



County of Fairfax, Virginia



Richmond Highway Bus Rapid Transit

Executive Committee Meeting #8
May 29, 2020



The Richmond Highway BRT project is funded in part by the Northern Virginia Transportation Authority.





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Agenda

- **Project Status**
 - FTA Status
 - Environmental Study (NEPA)
 - 30% Design
 - Hybla Valley Station Configuration
 - Stormwater Management Approach
- **Public Engagement Outlook**
- **Project Schedule Refinement**
- **BRT Station Architecture**
- **Discussion**



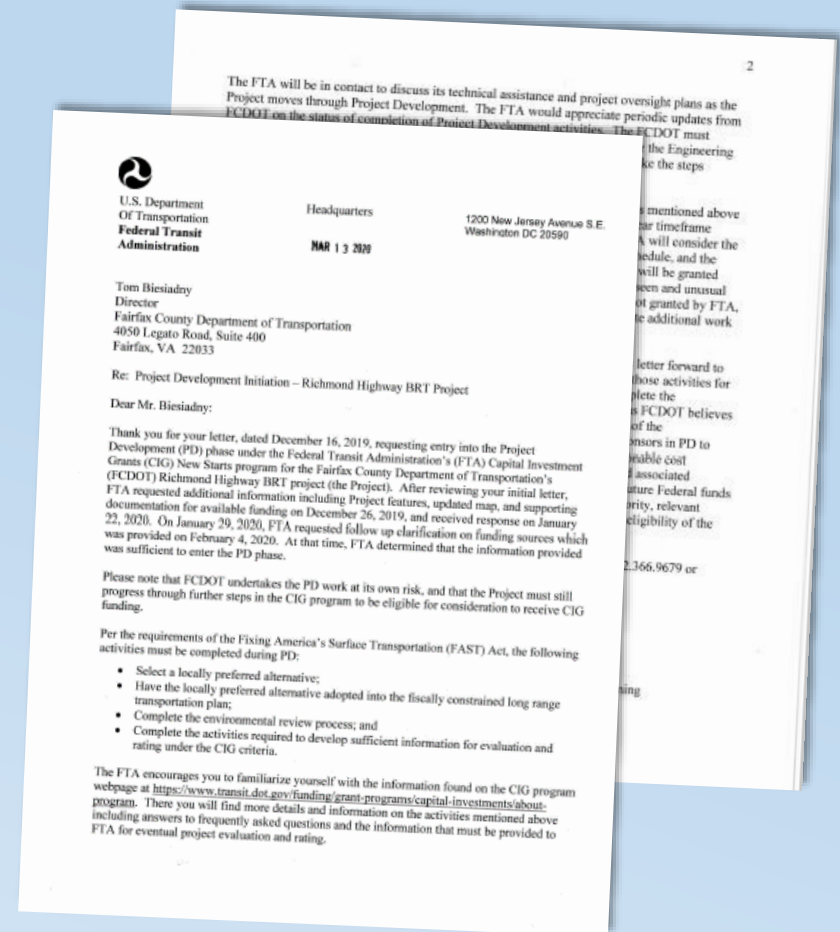


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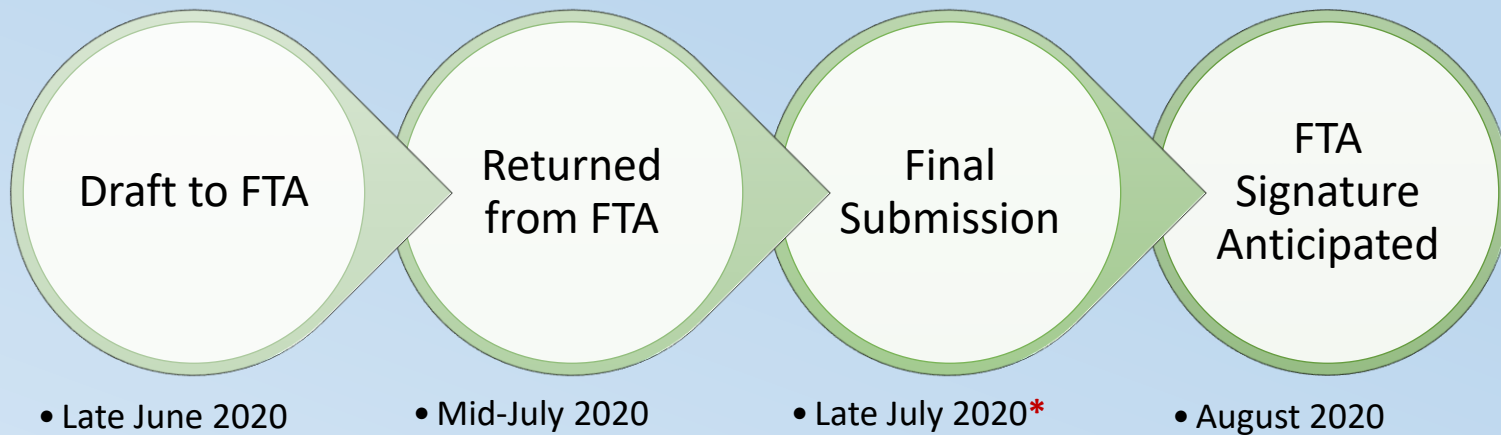
FTA Funding

- BRT project accepted in FTA “Project Development (PD)” phase
 - First step of application process
 - Accepted March 13, 2020
 - Program requirements must be completed within two years
 - Includes NEPA, preliminary engineering, management plans
- BRT project anticipated to advance to FTA “Engineering” phase in fall 2021





NEPA Categorical Exclusion



***Timing contingent on the Section 106 Process**

Section 106 Progress since February:

- Submitted second Addendum to the Historic Architectural Identifications Survey, Phase I Archaeology Survey, and Section 106 Consultation Summary to the Dept. of Historic Resources and Consulting Parties
- Held second Consulting Parties meeting on April 16, 2020
- Provided support for FTA – DHR discussions regarding the eligibility status of Gum Springs

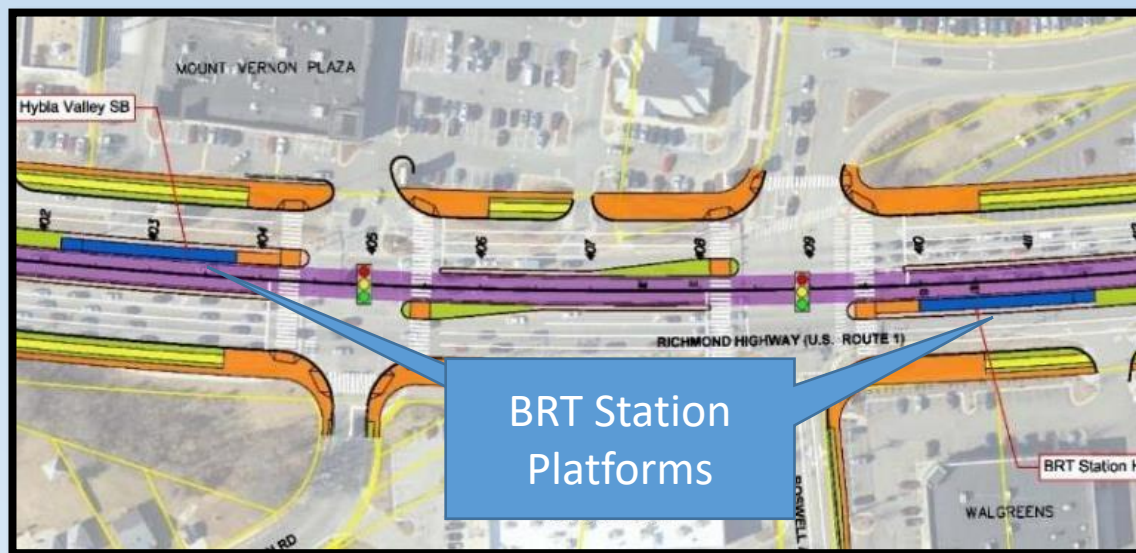


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Design Progress

- Anticipate completion of 30% design plans in July 2020
- Selection of Hybla Valley Station Option B announced to public; Fordson Road will remain open and operate as it does today





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Stormwater Management Approach

- Following County standards
- Includes
 - 63 dry swales and bioretention areas
 - 11 wet ponds
 - 3 underground storage tanks
- Facility locations shared during September 2019 meeting
- BRT Team working with VDOT on SWM design, in coordination with other County agencies (DPWES, DPD)

Existing



Potential





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Stormwater Management Approach

- **Design Elements Include:**
 - Naturalistic shapes
 - Enhanced tree canopy
 - Ornamental plantings
 - Use of natural elements for soil stabilization



- **Passive recreational features such as paths around facilities, seating, etc**



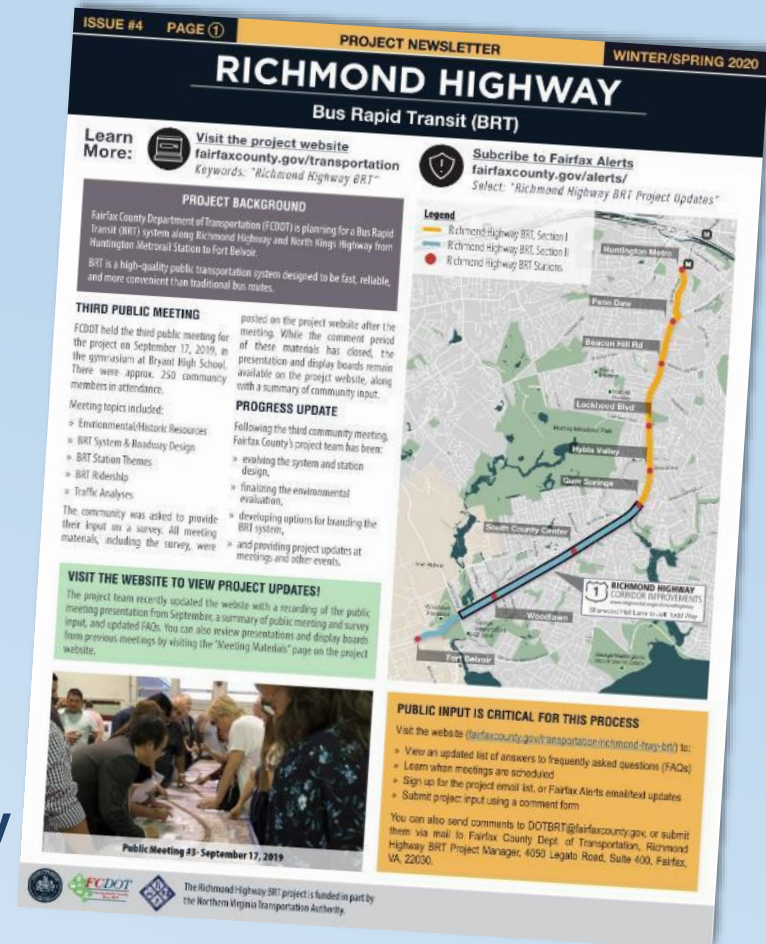


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Public Engagement

- E-newsletter sent to project mailing list April 1
- Deferred Richmond Highway Community Discussion due to pandemic; topics included BRT, VDOT widening project & DPD urban design guidelines
- Team working on other opportunities for virtual meeting with a live or recorded presentation, and materials/survey on the website





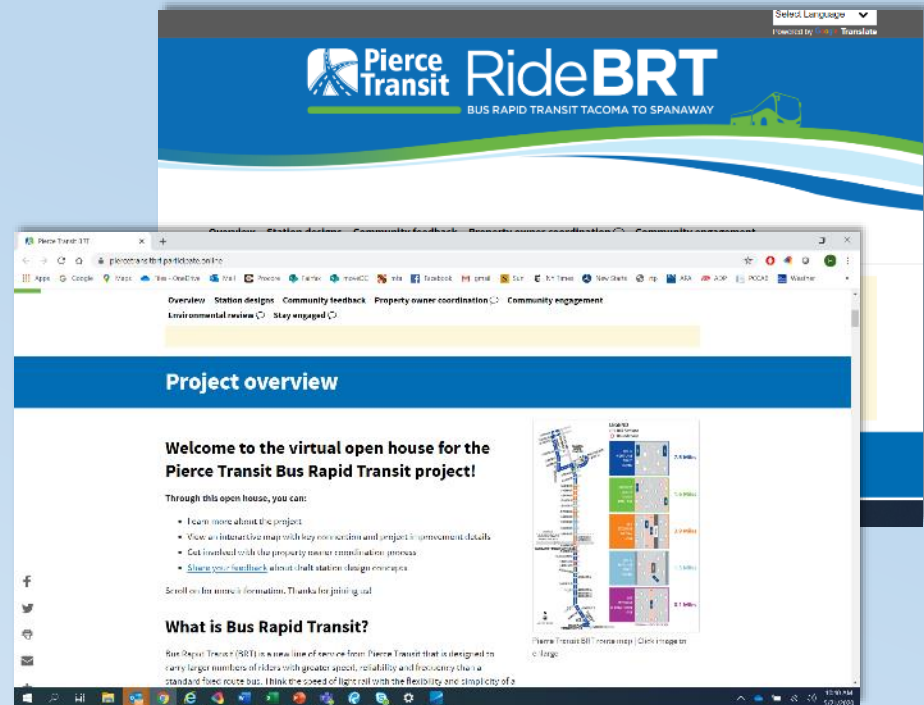
Options for Engagement Moving Forward

- **Key topics we need to discuss with the community**
 - Confirmation of the revised configuration for the Fordson/Boswell area
 - Shared with the community in an email newsletter
 - Station architecture preferences
 - NEPA progress/completion
 - Project schedule
- **In-person meetings are our preference; virtual meetings are a fallback**
- **Timing**
 - If in-person, would not plan to hold before September
 - If virtual, could hold as soon as July
- **Working with Neighborhood and Community Services on timing of public engagement events**



Virtual Meetings

- Goal is to provide the same experience of seeing a presentation and “walking through” exhibits
- Live webinar-style presentation with Q&A, or recorded presentation with subsequent Q&A session
- Website that walks attendees through a “meeting” – including a short video, project materials, survey questions



Example from Seattle region



Considerations for Outreach

- **Challenge**

- We could likely reach many people using virtual meetings, but would need to include a thoughtful and focused effort to share information with people who are not already tracking this project

- **Potential Strategies**

- Project email list (870 people)
- Radio outreach, TV (Channel 16)
- Social media ads
- Flyers distributed in partnership with NCS and corridor community services
- Potential postcard mailing
- Media/press release



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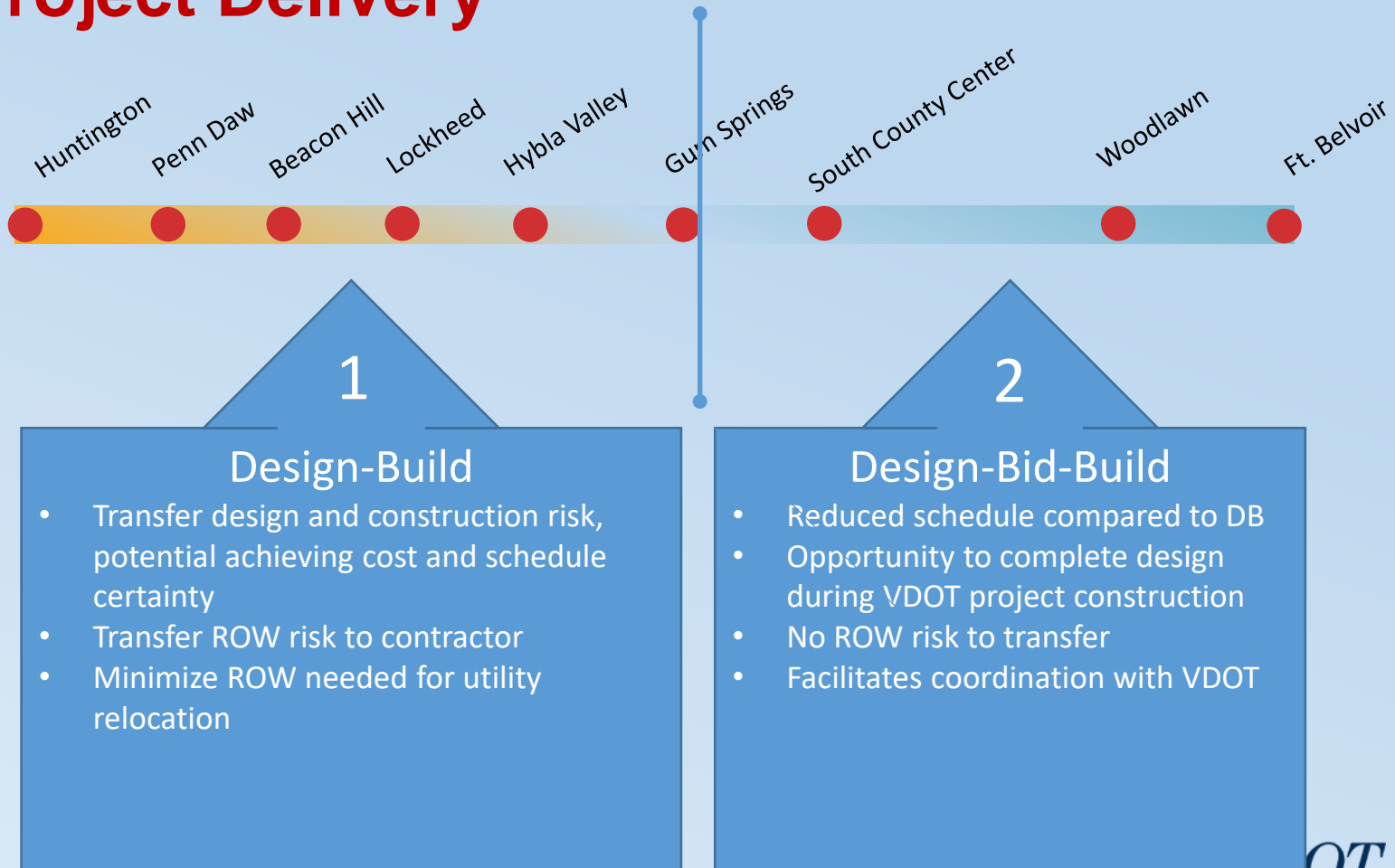
Schedule Refinement **Re-Baseline**

- May 2020 is first major re-baseline since DRPT study(2013-2015-
- Reflects decisions about project delivery, right-of-way needs & relationship to VDOT project
- Activity durations & dependencies further developed based on more detailed project information available today.
- Going forward, schedule will be regularly updated to reflect
 - Coordination with adjacent projects, e.g., VDOT widening, Huntington redevelopment
 - External events, e.g., FTA funding
 - Scope modifications, if any





Schedule Refinement **Project Delivery**

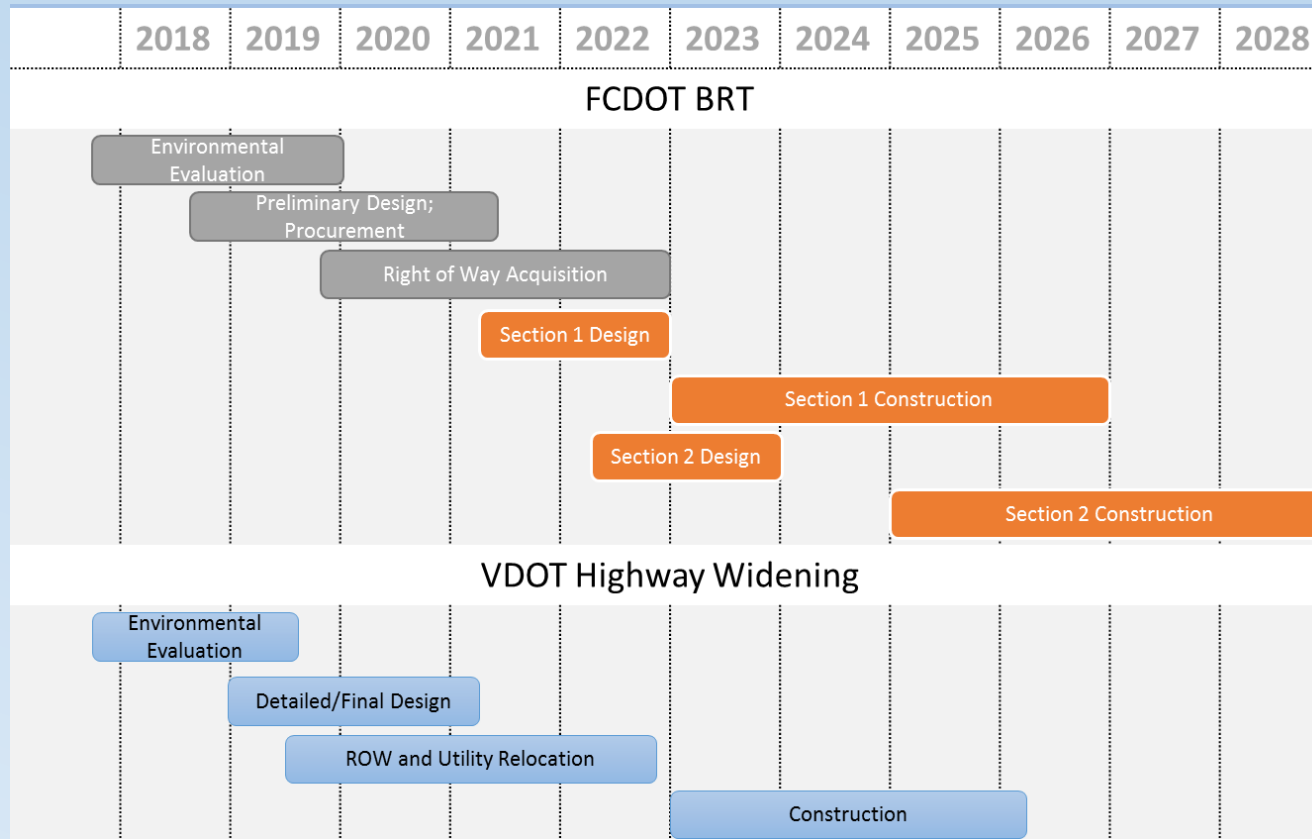




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Schedule Refinement **Legacy Schedule (High Level)**





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Schedule Refinement

Major Factors Contributing to Change

- **NEPA (12+ months)**
 - Unanticipated time for federally mandated consultation process; FTA shutdown delayed document review
- **Utilities (12+ months)**
 - Additional time allowed for utility relocations (Dominion, Cox, Verizon, AT&T, Washington Gas, Level 3, Sprint, Fairfax Water)
 - Significant underground telecommunications infrastructure between Boswell Ave and N. Kings Highway; requires Verizon design and approval
 - Large gravity sewer mains require complex design to ensure adequate flow

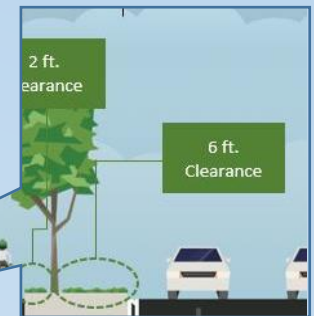




Schedule Refinement

Major Factors Contributing to Change

- **Design Changes & Enhancements (12 months)**
 - Development of minimization options to reduce impacts to sensitive resources, e.g., homes along Collard St, Groveton Baptist Church, historic fire station
 - Huntington bus loop redesign
 - Intersection reconfigurations in response to public input
 - Accommodation of Embark landscaping and pedestrian features around station
 - Corridor-wide lighting upgrade
 - Enhanced stormwater management approach

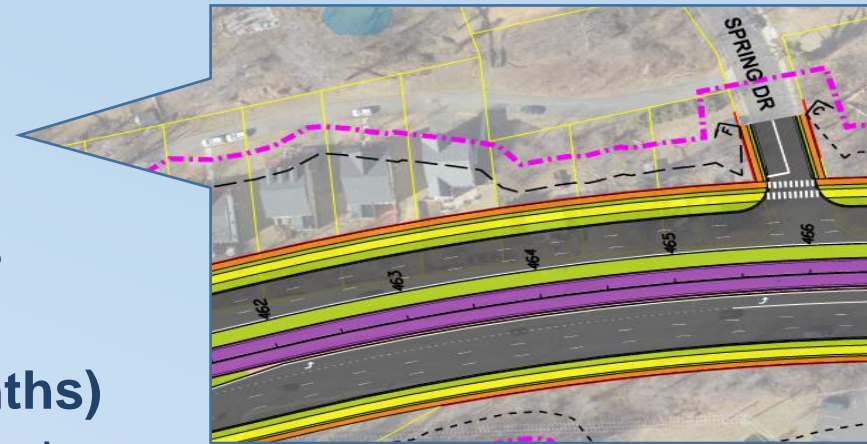




Schedule Refinement

Major Factors Contributing to Change

- **Right of Way (15 months)**
 - Design development resulted in additional full and partial acquisitions
 - To reduce risk and minimize acquisitions, partial acquisitions shifted to contractor
- **Construction Sequencing (30 months)**
 - Activity durations and dependencies have since been developed further which resulted in a more realistic total schedule durations, milestones, and end date

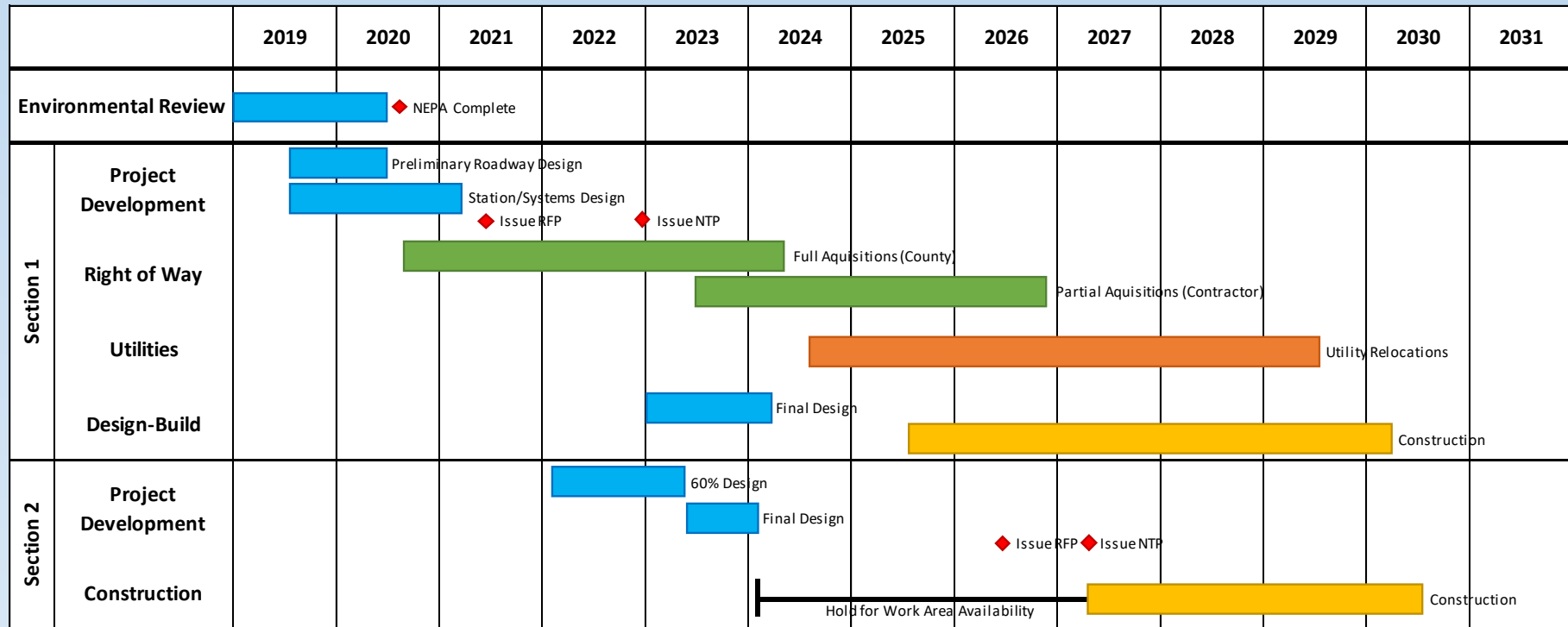




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Schedule Refinement **Detailed BRT Project Schedule (NEW)**



Note: Reflects VDOT's public schedule; BRT Team anticipates adjustment to reflect VDOT latest updated schedule





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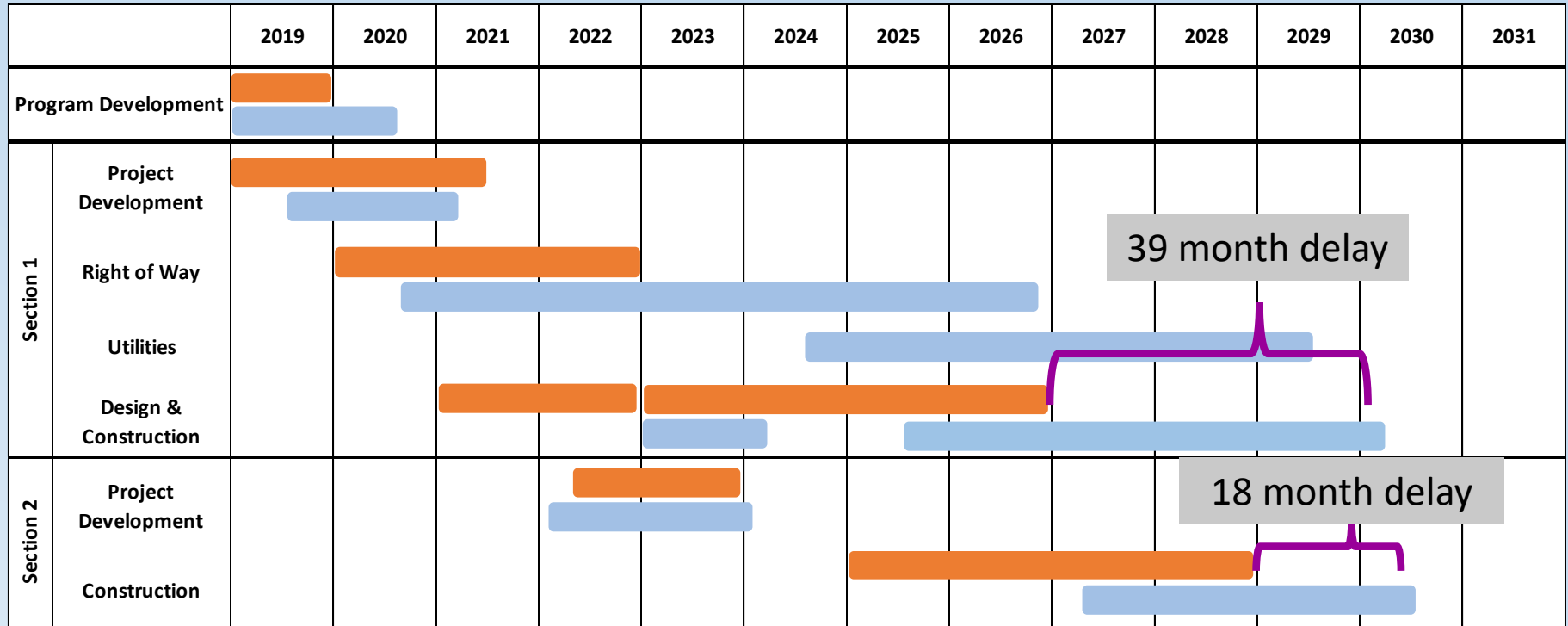


Schedule Refinement

Comparison of Legacy & New

Legacy

New



Note: Utilities combined with Construction in Legacy schedule





Schedule Refinement

Opportunities to Reduce Schedule

- **Start partial property acquisitions prior to completion of final plans**
 - ! Risk is right of way needs could change, requiring a second acquisition from the same owner
- **Seek input from contractors through industry review of schedule**
- **Advertise Section 2 contract prior to completion of VDOT work**
 - ! Risk is if VDOT is delayed we would be forced to delay notice to proceed or re-advertise
- **Include contract provisions to incentivize compressed schedule**
 - ! Risk is higher project costs to cover incentives



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Schedule Refinement

On-Going Schedule Risks

- Right-of-way acquisition and relocation
- Utility relocation
- Schedules for adjacent projects (VDOT widening, Huntington Station redevelopment)
- Hazardous materials mitigation
- Agency coordination and permits (VDOT, Ft. Belvoir, US Army Corps of Engineers, VA Department of Historic Resources)
- Accommodation of private development



BRT Station Architecture

- **Initial design development**
 - Influenced by
 - Embark
 - Richmond Highway Urban Design Guidelines
 - Local historical, cultural, institutional & natural resources
 - Refined by Stations Working Group comprised of county agency staff(DPD, TSD, CPTED)
- **Executive Committee (2/21/20)**
 - Six designs shared
 - Direction to advance Concept B
 - Direction to explore revised concept incorporating aspects of Option C and Option E
 - Direction to explore opportunities for community-specific identity, including art, within standard design



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Approved Concept B Design



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Approved Concept B Design



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Concept E Design Study



Original Concept Design



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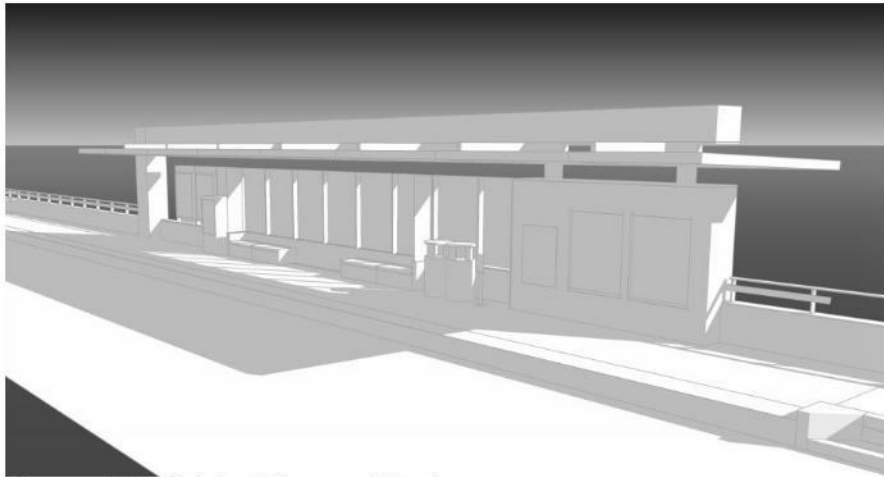




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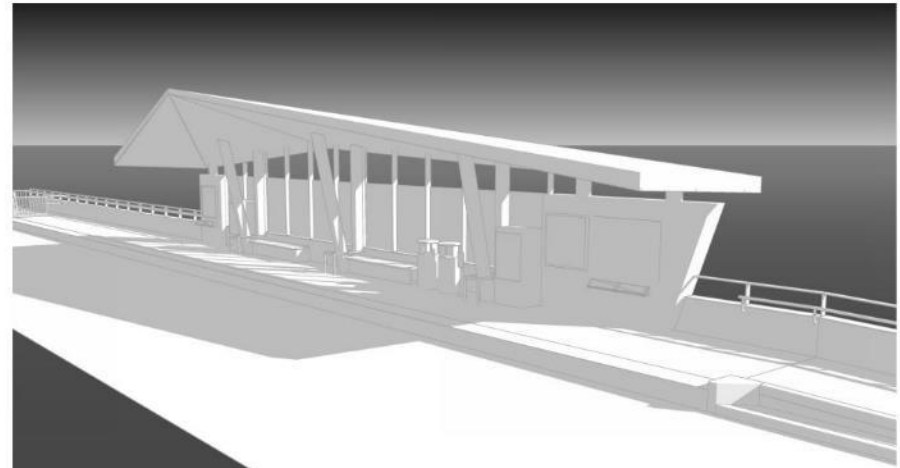
Original Concepts C & E Comparison



Concept C - Original Concept Design

Features:

- Clean lines
- Modern Feel
- Interior Layout



Concept E - Original Concept Design

Features:

- Distinctive Roof Line
- Connection to Tourism Themes
- Asymmetry



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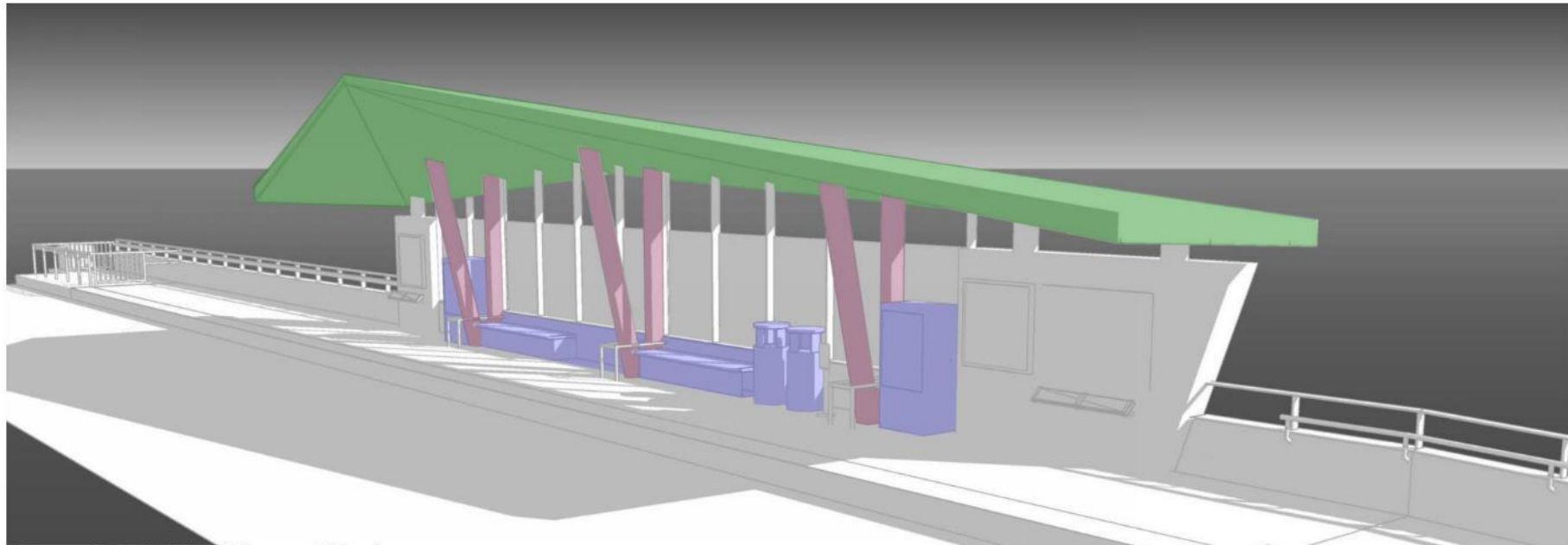




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Concept E Design Study Requests



Concept E - Original Concept Design

Design Study Requests:

- Reduce Roof Pitch and Maximize Rain Coverage
- Explore Different Column Approaches to Address Projection Clearance without Railing
- Simplify/Declutter Interior Layout and Integrate Fare Vending Machine into End Piece
- Introduce more Modern Color/Material Palette



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Concept E Design Study Progression - Study Approach 1



Concept E - Design Progression - Approach 1

Changes:

- Roof: Pitch Reduced, Increased Depth for Rain Coverage, Increased Height for Road Clearance
- No Change to Columns
- Interior Layout Simplified Similar to Concept C with Integrated Fare Vending Machine



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Concept E Design Study Progression - Study Approach 2



Concept E - Design Progression - Approach 2

Changes:

- All Changes from Approach 1
- “Y” Column Addresses Projection Clearance Requirements



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Concept E Design Study Progression - Study Approach 3



Concept E - Design Progression - Approach 3

Changes:

- All Changes from Approach 1
- Revised "V" Column and Base Addresses Projection Clearance Requirements
- Windscreen Structure Cantilevered from Wall without Roof Attachment



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Concept E Design Study Progression - Study Approach 4



Concept E - Design Progression - Approach 4

Changes:

- All Changes from Approach 3
- Revised Straight Column With Deeper Roof Structure Avoids Projection Clearance Issue
- Perpendicular Windbreaks Added to Seating Area



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Revised Rain Coverage Concepts C & E Comparison



Concept C - Rain Coverage Roof Concept Design



Concept E - Design Progression Study 4



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Recommended Concept E Renderings



Concept E - Approach 4 - Front Perspective from Roadway



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Recommended Concept E Renderings



Concept E - Approach 4 - Front View from Roadway



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Recommended Concept E Renderings



Concept E - Approach 4 - Approach to Canopy from Entry



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Recommended Concept E Renderings



Concept E - Approach 4 - Interior View from Near End



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Recommended Concept E Renderings



Concept E - Approach 4 - Interior View from Far End



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Recommended Concept E Renderings



Concept E - Approach 4 - Approach to Canopy from Far End



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for 25 Years and More





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Recommended Concept E Renderings



Concept E - Approach 4 - Rear View from Roadway/Sidewalk



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Recommended Concept E Renderings



Concept E - Approach 4 - Bird's-Eye View



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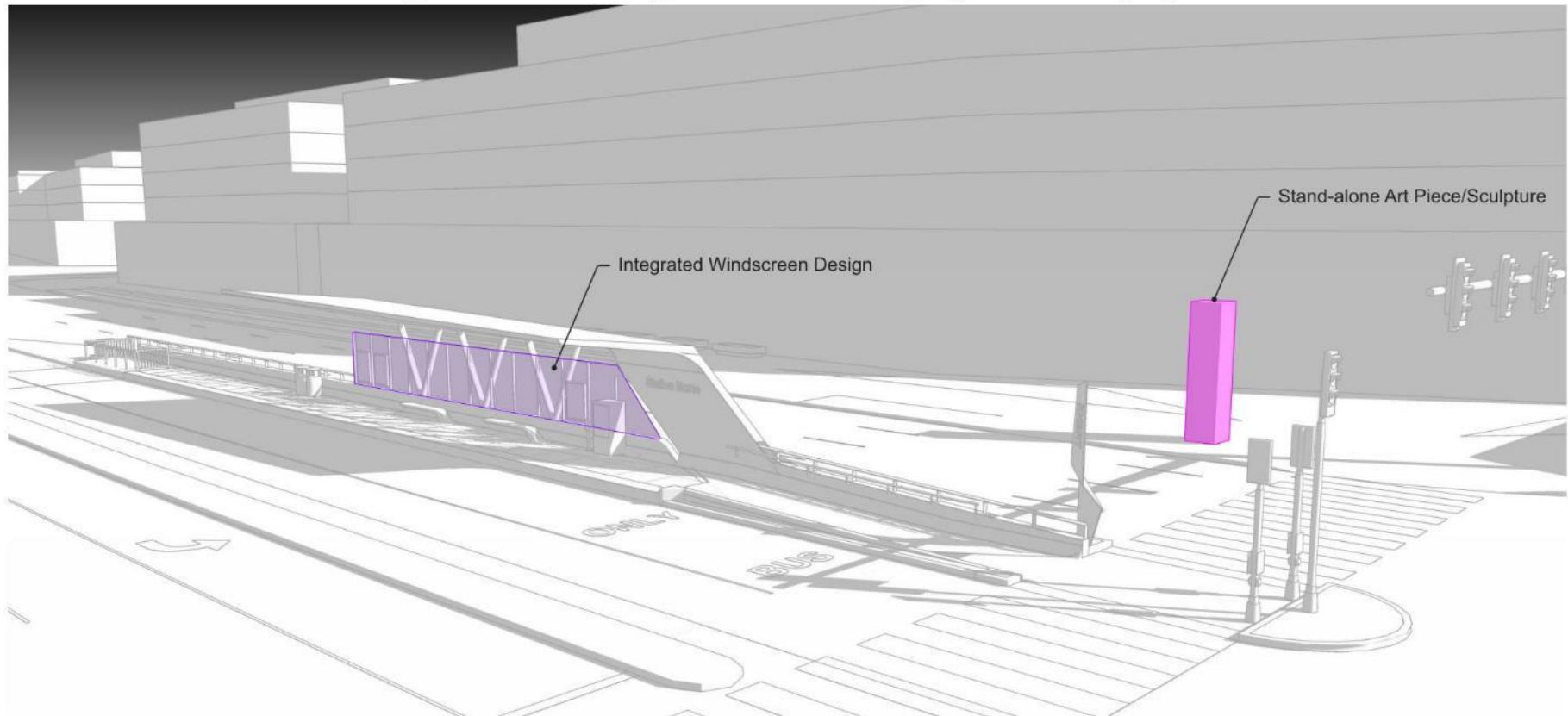




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Community Identity - Canopy B Opportunities



Concept B - Identity Approach 1 - Supervisor Preferred Approach with Strong Vertical Element and Windscreen Interlayer



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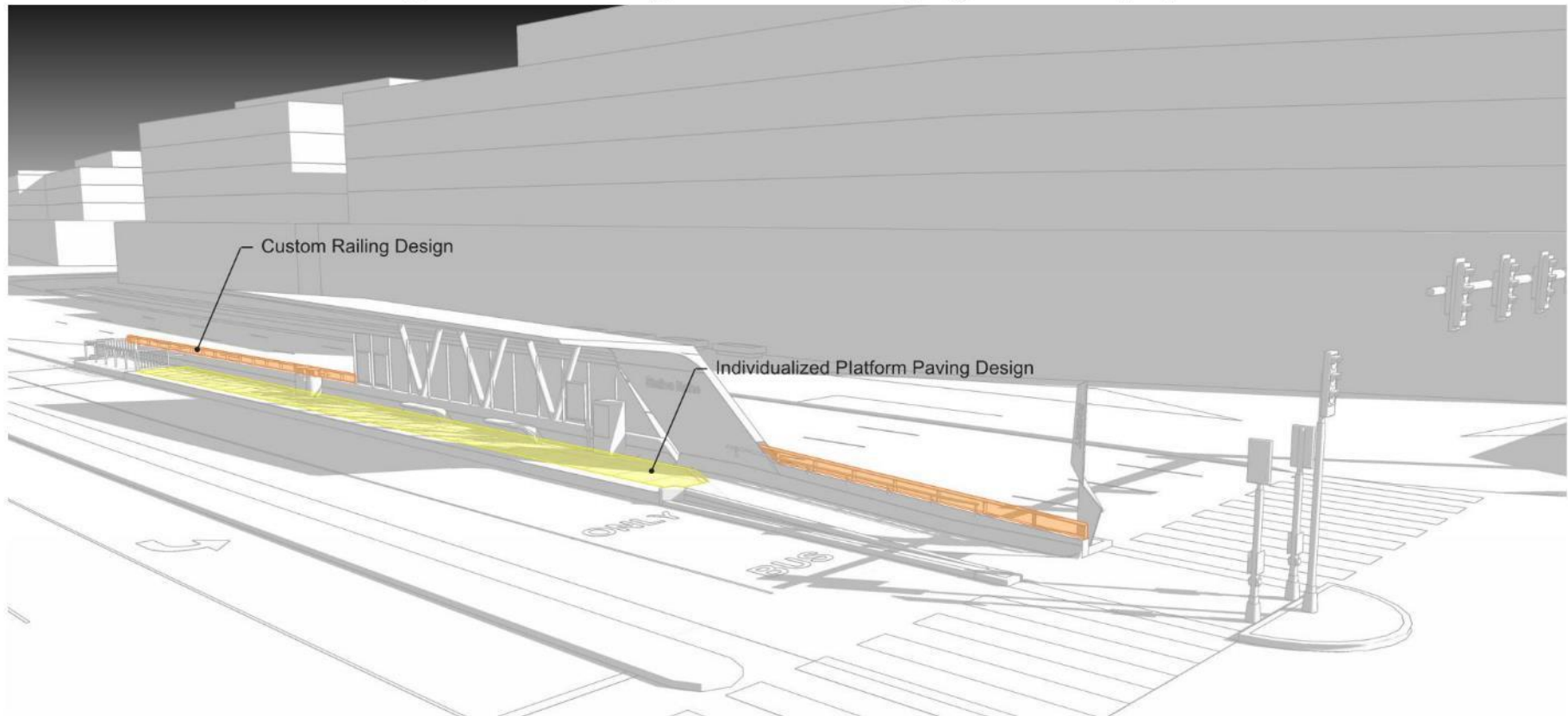




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Community Identity - Canopy B Opportunities



Concept B - Identity Approach 2 - Alternative Approach with Custom Railing and Platform Paving Designs



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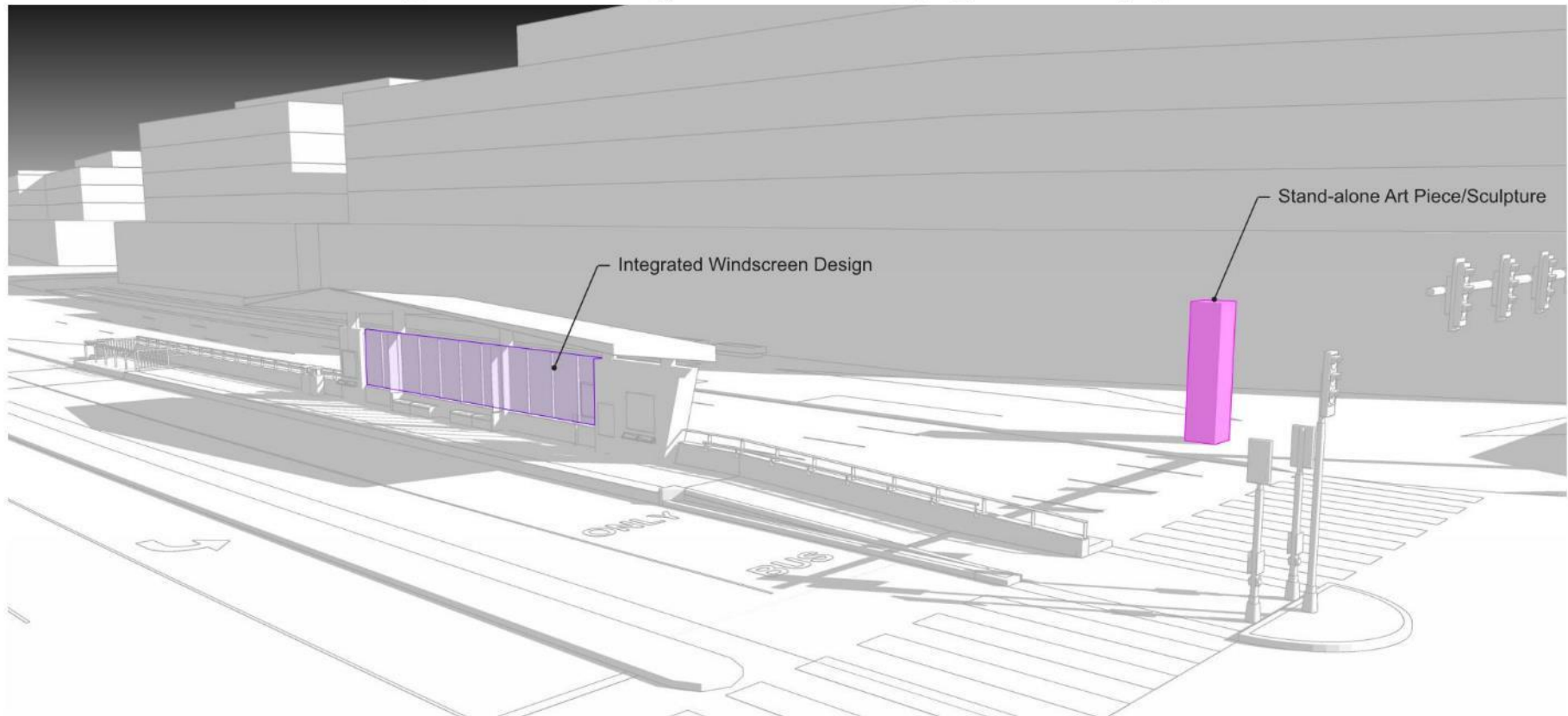




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Community Identity - Canopy E Opportunities



Concept E - Identity Approach 1 - Supervisor Preferred Approach with Strong Vertical Element and Windscreen Interlayer



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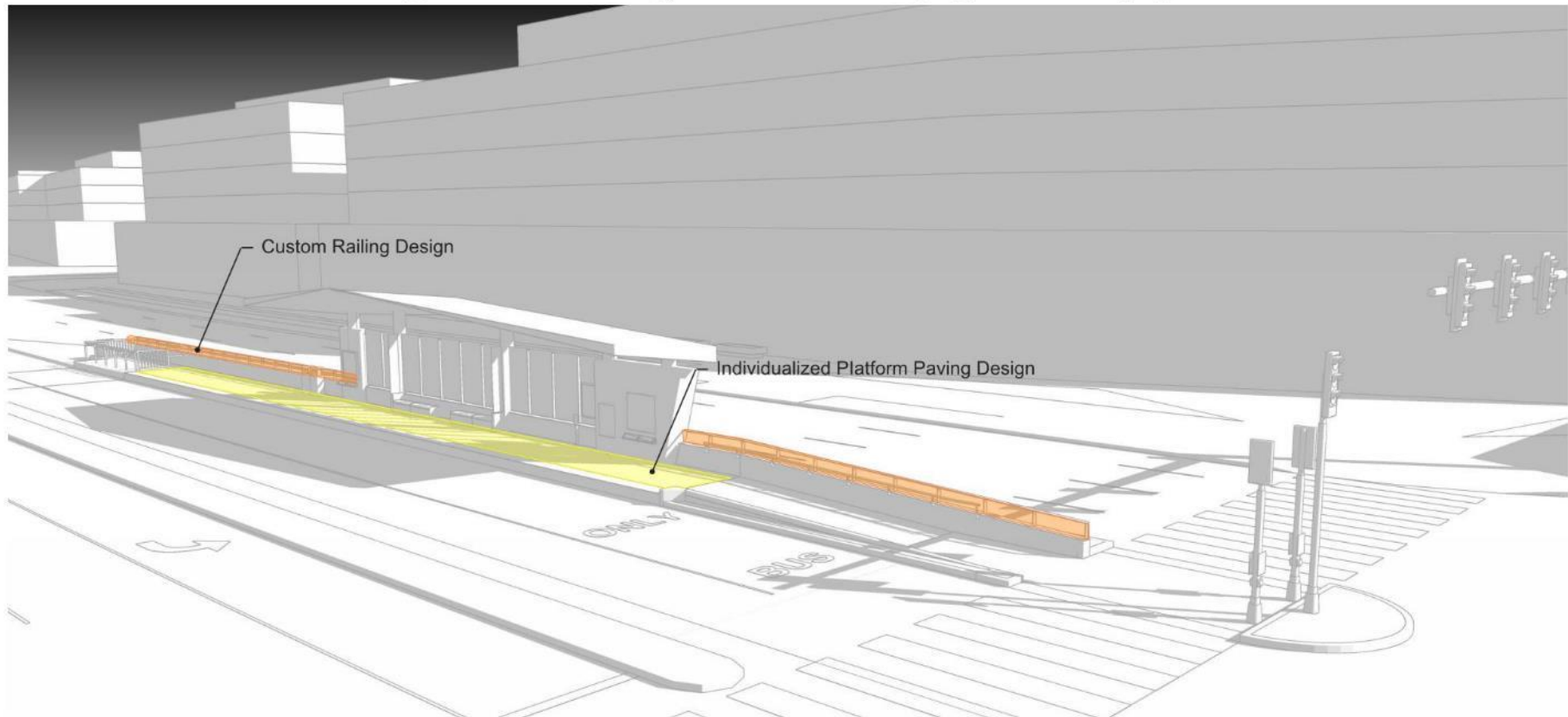




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Community Identity - Canopy E Opportunities



Concept E - Identity Approach 2 - Alternative Identity Approach with Custom Railing and Platform Paving Designs



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Next Steps for Station Architecture

Summer
2020

- Station design advances to 30%
- Input from Executive Committee on design progress, materials & colors
- Develop materials for public input, e.g., animated “fly through,” scale models, multiple renderings

Fall 2020

- Seek public input on canopy design, community identity features, windscreen graphics, materials & colors
- Executive Committee selects preferred design

Winter 2021

- Progress design of preferred concept to 60%
- Issue construction RFP





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Discussion

