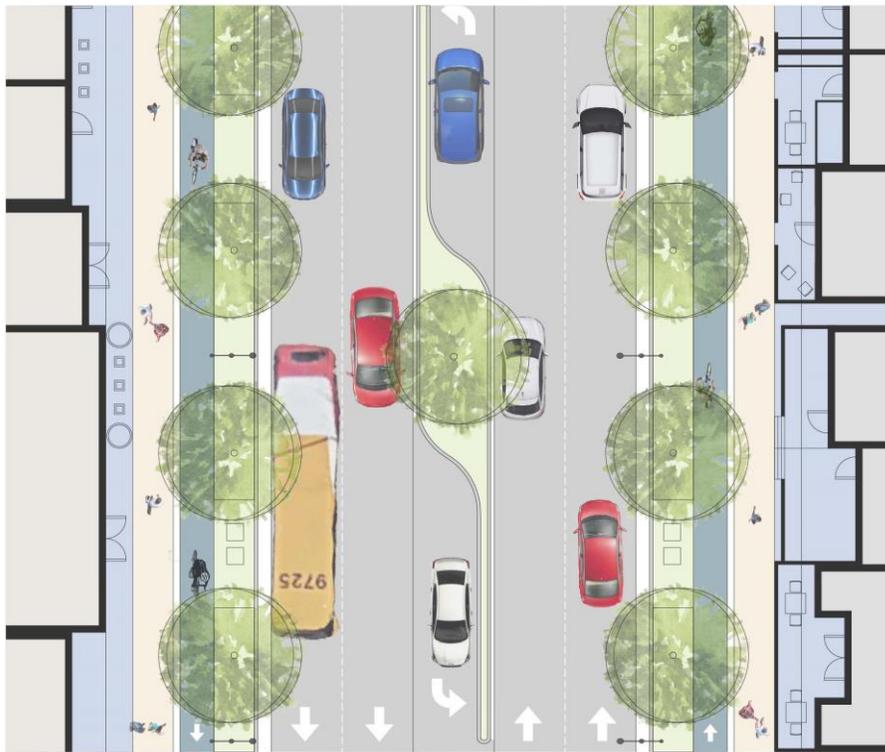
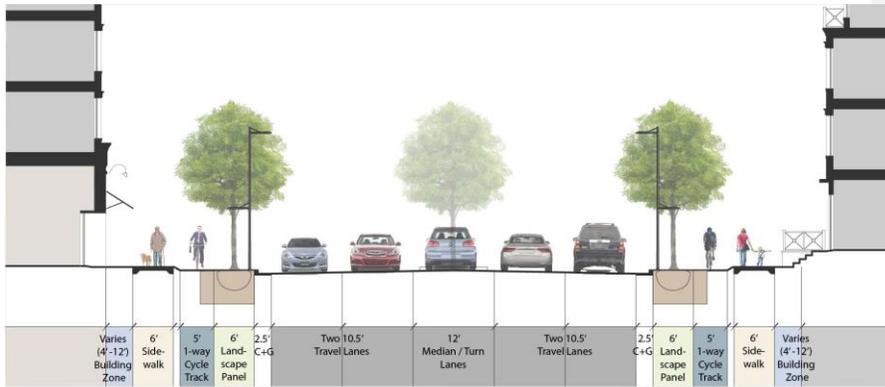
- 1155 1) Building Zone – See building zone guidance under Cross-sections and Streetscape Design.

1156 *Pedestrian and bicycle elements and dimensions for Chain Bridge Road from Ingleside*
1157 *Avenue/Tennyson Drive to Davidson Road:*
1158

- 1159 2) 8-10-foot Shared Use Path on both sides of the road.

1160
1161
1162

1163 Figure XX Chain Bridge Road (Avenue Type 2) cross-section graphic
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1167 **Local Street Type 1**

1168
1169 Beverly Road and Elm Street are recommended to be classified as Local Streets, Type 1
1170 (*Figure xx*) within the McLean CBC. These streets will generally have lower traffic volumes and
1171 slower moving traffic, compared to Chain Bridge Road and Old Dominion Drive. The cross-
1172 sections are narrow, with one drive lane in either direction. They are recommended to have parallel,
1173 on-street parking on one side of the road, at a minimum, with parallel parking on both sides of the
1174 road wherever feasible and appropriate. Measures to slow traffic such as raised mid-block
1175 pedestrian crossings, pedestrian-activated flashing lights, and sidewalk bulb-outs at intersections
1176 may be appropriate pending further study and coordination with VDOT.
1177

1178 *Local Street Type 1 Cross-section dimensions:*

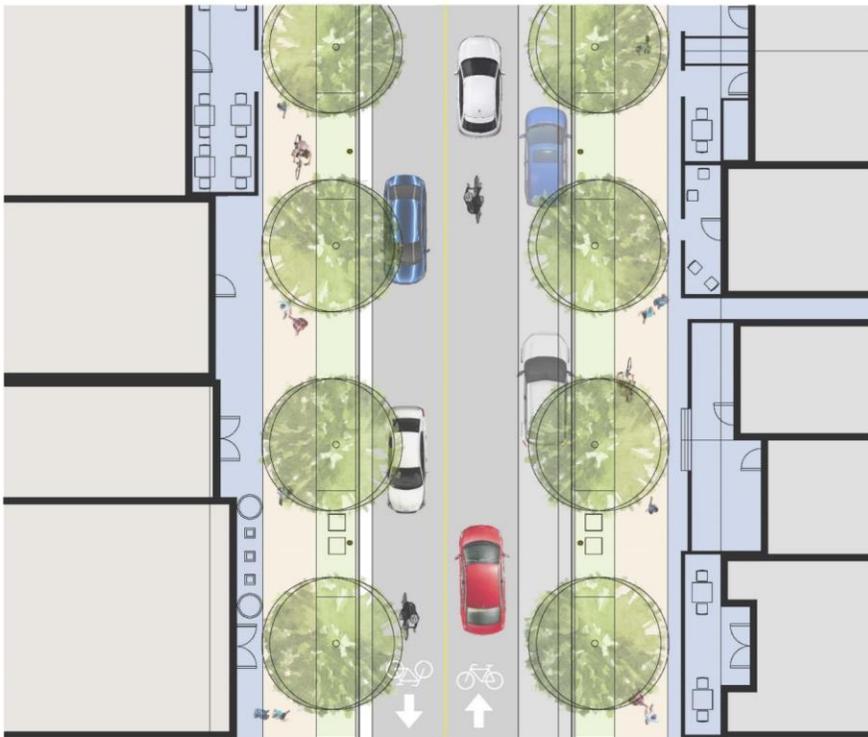
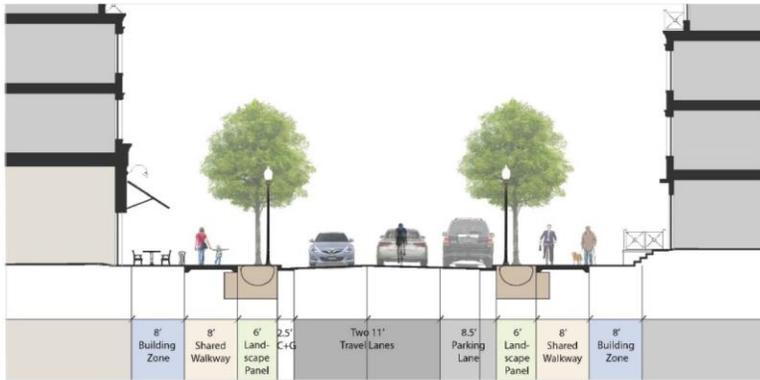
- 1179
1180 Within the right-of-way (61-foot minimum):
- 1181 • Drive Lane – One 11-foot travel lane per direction (typical for each lane). Sign as a bicycle
1182 route to indicate that bicyclists can use the travel lane.
 - 1183 • On-Street Parking – On-street parallel parking lane on one side of the street. If desired,
1184 and right-of-way is available, an additional on-street parking lane can be added on the other
1185 side of the road.
 - 1186 • Landscape Panel – A minimum 6-foot wide panel for landscaping and amenity areas on
1187 both sides of the street.
 - 1188 • Shared Walkway – 8-foot shared walkways, inclusive of the VDOT – required 1-foot
1189 maintenance buffer, on both sides of the street to accommodate pedestrians and bicyclists
1190 who do not feel comfortable riding in the street.

1191
1192 Outside of the right-of-way:

1193 Building Zone – See building zone guidance under Cross-sections and Streetscape Design.
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Figure XX: Beverly Road and Elm Street (Local Streets Type 1) cross section



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1214 **Local Street Type 2 – Other Streets and New Streets**

1215
1216 Like Beverly Road and Elm Street, the other local streets within the McLean CBC (*Figure*
1217 *xx*) will generally have low traffic volumes and slow-moving traffic. The cross-sections are narrow,
1218 with one lane in either direction, and are recommended to have parallel, on-street parking on one
1219 side of the road, with parallel parking on both sides of the road, wherever feasible. Measures to
1220 slow traffic, such as raised mid-block pedestrian crossings and sidewalk bulb-outs at intersections,
1221 may be appropriate, pending further study and coordination with VDOT. The character of the
1222 streetscape, including dimensions of elements, should generally be determined by the type of
1223 pedestrian activity generated by the adjacent land uses.

1224
1225 Due to low vehicle speeds, bicycles may be accommodated in the travel lane, rather than
1226 in a dedicated bicycle lane, unless otherwise noted on the Multimodal Network Map (Figure XX).

1227 *Local Street Type 2 Cross-section dimensions:*

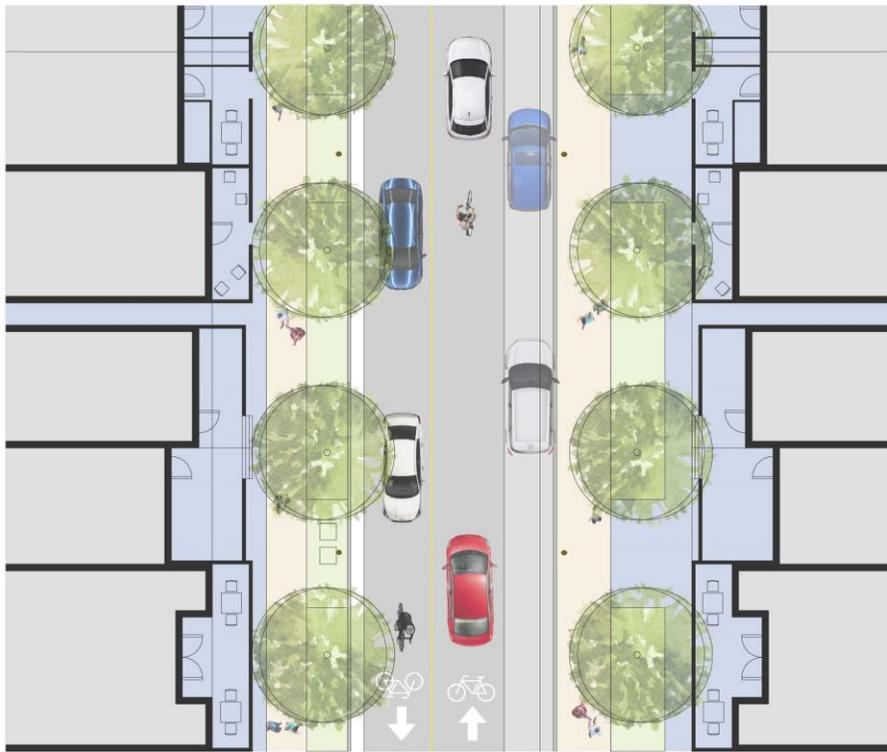
- 1228
1229 Within the right-of-way (57-foot minimum):
- 1230 • Drive Lane – one travel lane per direction (11-foot typical for each lane).
 - 1231 • On-Street Parking – On-street parallel parking lane on one side of the street. If desired and
1232 right-of-way is available, an additional on-street parking lane can be added on the other side
1233 of the road, where appropriate.
 - 1234 • Landscape Panel – A minimum 6-foot wide panel for landscaping and amenity areas on
1235 both sides of the street.
 - 1236 • Sidewalk – A minimum 6-foot sidewalk, inclusive of the VDOT-required 1-foot
1237 maintenance buffer, on both sides of the street. If the sidewalk is provided adjacent to the
1238 curb, additional space is needed to accommodate streetlights, signs and other elements while
1239 maintaining a 6-foot clear zone.

1240
1241 Outside of the right-of-way:

- 1242 • Building Zone – See building zone guidance under Cross-sections and Streetscape Design.
1243 Where street trees cannot be accommodated within the right-of-way, they should be
1244 provided within the building zone. Single family residential uses should use the Zoning
1245 Ordinance setback rather than providing a building zone.

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1257 Figure XX: Other streets and new streets (Local Streets Type 2) cross section
 1258



1259

1260 URBAN DESIGN

1261
1262 The Center Zone is recommended to have the highest intensity mix of uses and a public
1263 park that can accommodate community activities to create a town center character. Development
1264 in this zone should foster a walkable, vibrant environment and opportunities for activity throughout
1265 the day. Residential uses should be multi-family and are expected as part of mixed-use
1266 development. Within the General Zone, low to mid-rise development is intended to provide a
1267 transition from the Center Zone to the Edge Zone, and in some cases to single-family
1268 neighborhoods outside of the CBC. The Edge Zone is primarily low-density residential
1269 development and is planned to retain the low density and intensity character, as it provides a buffer
1270 between the CBC and single family detached residential neighborhoods.

1271 In order to achieve the vision for the CBC, urban design is used to align the desired scale
1272 and character of development with the social, economic, and aesthetic values of a community. It
1273 guides the physical features that define the arrangement and appearance of building form, open
1274 spaces, streets, blocks, and communities. The recommendations regarding the desired character of
1275 the built environment are contained in the Design Guidelines, which serve as a companion
1276 document to the Comprehensive Plan. The McLean CBC is also subject to in the Volume I: *Urban*
1277 *Design Guidelines for Fairfax County Commercial Revitalization Districts and Areas*. A future
1278 volume of design guidelines containing McLean CRD-specific details and recommendations to
1279 guide the implementation of the plan is anticipated to be developed.

1280 Site Design and Street Network

1281
1282 The design of sites and blocks should create an inviting, vibrant, and walkable environment
1283 that is scaled for the pedestrian. The pedestrian experience is influenced significantly by the scale
1284 of buildings that are located adjacent to the sidewalk. Buildings should be sited and designed to
1285 create a sense of enclosure for pedestrians, with connections to create walkable blocks. Typically,
1286 active storefronts and other uses that can engage pedestrians such as outdoor seating should be
1287 located close to the sidewalk. Ground floor non-residential uses should be accessed directly from
1288 the adjacent public sidewalk or building zone. The location of buildings or other site features should
1289 not interrupt the pedestrian circulation system. Loading docks, mechanical rooms, utility vaults,
1290 exposed parking garages, or other uses that detract from the public realm and should be located on
1291 shared lanes or alleys, or placed internally to the building envelope to minimize their negative
1292 aesthetic impacts.

1293
1294 Existing buildings will not necessarily conform to the building setback established by an
1295 adjacent proposed development. Development proposals, especially projects that are phased,
1296 should incorporate visual and physical linkages to existing buildings to create a high-quality
1297 pedestrian realm. New buildings may also use landscaping or other architectural features to visually
1298 align with existing buildings.

1299 Building Articulation and Facades

1300
1301 Building articulation, or changes in the façade, should be used to visually reduce the scale of
1302 a building and avoid monotonous building elevations. The façades of ground floor uses such as
1303 residential lobbies and common areas should be primarily transparent. Windows should provide
1304 building detail and visual interest and not contain opaque, mirrored, or translucent glass. Long
1305 expanses of blank walls without windows or entrances detract from the pedestrian experience and
1306 are discouraged. If blank façades cannot be avoided, strategies should be used to mitigate their
1307 impact on the public realm.

1308 In residential buildings, the degree of transparency in the ground floor should consider private
1309 uses, such as living areas. Residential lobbies and other common spaces should demonstrate higher
1310 transparency and provide a visual connection to the outside. Ground-floor residences with
1311 individual entrances should be grade-separated from the public sidewalk to provide some privacy.
1312 Stoops, bays, porches or entries that establish a distinct transition between private residential use
1313 and the public realm are encouraged. When grade separation cannot be achieved, a landscaped
1314 building zone should be provided between the residence and the public sidewalk. Stairs or porches
1315 should not encroach on the sidewalk so as to not impact pedestrian movement.

1316 Variations in individual building height as well as among buildings within the same block
1317 should be incorporated to distinguish uses, provide visual interest, and denote transitions between
1318 the Central, General and Edge zones. Sites should be designed to achieve the desired building
1319 height and/or intensity goals while remaining sensitive to the impact on the surrounding context.
1320 Building massing should allow for light at the street level and minimize long periods of shadow on
1321 the street, on adjacent buildings, and on open spaces.

1322 Signage and Wayfinding

1323
1324 Generally, signage should be integrated with building architecture to avoid visual clutter.
1325 Building-mounted signs or monument-style ground-mounted signs incorporated within the
1326 building zone are encouraged. Pedestrian-scaled signage should be incorporated into all new uses.
1327 Pole-mounted signs are discouraged.

1328 Wayfinding includes tools to orient people within their surroundings and to enhances their
1329 understanding of places. Wayfinding measures should be incorporated as appropriate to help people
1330 navigate the physical environment and to contribute to the overall identity of McLean through use
1331 of consistent themes. A coordinated program of public art, signage, historic markers, and/or other
1332 way-finding elements throughout the CBC should be considered to facilitate placemaking and
1333 navigation as well as to provide information about McLean.

1334 Public Art

1335
1336 Public art can help build authenticity and community vitality, ~~can~~ recall historically
1337 significant events and persons, ~~can~~ increase a sense of pride and place, and ~~can~~ create an inviting
1338 and attractive environment for residents, employees, and visitors. Private developments and public
1339 spaces are encouraged to include art in their design as per Policy Plan guidance. Art installations
1340 should be located in prominent public spaces and integrated with other urban design features.

1342 Parking Types and Design

1343
1344 The proper location and amount of parking is essential to sustaining commercial uses in the
1345 McLean CBC. Parking ~~areas~~ should be designed to minimize conflicts between vehicles and
1346 pedestrians. Vehicular access to parking lots and garages should be limited to local streets, shared
1347 lanes, or alleys when feasible.

1349 Underground and Structured Parking

1350 ~~Most parking~~ Parking is expected to be accommodated in structures or placed underground
1351 under the optional level of development within the Center and General Zones. Of these two parking
1352 types, uUnderground parking is ~~the~~ the preferred approach ~~method for providing parking with~~
1353 ~~development in the Center Zone.~~ However, the provision of underground parking may not always
1354 be feasible, particularly in the General Zone, in which case ~~Therefore,~~ above-grade structured

1355 parking, or podium parking, may be appropriate in the Center Zone. Structured parking is also
1356 encouraged in the General Zone, particularly for office and multifamily residential uses.

1357
1358 Throughout the CBC, parking structures should be integrated into buildings and
1359 freestanding parking structures are discouraged. In all cases, efforts should be made to limit the
1360 visual impacts of structured parking on the community. ~~Where the facades of parking structures~~
1361 ~~are exposed, architectural detailing, lighting, and landscaping should be employed to mitigate~~
1362 ~~negative visual impacts. Access to parking structures should be attractive and coordinated with~~
1363 ~~the architecture of the building through the use of architectural treatments on doors or similar~~
1364 ~~treatments. Consideration should be given to reducing glare and other potential negative visual~~
1365 ~~impacts from light sources. Where the facades of parking structures are exposed, architectural~~
1366 ~~detailing, lighting, and landscaping should be employed to mitigate negative visual~~
1367 ~~impacts. Exposed parking structures are strongly discouraged adjacent to parks and plazas. Access~~
1368 ~~to parking structures should be attractive and coordinated with the architecture of the building~~
1369 ~~through the use of architectural treatments on doors or similar treatments. Consideration should be~~
1370 ~~given to reducing glare and other potential negative visual impacts from light sources. Where the~~
1371 ~~facades of parking structures are exposed, architectural detailing, lighting, and landscaping should~~
1372 ~~be employed to mitigate negative visual impacts. Exposed parking structure facades are strongly~~
1373 ~~discouraged adjacent to parks and plazas.~~

1374
1375 *On-Street Parking*

1376 On-street parking provides convenient and accessible parking for residential and retail uses.
1377 ~~On-street parking also enhances while enhancing~~ the pedestrian experience by increasing the safety
1378 ~~and the level of comfort by providing space~~ between the travel lanes and the sidewalk of
1379 ~~the people using the sidewalk. On-street parking is recommended as part of the Local Street cross-~~
1380 ~~sections, and may also be feasible on other streets in the CBC after further study and analysis. On-~~
1381 ~~street parking should be parallel to the street. Angled and perpendicular on-street parking is~~
1382 ~~discouraged. Landscaped bulb-outs within on-street parking areas at intersections may be used to~~
1383 ~~reduce crosswalk distances for pedestrians.~~

1384
1385 *Surface Parking*

1386 ~~New surface parking lots are not envisioned under the optional level of development in the~~
1387 ~~Center and General Zones. However, adequate and convenient parking is essential for the~~
1388 ~~economic vitality of retail uses. Therefore, a limited amount number of teaser surface parking~~
1389 ~~spaces in front of retail building uses may be appropriate. A limited number of at grade teaser~~
1390 ~~parking spaces to serve commercial uses may be appropriate in the General Zone.~~

1391
1392 ~~There may be instances where parking is proposed to support neighborhood-serving retail~~
1393 ~~in the form of surface parking in the General and Edge Zones. In these cases, it is generally~~
1394 ~~preferable to locate surface parking to the side or rear of a building, with clearly delineated~~
1395 ~~pedestrian connections to the associated building. Such lots should be intensively well-landscaped~~
1396 ~~and well-lit. They also should be designed to contribute to onsite stormwater management by using~~
1397 ~~elements such as planter areas and permeable paving in the parking stall area. The redesign and~~
1398 ~~consolidation of existing private surface parking lots is encouraged. Although surface parking lots~~
1399 ~~are not envisioned for and General Zone developments, if a surface parking lot is included to~~
1400 ~~support neighborhood serving retail uses, it should be located to the side or rear of the primary use~~
1401 ~~and should contain clearly delineated pedestrian connections to the associated building. Such lots~~
1402 ~~should be intensively landscaped and well lit. They also should be designed to contribute to onsite~~
1403 ~~stormwater management by using elements such as planter areas and permeable paving in the~~
1404 ~~parking stall area. The redesign and consolidation of existing, private, surface parking lots is~~
1405 ~~encouraged.~~

1406 *****
 1407 *****

1408
 1409
 1410 LAND USE

1411
 1412 Land Units and Development Potential

1413
 1414 The McLean CBC is divided into twelve land units that correspond to the Center, General,
 1415 and Edge Zones as shown in Figure X, Land Units Map. Land Units C-1 and C-2 comprise the
 1416 Center Zone; Land Units G-1 through G-4 comprise the General Zone; and Land Units E-1 through
 1417 E-6 comprise the Edge Zone. Recommendations for a baseline and optional level of development
 1418 are described below. In some instances, existing development may be greater than the planned
 1419 baseline intensity. Plan recommendations regarding park spaces, the character of development,
 1420 urban design, transportation, implementation, building heights, and other guidance found in this
 1421 Plan should be used in the evaluation of development proposals.

1422
 1423
 1424 *Optional Level of Development in Center and General Zones – Form Based Approach*

1425
 1426 The preferred vision for the Center and General Zones is guided by the descriptions for each
 1427 zone found in the Vision and Guiding Planning Principles section, as well as by the height map and
 1428 design recommendations. The preferred vision is articulated as an optional level of development
 1429 above the base plan. It is envisioned to be implemented through a form-based approach that
 1430 includes an overall total amount of development potential within the Center and General Zones. ~~is~~
 1431 ~~recommended as an optional level of development above the base plan, using a form-based~~
 1432 ~~approach that encourages flexibility in the mix of uses within a total amount of development~~
 1433 ~~potential for the Center and General Zones. The Plan allows for~~ This approach encourages
 1434 flexibility among the in the mix and types of non-residential uses, provided that the total amount
 1435 of non-residential uses is not exceeded, and that the development achieves the overall vision for
 1436 the zones CBC. ~~Plan recommendations regarding park spaces, the character of development, urban~~
 1437 ~~design, transportation, implementation, building heights, and other guidance found in this Plan~~
 1438 ~~should be used in the evaluation of development proposals. NOTE – will include guidance about~~
 1439 ~~being able to achieve a 3.0 FAR per proffer legislation~~

1440
 1441 Figures 10 and 11 include the maximum planned development potential for the McLean CBC
 1442 and the Center and General Zones, respectively individually. The Plan potential is expressed in
 1443 terms of a total amount of residential dwelling units and as a total amount of square feet of
 1444 nonresidential use. The Plan potential for the Edge Zone is described in the individual land units
 1445 that comprise the Edge Zone that zone.

1446
 1447 Figure 10: Planned Development Potential for the McLean CBC

Land Use Category	Plan Potential
Total Residential	3,850 dwelling units
Total Non-residential	3,150,000 square feet

1448
 1449 Figure 11: Planned Development Potential for the Center and General Zones

Land Use Category	Plan Potential
Residential	3,150 dwelling units
Non-residential	2,705,000 square feet

1450

1451 Note: In Figures 10 and 11, the residential development potential does not include housing bonuses allowed under the
1452 Affordable Dwelling Unit Ordinance and the Guidelines for the Provision of Workforce Housing.

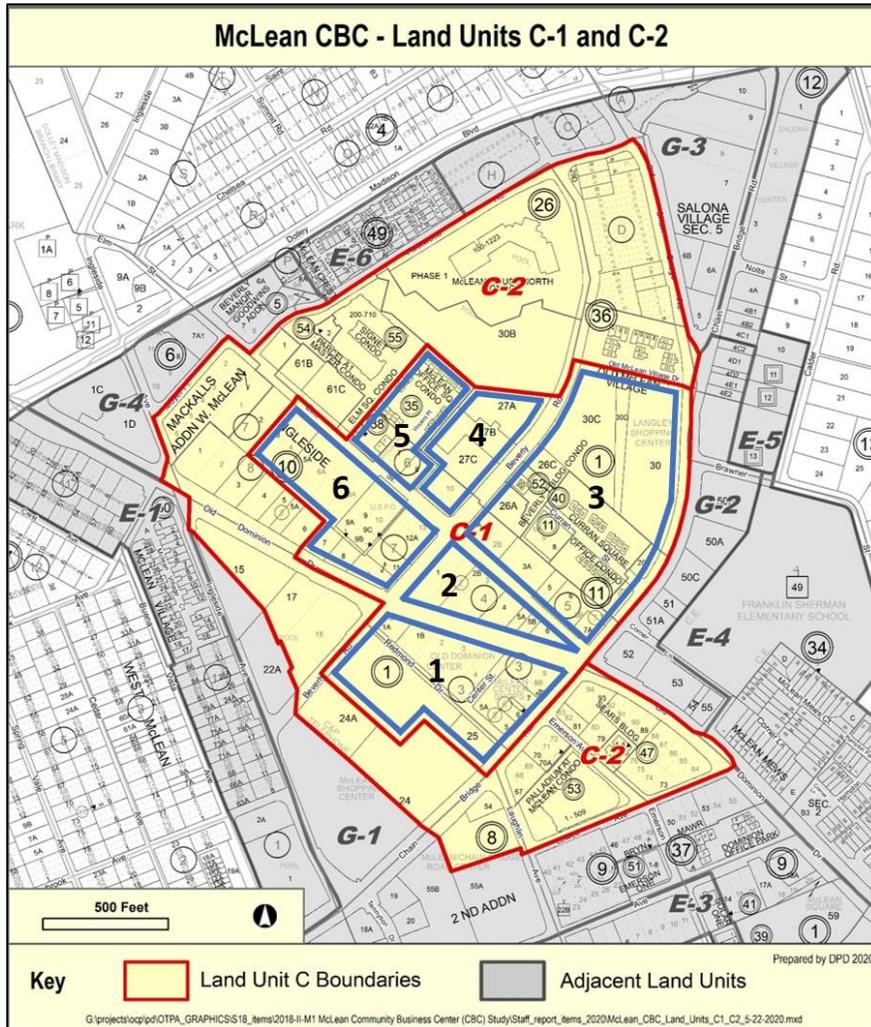
1453
1454 *Base Plan*

1455
1456 The base plan generally reflects the existing and/or approved uses and intensities for each land unit.
1457 In some cases, an average intensity or density is provided for a parcel grouping and conformance
1458 with the Comprehensive Plan should be evaluated by calculating the average intensity across the
1459 group of parcels. The specific base plan recommendations below are for groups of parcels
1460 corresponding to the following maps. If an individual parcel seeks a special exception or special
1461 permit, or other type of application that includes a review of the adopted Comprehensive Plan
1462 recommendations, then the existing intensity or density should be used for those reviews. If the
1463 parcel is developed at a higher intensity or density than its base recommendation, then it may still
1464 be considered in conformance with the base intensity recommendations of the Comprehensive Plan.

1465
1466 Center Zone: Land Unit C-1

1467
1468 Land Unit C-1 is primarily developed with neighborhood retail and commercial uses as
1469 shopping centers, restaurants, and offices.

1470



1471
 1472

Base Plan

1474 The baseline recommendation for Land Unit C 1 is for non residential use, with including
 1475 neighborhood serving retail uses, and office uses at an intensity up to 0.35 FAR, with a minimum
 1476 of 30 percent retail use as part of the non residential Non residential uses for in the area north of
 1477 Old Dominion Drive along Chain Bridge Road should be a minimum of 30 percent retail. Of that
 1478 retail use, half is expected to be community and/or neighborhood serving and the remainder is

1479 ~~encouraged for restaurant and entertainment, with office for the residual. The exceptions to this~~
1480 ~~recommendation include the following:~~

1481
1482 The specific base plan recommendations below are for groups of parcels corresponding to the map
1483 of Land Unit C-1.

1484
1485 1) ~~The area bounded by Redmond Drive, Chain Bridge Road, and Beverly Road is~~
1486 ~~recommended for neighborhood serving retail and office uses at an intensity up to 0.35~~
1487 ~~FAR. The area bounded by Chain Bridge Road, Old Dominion Drive, Beverly Road, and~~
1488 ~~Land Unit C-2 is planned for neighborhood serving retail and office uses at an intensity up~~
1489 ~~to 0.35 FAR. Fifty percent of the retail use should be community serving. The former~~
1490 ~~McLean Fire Station, now known as the The “Old Firehouse”, has been adaptively reused~~
1491 ~~and should be maintained as an historic structure, and the The areas around it should be are~~
1492 ~~planned for a public gathering space. The building should be maintained as an historic~~
1493 ~~structure. It has been adaptively reused. All development should respect the historic~~
1494 ~~integrity of the resource.~~

1495
1496 2) The triangular area bounded by Old Dominion Drive, Beverly Road, and Elm Street is
1497 planned for ~~and developed with~~ office and retail uses at an average intensity of
1498 approximately 0.45 FAR.

1499
1500 3) The area ~~is~~ bounded by Elm Street, Chain Bridge Road, Land Unit C-2, and Beverly Road
1501 is planned for ~~and developed~~ retail and office uses at an average intensity of 0.40-~~35~~ FAR.

1502
1503 4) The parcels along the northside of Beverly Road (Tax Map 30-2 ((1)) 27A, 27B, and 27C;
1504 and 30-2 ((10)) (6) 9 are planned for office at an intensity up to 0.35 FAR.

1505
1506 5) The parcels along the north side of Elm Street (Tax Map Parcels 30-2 ((10)) (6) 7, and 8,
1507 ~~and 9;~~ Tax Map Parcels 30-2 ((38)) 1, 2, and 3; and Tax Map Parcels 30-2 ((35)) 1-23) are
1508 planned for ~~and developed with~~ office and ground-floor retail uses at an average intensity
1509 up to 0.50-~~45~~ FAR.

1510
1511 6) The parcels along Old Dominion Drive, Beverly Road, and the south side of Elm Street
1512 (Tax Map Parcels 30-2((10)) (7) 3, 5A, 9, 11, and 12A; Tax Map Parcels 30-2 ((10)) (8) 7,
1513 and 8), ~~is are~~ planned for office and retail uses up to 0.50 FAR at an average intensity of
1514 0.75 FAR.

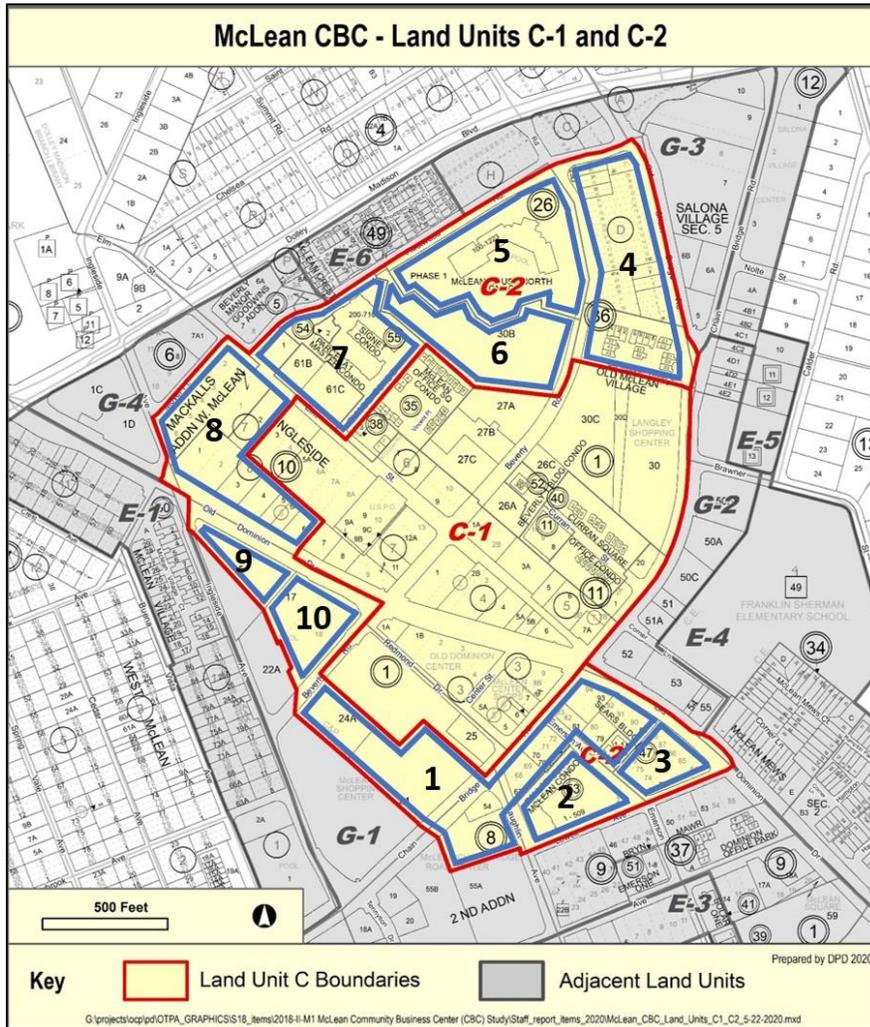
1515

Commented [SDB12]: The Task Force voted to remove language regarding the Old Firehouse. Staff recommends retaining this language.

1516 *Option for Land Unit C-1 Optional Level of Development – Special Considerations*
1517 ~~As an option, future development and redevelopment in Land Unit C-1 will be guided by~~
1518 ~~the vision and character described for the Center Zone. For this land unit, the option recommends~~
1519 ~~a mix of uses at a maximum building height of seven stories, guided by the development potential~~
1520 ~~for the Center and General Zones shown in Figure 11. Land Unit C-1 encompasses includes the~~
1521 ~~“Bonus Height Area” identified on the Height Map. Within Land Unit C-1, a proposal that includes~~
1522 ~~consolidation of four to six acres may be developed with building heights up to ten stories provided~~
1523 ~~— The additional height will be considered if that the development provides a vibrant, mixed-use,~~
1524 ~~pedestrian-oriented place with a central urban park. Other Plan recommendations regarding the~~
1525 ~~public park space, character of development, urban design, transportation, implementation, and~~
1526 ~~other guidance found in this Plan should be used in the evaluation of development proposals.~~
1527 Additional public park spaces are envisioned in Land Unit C-1 as described by the Public Parks
1528 and Open Space Concept.
1529

1530 Center Zone: Land Unit C-2

1531
1532 Land Unit C-2 is developed with a mix of uses, including neighborhood retail and
1533 commercial uses as shopping centers, restaurants, and offices. Multi-family residential uses include
1534 the Signet, the McLean House, the Ashby at McLean, and the Palladium, ~~and Benchmark.~~
1535



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Base Plan

The baseline recommendation for Land Unit C 2 is neighborhood serving retail uses and office uses at an intensity up to 0.35 FAR. The exceptions to this recommendation include the following:

1543 The specific base plan recommendations below are for groups of parcels corresponding to the map
1544 of Land Unit C-2.

- 1545
- 1546 1) The area bounded by Chain Bridge Road, Land Unit C-1, Beverly Road and Land
1547 Unit G-1 and the parcels located along the south side of Chain Bridge Road and Old
1548 Dominion Drive are planned for neighborhood serving retail and office uses up to
1549 0.35 FAR.
- 1550
- 1551 2) The Palladium is planned for and developed with approximately 70 residential units,
1552 with ground floor commercial uses and publicly accessible open space.
- 1553
- 1554 3) Tax Map 30-2 ((9)) 73 is planned for office uses with ground floor retail and
1555 multifamily residential use at an intensity up to 2.0 FAR. Intensities above .50 FAR
1556 should be comprised of predominantly residential use; the number of residential
1557 units should be limited to a maximum of 50; the height of any new residential
1558 building should not exceed 90 feet; building facades are articulated with upper floors
1559 stepped back to promote compatibility with adjacent nearby buildings; structured
1560 parking should be integrated into building design and architectural treatments
1561 should be used to minimize visual impacts; access to garages should be internal to
1562 the site and garages should not front onto streets; and adequate, useable on-site open
1563 space should be provided to serve the residents and provide amenities for the
1564 community.
- 1565
- 1566 4) The northeast portion of Land Unit C-2 located between Beverly Road and Old
1567 Chain Bridge Road, south of Fleetwood Drive is planned for ~~and developed office~~
1568 and retail uses at an average intensity of up to 1.035 FAR. Tax Map 30-2 ((4)) D-
1569 11B and D-47A is planned for office and self-storage at an intensity up to 1.25 FAR.
- 1570
- 1571 5) The McLean House is planned for and developed with residential use at a density of
1572 approximately 40 du/ac.
- 1573
- 1574 6) The Ashby at McLean is planned for and developed with residential use at a density
1575 of approximately 60 du/ac with limited ground floor retail use.
- 1576
- 1577 7) ~~The area bounded by Elm Street, Fleetwood Road, Beverly Road, and Land Unit~~
1578 ~~C-1, is Signet and Tax Map # 30-2 ((1)) 61C are planned for office and residential~~
1579 ~~uses with ground floor retail use at an intensity up to of .501.95 FAR. Tax Map~~
1580 ~~30-2 ((10)) (6) 1 is planned for office and ground floor retail at an intensity of 0.70~~
1581 ~~FAR.~~
- 1582
- 1583 8) The area bounded by Old Dominion Drive, Moyer Place, Elm Street, and Land Unit
1584 C-1 is planned for office use with ground floor retail use at an average intensity up
1585 to 0.50 FAR.

1586
1587 9) Tax Map Parcel 30-2 ((1)) 15 is located in both Land Units C-2 and G-1, and is
1588 planned for ~~and developed~~ office use at an intensity of approximately 0.80 FAR.

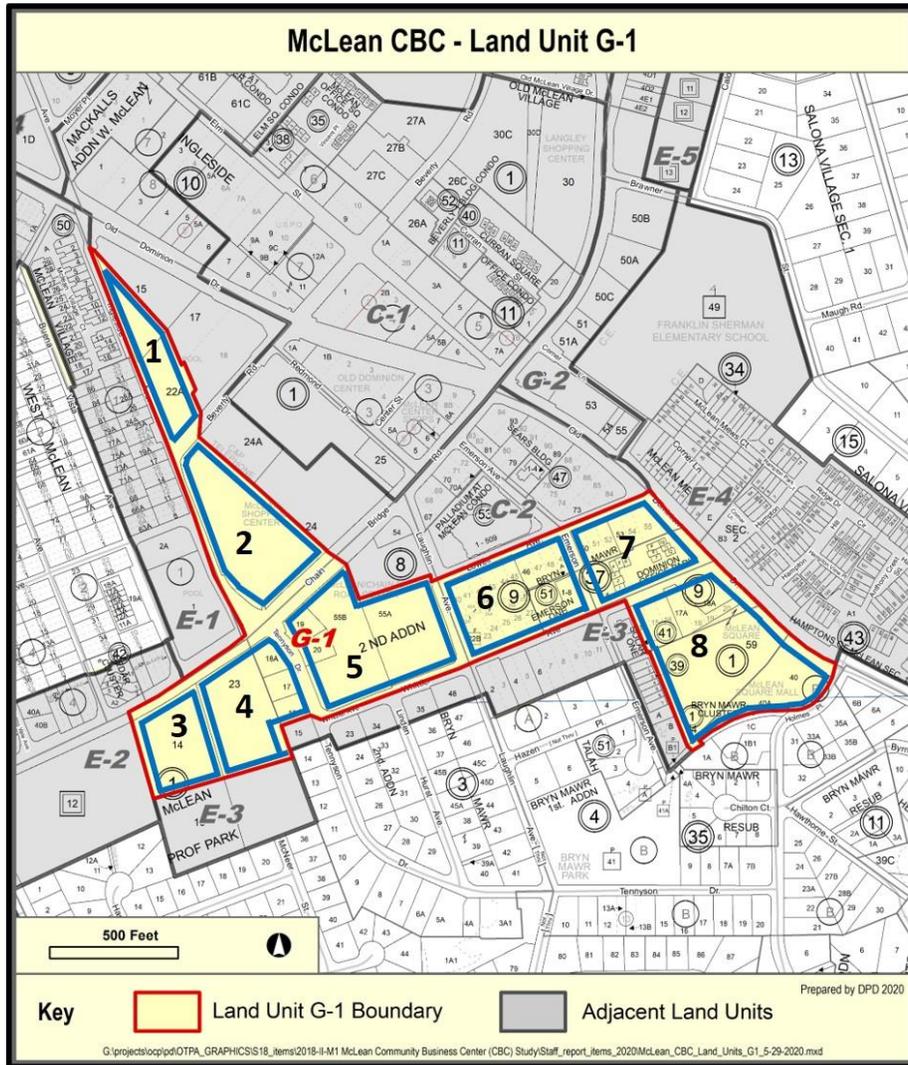
1589
1590 10) Tax Map Parcel 30-2 ((1)) 17 located at the southwest corner of Old Dominion Drive
1591 and Beverly Road is planned for hotel use at an intensity of approximately 1.0 FAR.

1592
1593 *~~Option for Land Unit C-2~~*

1594 ~~As an option, future development and redevelopment in Land Unit C-2 will be guided by~~
1595 ~~the vision and character described for the Center Zone. For this land unit, the option recommends~~
1596 ~~a mix of uses at a maximum building height of seven stories, guided by the development potential~~
1597 ~~for the Center and General Zones shown in Figure 11. Other Plan recommendations regarding the~~
1598 ~~public park space, character of development, urban design, transportation, implementation, and~~
1599 ~~other guidance found in this Plan should be used in the evaluation of development proposals.~~

1600
1601 General Zone: Land Unit G-1

1602
1603 Land Unit G-1 is generally developed with a mix of uses including institutional,
1604 commercial, neighborhood-serving retail, and office uses.
1605



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The specific base plan recommendations below are for groups of parcels corresponding to the map of Land Unit G-1.

Base Plan

The baseline recommendations for Land Unit G-1 are as follows:

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1649
- 1) The portion of the block bounded by Ingleside Avenue, Beverly Road, and Land Unit C-2 is planned for ~~and developed with office use and ground floor retail, at an average~~ Tax Map 30-2 ((1)) 15 is planned for an intensity up to .80 FAR and Tax Map 30-2 ((1)) 22 A is planned for an intensity of ~~approximately up to~~ 1.0 FAR.
 - 2) The area bounded by Beverly Road, Ingleside Avenue, Chain Bridge Road and Land Unit C-2 is planned for neighborhood-serving retail uses and low-density office uses up to an intensity of 0.35 FAR.
 - 3) Tax Map Parcel 30-4 ((1)) 14 is planned for and developed with low/medium intensity commercial office at an intensity of approximately 0.35 FAR. A medical care facility for assisted living up to 0.93 FAR was approved for the parcel.
 - 4) The portion of the McLean Professional Park in Land Unit G-1 is planned for ~~and developed with~~ low/medium intensity commercial office at an intensity of approximately 0.55 FAR and the McLean Commerce Center is planned for retail at an intensity of approximately 0.50 FAR.
 - 5) The area bounded by Tennyson Drive, Whittier Avenue, Laughlin Avenue, ~~and Chain Bridge Road, and Land Unit C-2~~ is planned for neighborhood-serving retail uses and low-density office uses up to an intensity of 0.35 FAR. ~~The northeast corner is within Land Unit C-2.~~
 - 6) The block bounded by Laughlin Avenue, Lowell Avenue, Emerson Avenue, and Whittier Avenue is planned for office and retail use at an average intensity up to of 0.70-40 FAR. Tax Map 30-2 ((9)) 22B is planned for public facilities use and developed with the McLean Fire Station # 1.
 - 7) The area bounded by Emerson Drive, Lowell Avenue, Old Dominion Drive, and Whittier Avenue is planned for ~~and developed with~~ retail and townhouse-style office at an intensity up to 0.40-35 FAR.
 - 8) The block bounded by Whittier Avenue, Old Dominion Drive, Holmes Place and edge of the CBC, and Land Unit E-3 is planned for office and retail use at an intensity up to 0.50 FAR.

Option for Land Unit G-1 Optional Level of Development – Special Considerations

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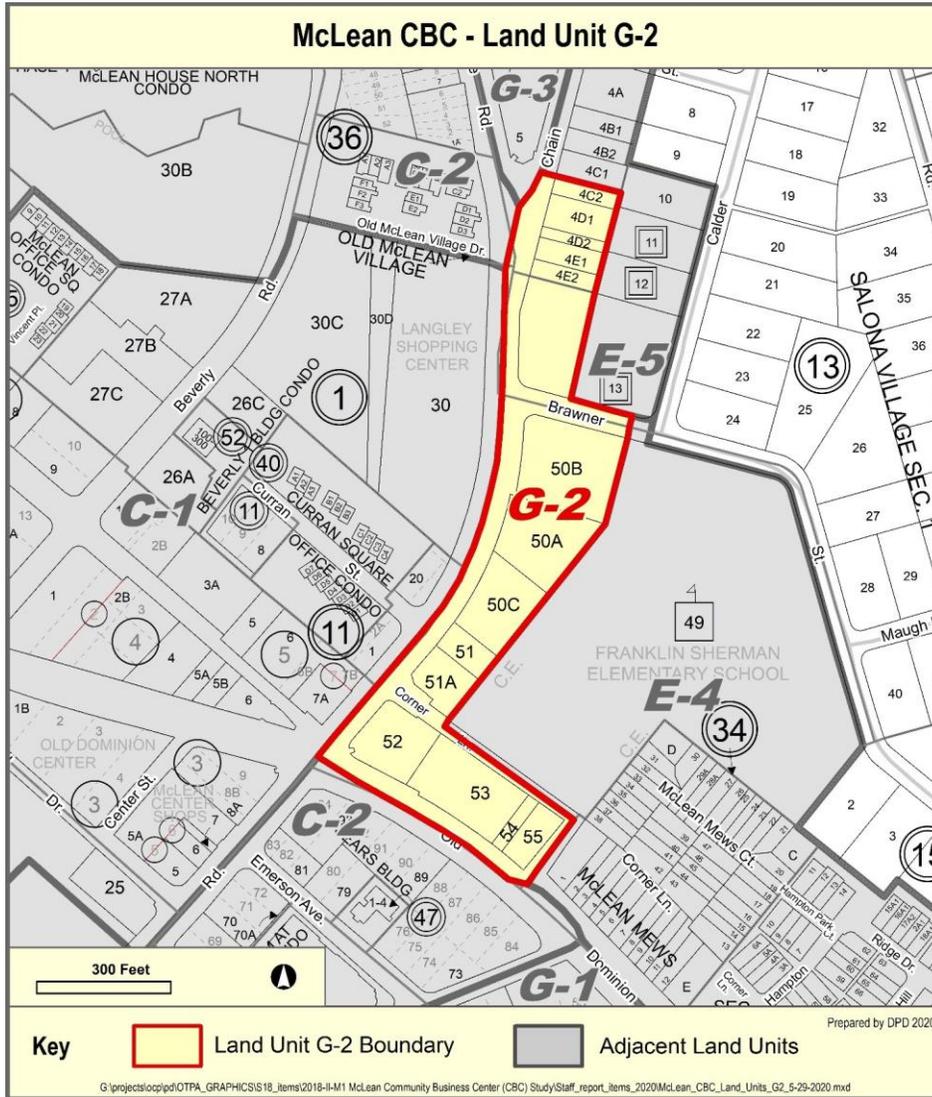
As an option, future development and redevelopment in Land Unit G-1 will be guided by the vision and character described for the General Zone. This option recommends a mix of uses at a maximum building height of five stories, guided by the development potential for the Center and

1655 ~~General Zones shown in Figure 11. The southeastern portion of Land Unit G-1 abuts the edge of~~
1656 ~~the CBC and is planned as a transition from the General Zone immediately adjacent to residential~~
1657 ~~uses both within and outside of the CBC, as there is no abutting without an Edge Zone to provide a~~
1658 ~~transition. Transitions between any new development and these existing residential uses should be~~
1659 ~~carefully designed, particularly as related to height. Portions of Tax Map Parcels 30-2 ((1)) 59 , 30-~~
1660 ~~4 ((1)) 74, and 30-4 ((4)) (B) 40 and 40A, developed with McLean Square, abut single-family~~
1661 ~~attached residential uses, outside the edge of the CBC boundary, and is planned as a without an~~
1662 ~~Edge Zone as a transition area, from the General Zone immediately to residential uses outside of~~
1663 ~~the CBC, as there is no Edge Zone in this location. This area also abuts townhouses along Emerson~~
1664 ~~Avenue in Land Unit E-3. Development in this area adjacent to the residential uses should be~~
1665 ~~designed as a transition area, with compatible building heights and high-quality landscaping.~~
1666 ~~Within a buffer area of approximately 75 feet, measured from the property boundaries closest to~~
1667 ~~the residential uses shown in Figure X, height should be limited to a maximum of three stories;~~
1668 ~~and an open space or landscaped buffer of a minimum of 25 feet along the edges should be provided.~~
1669 ~~This particular area transitions from the General Zone immediately to residential uses outside of~~
1670 ~~the CBC, without an Edge Zone as a transition area. Transitions between any new development and~~
1671 ~~the existing uses outside of the CBC should be carefully designed, particularly as related to height.~~
1672 ~~Plan recommendations regarding open space, character of development, urban design,~~
1673 ~~transportation, implementation, and other guidance found in the CBC Plan should be used in the~~
1674 ~~evaluation of development proposals.~~

1675
1676 General Zone: Land Unit G-2
1677

1678 Land Unit G-2 is developed with non-residential uses, including retail, office and
1679 institutional uses.
1680

Commented [SDB13]: Note: The task force voted to move the portions of these parcels within 75 feet from the adjacent residential uses to the Edge Zone. Proposed plan text is for consideration if this area is kept in the General Zone.



1681
 1682

1683 *Base Plan*

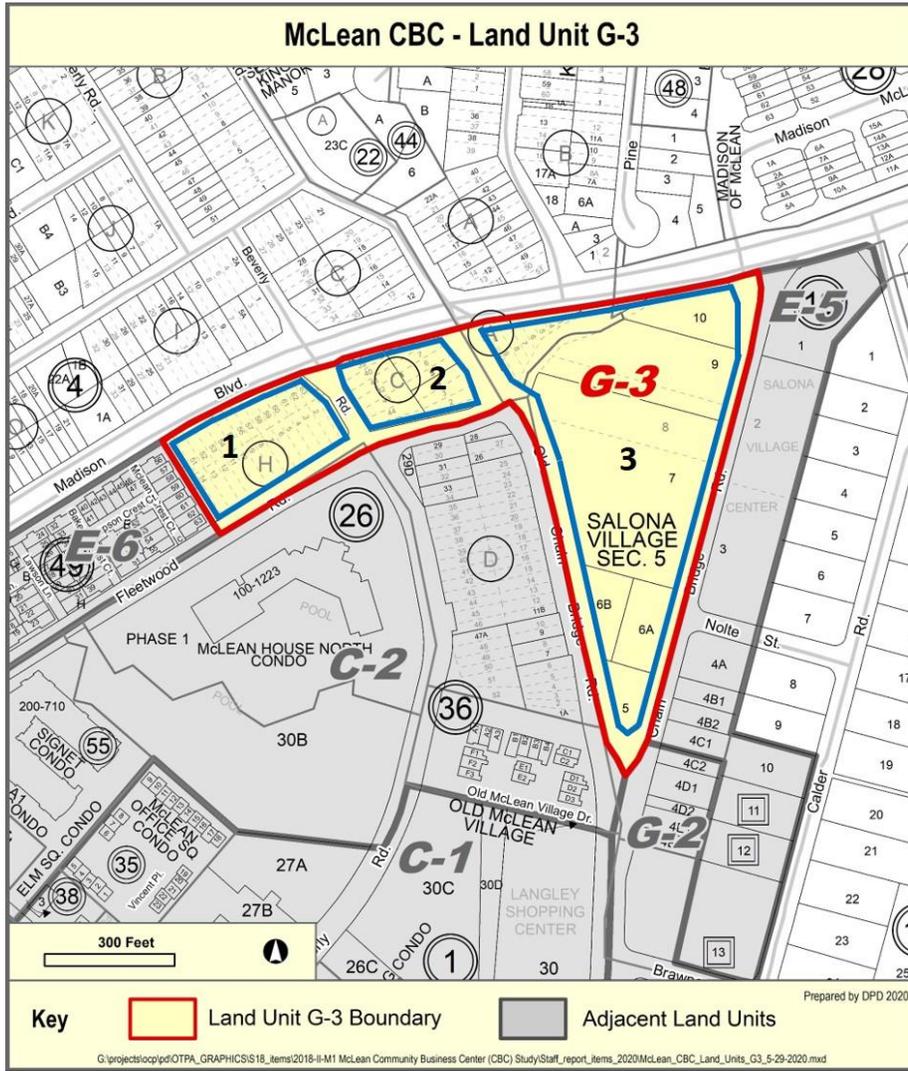
1684
1685 The baseline recommendation for ~~Land Unit G-2~~ is for non-residential uses, including
1686 community-serving retail and office uses at an intensity up to 0.35 FAR. The mini-park on Chain
1687 Bridge Road should be retained.

1688
1689 *Option for Land Unit G-2 Optional Level of Development – Special Considerations*

1690
1691 ~~As an option, future development and redevelopment in Land Unit G-2 will be guided by~~
1692 ~~the vision and character described for the General Zone. This option recommends a mix of uses at~~
1693 ~~a maximum building height of five stories, guided by the development potential for the Center and~~
1694 ~~General Zones shown in Figure 11. This land unit is bounded by Franklin Sherman Elementary~~
1695 ~~School along the eastern and northern edge. Transitions between new development and the school~~
1696 ~~should be carefully designed, particularly as related to height. Plan recommendations regarding~~
1697 ~~open space, character of development, urban design, transportation, implementation, and other~~
1698 ~~guidance found in this CBC Plan should be used in the evaluation of development proposals.~~

1699
1700 General Zone: Land Unit G-3

1701
1702 Land Unit G-3 is developed with retail and office uses.



1703
 1704
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Base Plan

The specific base plan recommendations below are for groups of parcels corresponding to the map of Land Unit G-3.

1710 The baseline recommendations for Land Unit G-3 are as follows:

- 1711
- 1712 1) The portion of the block bounded by Dolley Madison Boulevard, Beverly Road,
1713 Fleetwood Road and Land Unit E-6 is planned for and developed with office use at
1714 an intensity ~~at of~~ approximately 0.70-75 FAR.
- 1715
- 1716 2) The block bounded by Dolley Madison Boulevard, Beverly Road, Fleetwood Road,
1717 and Old Chain Bridge Road is planned for office use at an intensity up to
1718 ~~approximately 0.50-~~35 FAR.
- 1719
- 1720 3) The triangular area bounded by Dolley Madison Boulevard, Old Chain Bridge Road,
1721 and Chain Bridge Road is planned for ~~and developed with~~ predominately
1722 community-serving retail use, with options for office and entertainment uses, at an
1723 intensity up to ~~approximately of 0.50-~~35 FAR.

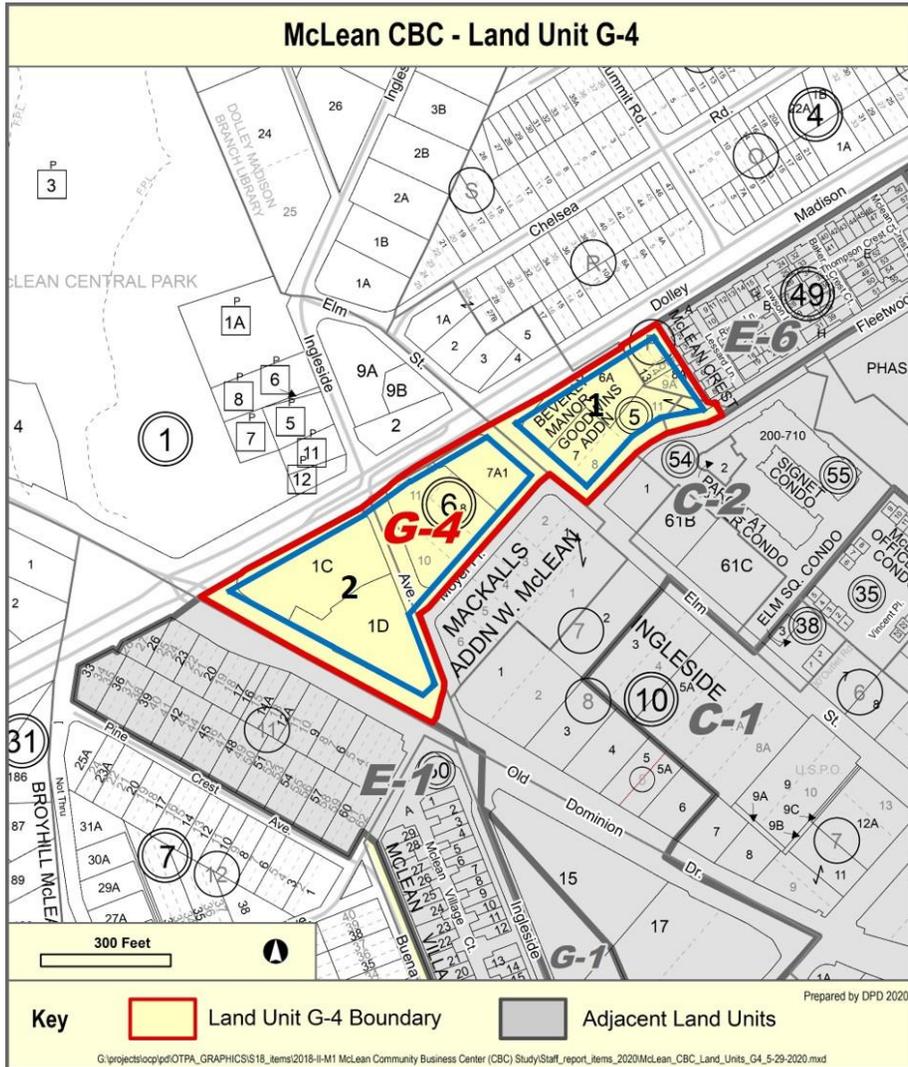
1724
1725 *Option for Land Unit G-3 Optional Level of Development – Special Considerations*

1726 ~~An urban park should be located in Land Unit G-3. As an option, future development and~~
1727 ~~redevelopment in Land Unit G-3 will be guided by the vision and character described for the~~
1728 ~~General Zone. This option recommends a mix of uses at a maximum building height of five stories,~~
1729 ~~guided by the development potential for the Center and General Zones shown in Figure 11. A~~
1730 ~~portion of this land unit is bounded by Dolley Madison Boulevard, with residential uses located~~
1731 ~~outside the CBC further north. Although Dolley Madison Boulevard provides separation from the~~
1732 ~~CBC, this particular area transitions from the General Zone immediately to residential uses outside~~
1733 ~~of the CBC, without an Edge Zone as a transition area. Transitions between any new development~~
1734 ~~and the existing uses outside of the CBC should be carefully designed, particularly as related to~~
1735 ~~height. An urban park should be located in Land Unit G-3. Plan recommendations regarding open~~
1736 ~~space, character of development, urban design, transportation, implementation, and other guidancee~~
1737 ~~found in this CBC Plan should be used in the evaluation of development proposals.~~

1738
1739 General Zone: Land Unit G-4

1740
1741 Land Unit G-4 is developed with office and retail uses.

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Base Plan

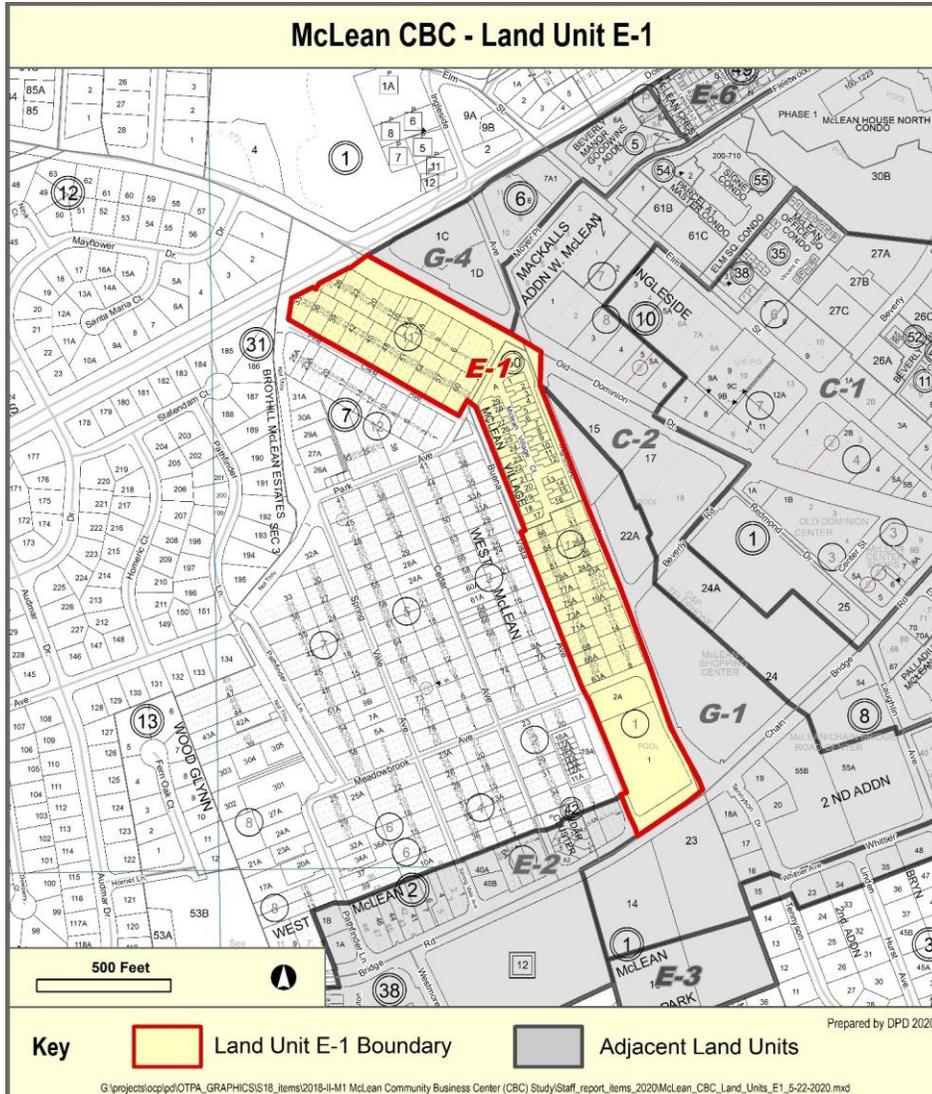
The specific base plan recommendations below are groups of parcels corresponding to the map of Land Unit G-4.

- 1751
1752 The baseline recommendation for Land Unit G-4 are as follows:
1753
1754 1) The block bounded by Dolley Madison Boulevard, Elm Street, Fleetwood Road, and Land Unit
1755 E-6 is planned for ~~and developed with~~ office use at an average intensity of approximately 0.35
1756 FAR.
1757
1758 2) ~~The remainder of Land Unit G-4~~This area is planned for office and retail use at an average
1759 intensity ~~up to~~ of 0.50-40 FAR.
1760

1761 *Option for Land Unit G-4: Optional Level of Development – Special Considerations*

1762 ~~As an option, future development and redevelopment in Land Unit G-4 will be guided by~~
1763 ~~the vision and character described for the General Zone. This option recommends a mix of uses at~~
1764 ~~a maximum building height of five stories, guided by the development potential for the Center and~~
1765 ~~General Zones shown in Figure 11. A portion of this land unit is bounded by Dolley Madison~~
1766 ~~Boulevard, with residential uses located outside the CBC further north. Although Dolley Madison~~
1767 ~~Boulevard provides separation from the CBC, this particular area transitions from the General Zone~~
1768 ~~immediately to residential uses outside of the CBC, without an Edge Zone as a transition area.~~
1769 ~~Transitions between any new development and the existing uses outside of the CBC should be~~
1770 ~~carefully designed, particularly as related to height. Plan recommendations regarding open space,~~
1771 ~~character of development, urban design, transportation, implementation, and other guidance found~~
1772 ~~in this CBC Plan should be used in the evaluation of development proposals.~~
1773

1774 Edge Zone: Land Unit E-1
 1775
 1776



1777

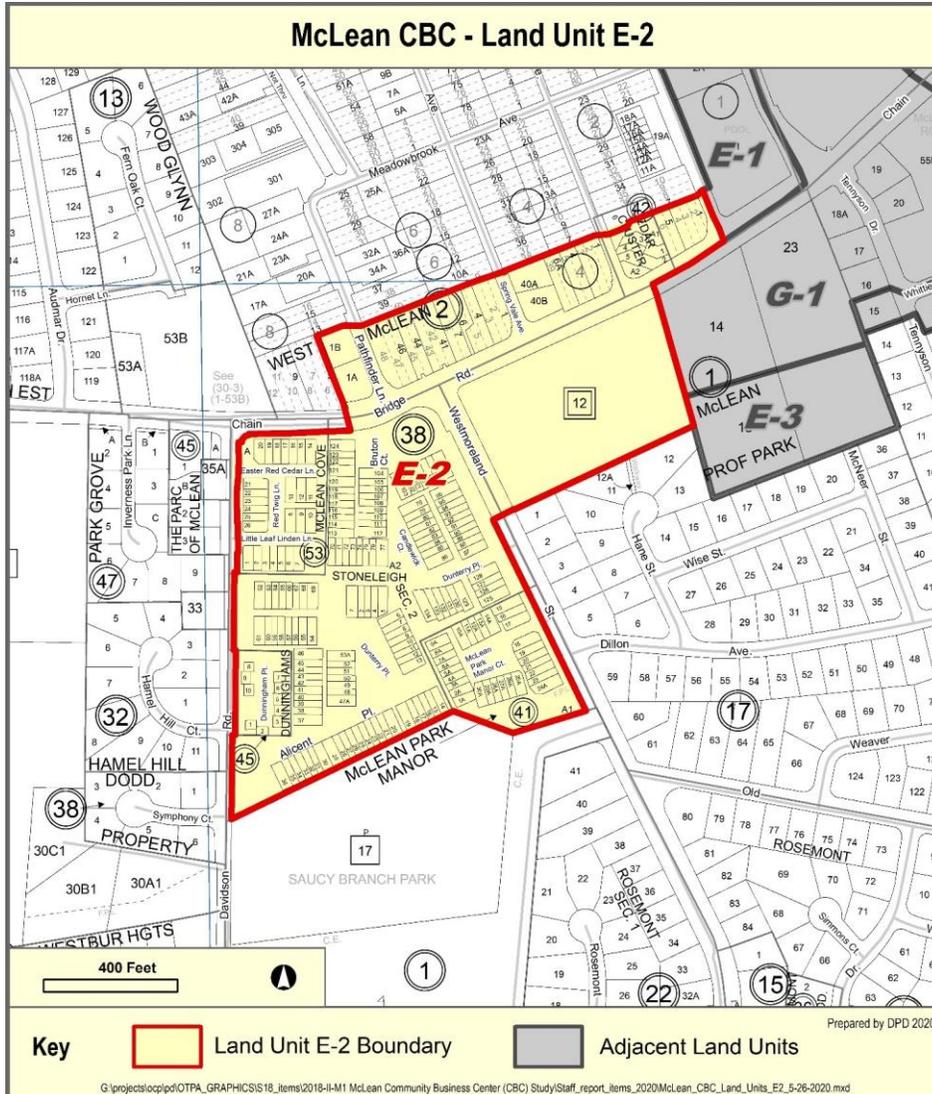
1778 Land Unit E-1 is developed with single-family detached and attached residential uses, and
1779 private recreation use. The block bounded by Chain Bridge Road, Ingleside Avenue, Buena Vista
1780 Avenue, and Meadowbrook Avenues should remain in private recreation uses. If redevelopment
1781 occurs, infill of low intensity commercial or medium intensity mixed-use to include office and
1782 residential with no retail would be appropriate, provided building heights do not exceed three
1783 stories and intensity does not exceed 0.50 FAR; all office access is limited to Ingleside Avenue;
1784 and extensive landscaped buffering ~~to residential uses~~ is provided to residential uses; ~~an on-street~~
1785 ~~bicycle facility is provided along Ingleside Avenue~~; and a primary pedestrian connection to West
1786 McLean is enhanced along Meadowbrook Avenue. If developed with office uses, an urban park
1787 should be provided. ~~If office use is proposed, vehicular access is recommended to be limited to~~
1788 ~~Ingleside Avenue. If mixed-use development is proposed, retail use is not recommended as a~~
1789 ~~component of the mix of uses.~~

1791 The remainder of Land Unit E-1 is planned for and developed with medium density
1792 townhouses. In order to contain future commercial growth within the CBC, to provide additional
1793 housing within the CBC, and to establish a residential buffer for West McLean, future development
1794 should be townhouse residential at a base range of 5-8 du/ac. A mid-range density of 8-12 du/ac
1795 could be achieved provided ~~there is that~~ consolidation of ~~minimum areas of at least one acre is~~
1796 ~~provided and that consolidation allows for~~; pedestrian amenities; include five foot wide sidewalks
1797 on all streets with mid-block pedestrian connections; landscaped buffers between single-family
1798 residential ~~are included~~; innovative design and architectural compatibility with single-family
1799 detached areas is achieved; and buildings are limited to 35 feet in height. Parking should be below
1800 ground, in structures, or screened. An overlay density range of 12-16 du/ac can be achieved along
1801 Ingleside Avenue or Old Dominion Drive only, with complete block consolidation, provision of
1802 affordable dwelling units, and all design elements listed above. Development along Pine Crest
1803 Avenue and Buena Vista Avenue is encouraged to be residential detached single-family housing.

Commented [SDB14]: Should this be changed to three stories?

1804
1805

1806 Edge Zone: Land Unit E-2
 1807
 1808



1809

1810 Land unit E-2 is developed with single-family detached and attached residential uses,
1811 institutional use, ~~and low-scale townhouse office uses at an intensity up to .35 FAR.~~ The western
1812 edge of the land unit on the west side of Westmoreland Street is planned for ~~and developed with~~
1813 residential use at 8-12 du/ac.

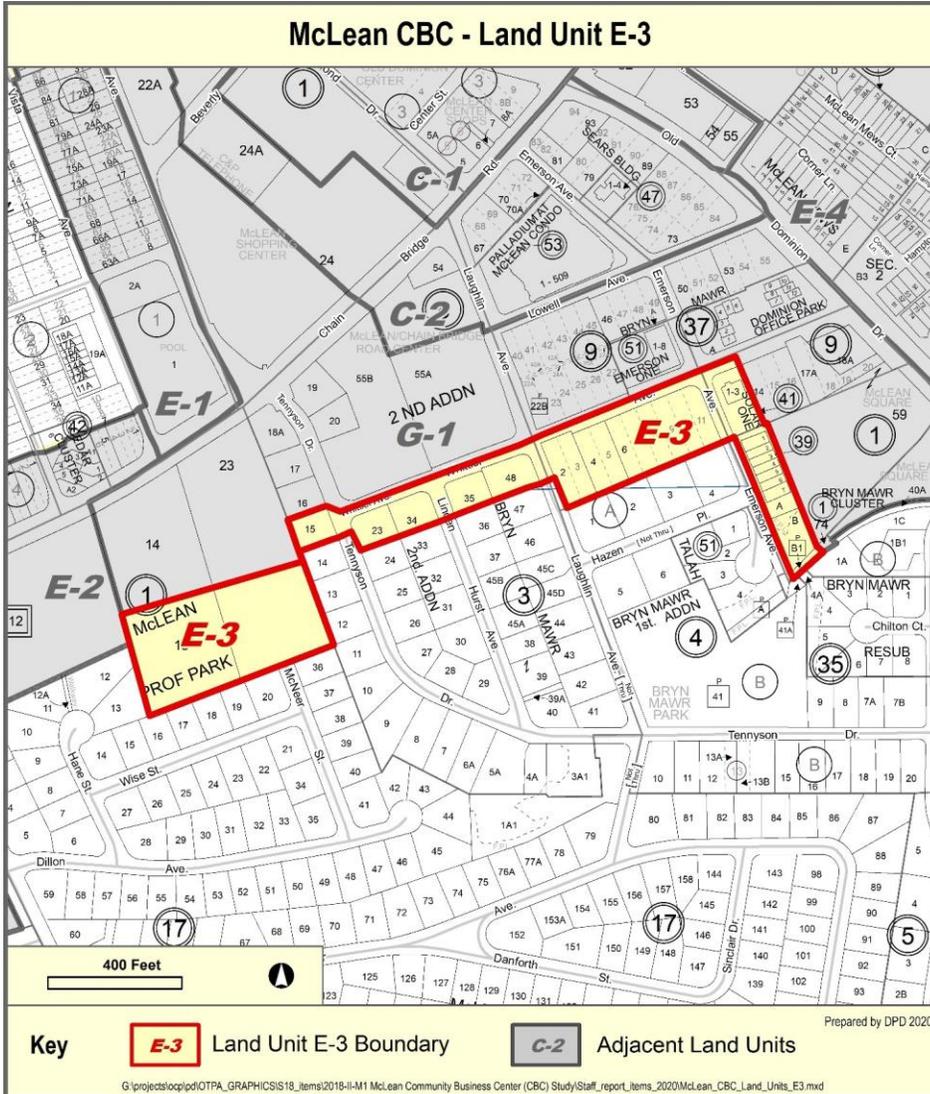
1814
1815 The area bounded by Chain Bridge Road, ~~Pathfinder Lane~~ the western boundary of the CBC,
1816 Buena Vista Avenue, and the rear property lines of land fronting Chain Bridge Road should develop
1817 in medium density residential townhouses to serve as a buffer between CBC commercial uses and
1818 single-family residential uses in West McLean. A base density range of 5-8 du/ac would be
1819 appropriate provided that: complete blocks are consolidated; an innovative layout of townhouses is
1820 provided with no direct vehicular access to Chain Bridge Road; streetscape improvements are made
1821 ~~according to the McLean Design Standards,~~ including street trees and a landscaped median;
1822 building heights are limited to a maximum of two stories, up to 35 feet; appropriate buffering to
1823 adjacent single-family residential is provided; five-foot wide sidewalks wrapping to side streets are
1824 installed; and, a bus shelter on Chain Bridge Road is provided.

1825
1826 The existing place of worship located at the southeast intersection of Chain Bridge Road
1827 and Westmoreland Street is recommended to be retained; if redeveloped, the site is recommended
1828 for townhouse-style residential use at a density of 5-8 du/ac and should be harmonious in scale,
1829 character, and site layout with the Stoneleigh and McLean Park Manor townhouses to the west and
1830 single-family detached houses to the south.

1831
1832

Commented [SDB15]: Should the recommendation for 35' be deleted? Is the intent two stories townhomes?

1833 Edge Zone: Land Unit E-3
 1834



1835
 1836
 1837 Land Unit E-3 is primarily planned for townhouse-style commercial use. The parcels
 1838 located between Tennyson Drive, Land Unit G-1 and Laughlin Avenue are planned for office use at

1839 an intensity up to 0.35 FAR. The parcels located between Laughlin Avenue and Emerson Avenue
1840 are planned for commercial row houses at an intensity up to 0.50 FAR. The area located between
1841 Tennyson Drive and Emerson Avenue would be appropriate for residential development but is
1842 zoned for commercial use. Context-sensitive commercial development such as low-density
1843 townhouse office would be appropriate in this area since commercial development already exists
1844 on adjacent blocks, provided that new development is carefully screened from surrounding
1845 residential properties.

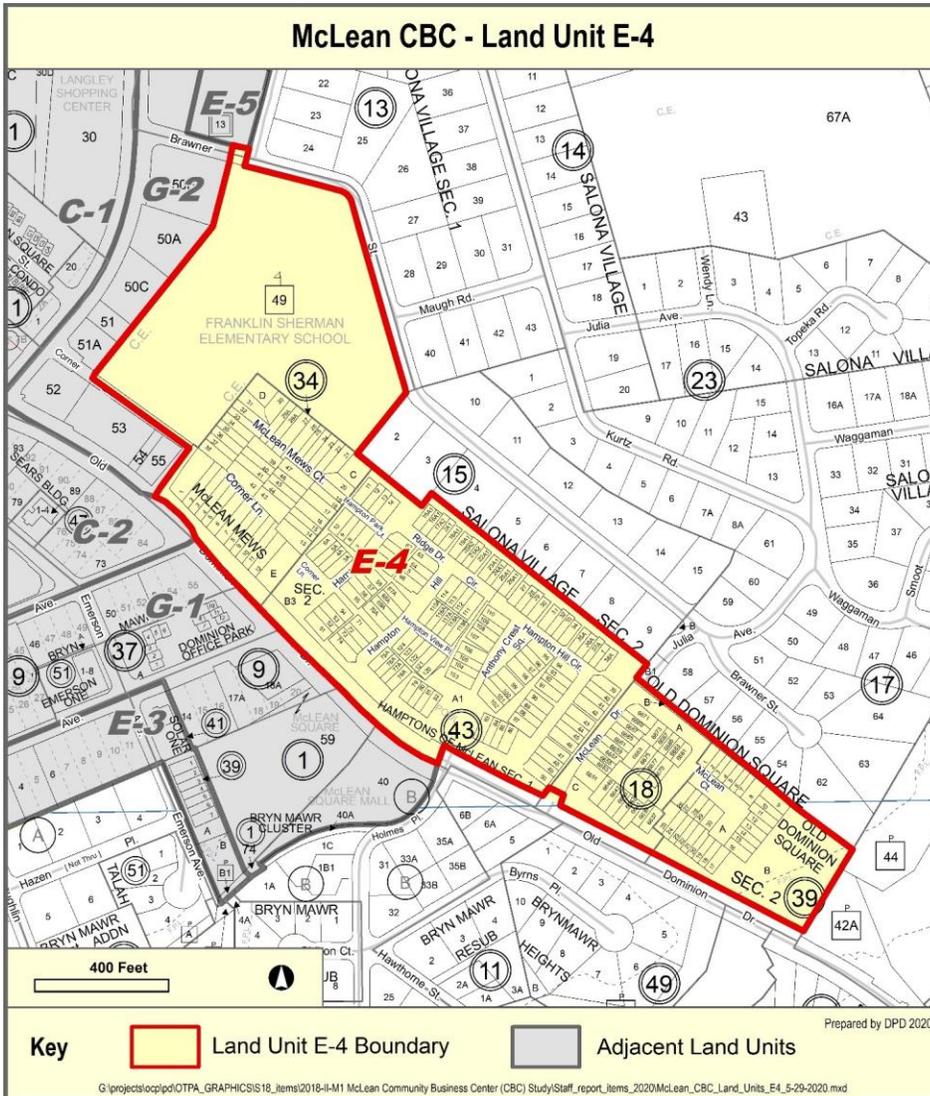
1846
1847 The parcels located on the east side of Emerson Avenue are planned for community-serving
1848 retail and office use at an intensity up to 0.50 FAR. The parcels located ~~on~~ at the southeast side
1849 corner of Whittier Avenue and Emerson Avenue (Tax Map 30-2 ((41)) 1, 2, and 3) are planned for
1850 community serving retail and office use at an intensity up to 0.50 FAR. The parcels located on the
1851 east side of Emerson Avenue (Tax Map 30-2 ((39)) 1, 2, 3, 4, 5, 6, 7, and 8) are planned for
1852 townhouse style residential use at 8-12 du/ac.

1853
1854 ~~The area bounded by Tennyson Drive, Land Unit G-1, Laughlin Avenue, Whittier Avenue~~
1855 ~~and the southern border of the CBC would be appropriate for residential development but is zoned~~
1856 ~~for commercial use. Sensitive Context sensitive commercial development such as low density~~
1857 ~~townhouse office would be appropriate in this two block area since commercial development~~
1858 ~~already exists on adjacent blocks, provided that new development is carefully screened from~~
1859 ~~surrounding residential properties.~~

1860
1861 The portion of the McLean Professional Park in Land Unit E-3 is planned for ~~and developed~~
1862 ~~with~~ low/medium intensity commercial office at an intensity of approximately 0.55 FAR.
1863 Maximum heights should be no more than 40 feet.

1864
1865

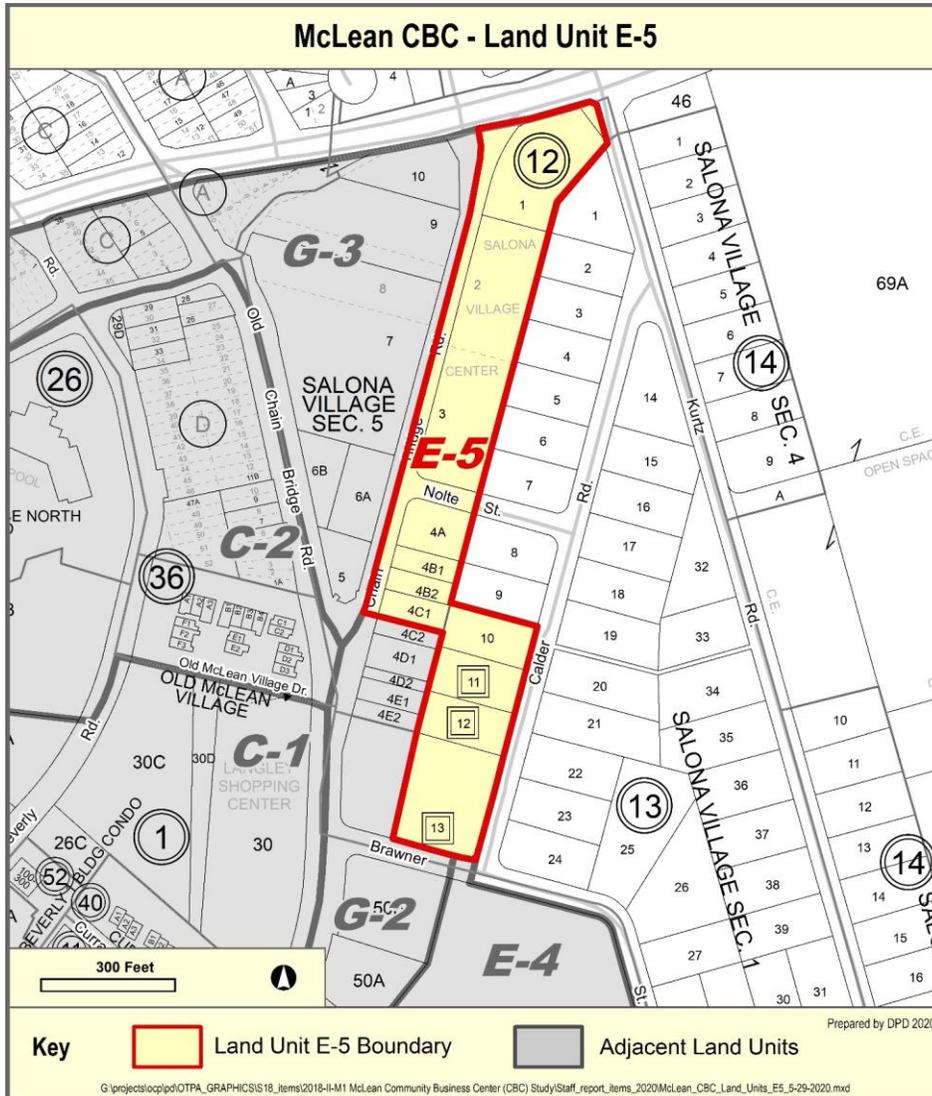
1866 Edge Zone: Land Unit E-4
 1867



1868
 1869 Land Unit E-4 is planned for and developed with single-family attached residential uses at 8-12
 1870 du/ac and institutional use at a base intensity up to 0.35 FAR (maximum intensity of 0.50 FAR).
 1871

1872 The Franklin Sherman Elementary School is recommended to be retained. If the school
1873 ~~redeveloped~~ redevelops, it may be appropriate for ~~an alternate mixed-use Civic Place~~ a mix of uses
1874 at an intensity up to 0.50 FAR with a civic green-type park. ~~at an intensity up to 0.50 FAR.~~
1875
1876

1877 Edge Zone: Land Unit E-5
 1878



1879
 1880 Land Unit E-5 primarily consists of the Salona Village Shopping Center and Tax Map
 1881 Parcels 30-2 ((12) 4A, 4B1, 4B2, 4C1, and 4C2. This portion This portion of Land Unit E-5 is
 1882

1883 planned for predominately community – serving retail use with limited office at an average intensity
1884 ~~up to~~ of 0.35 FAR. Land Unit E-5 is also developed with a place of worship (McLean Baptist
1885 Church). If the church site redevelops, there is an option for development ~~for at~~ an intensity up to
1886 0.35 FAR, with single-family detached residential ~~facing Calder Road~~ or a 50-foot landscaped
1887 buffer with a 7-foot high brick wall facing Calder Road. Vehicular access is not recommended on
1888 Brawner Street or Calder Road. This option includes the portion of the church property located in
1889 Land Unit G-2.

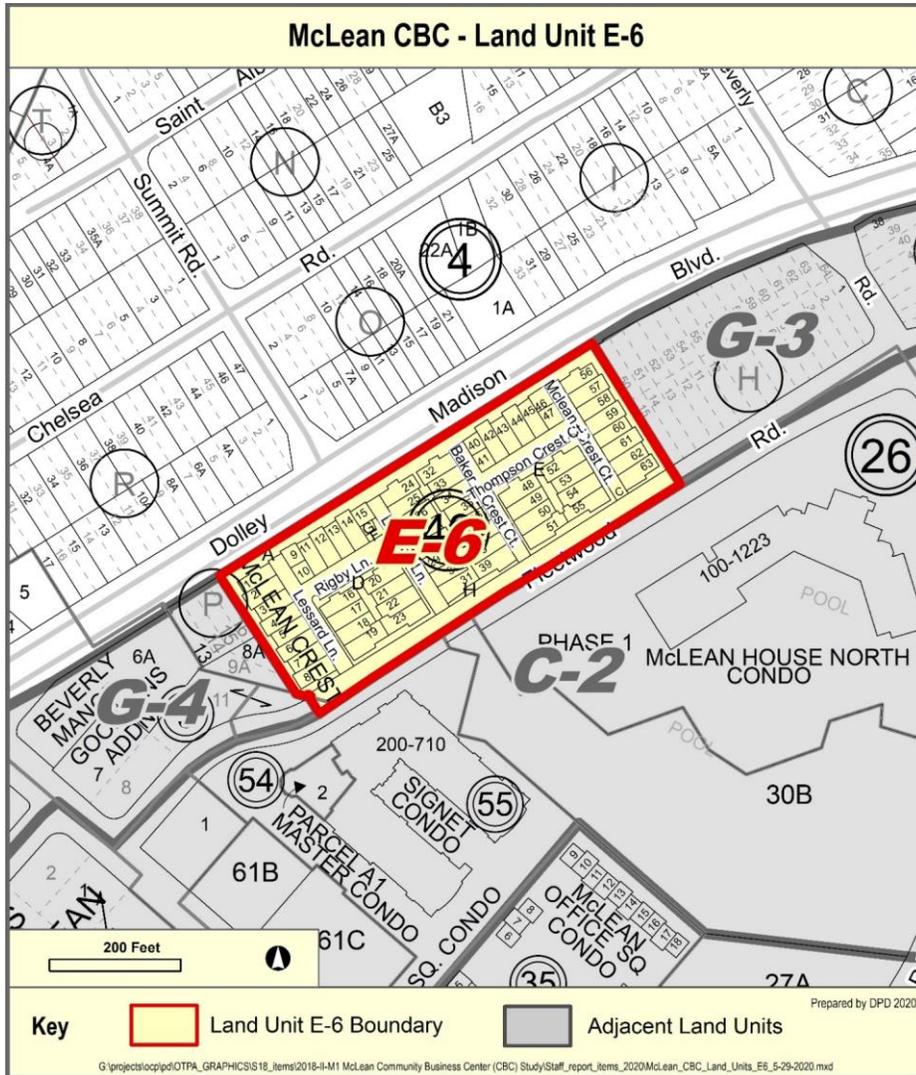
1890 **Proposed Alternative**

1892 Edge Zone: Land Unit E-5

1894
1895 The majority of Land Unit E-5 is planned for predominately community – serving retail use (Salona
1896 Village Shopping Center) with limited office at an average intensity up to 0.35-40 FAR. Land Unit
1897 E-5 is also developed with a place of worship (McLean Baptist Church). If the church site
1898 redevelops, there is an option for the development of community serving retail use with limited
1899 office at an intensity up to 0.35 FAR with single-family detached residential uses ~~facing Calder~~
1900 ~~Road~~ or a 50-foot landscaped buffer and a 7-foot high brick wall facing Calder Road. Vehicular
1901 access is not recommended to retail or office uses from Brawner Street or Calder Road.
1902
1903

Commented [SDB16]: Clarification to existing recommendation.

1904 Edge Zone: Land Unit E-6
 1905



1906
 1907 Land Unit E-6 is planned and developed with for residential townhouses use at an intensity of 12-
 1908 16 du/ac.